



**Date** March 27, 2023

**RESOLUTION HOLDING HEARING ON REQUEST FROM HUBBELL REALTY COMPANY (OWNER), REPRESENTED BY CALEB SMITH (OFFICER), FOR REVIEW AND APPROVAL OF PUD FINAL DEVELOPMENT PLAN FOR “GRAY’S STATION PLAT 5 TOWNHOMES AND CLUBHOUSE,” FOR PROPERTY LOCATED IN THE VICINITY OF 1300 TUTTLE STREET**

**WHEREAS**, on February 20, 2023, by Roll Call No. 23-0226, the City Council received a communication from the City Plan and Zoning Commission advising that the Commission voted 8-1-1 to **APPROVE** a request from Hubbell Realty Company (Owner), represented by Caleb Smith (Officer), to approve the PUD Final Development Plan for “Gray’s Station Plat 5 Townhomes and Clubhouse” for property located in the vicinity of 1300 Tuttle Street to allow development of the property with the construction of 65 townhomes and a clubhouse amenity building, subject to compliance with all administrative review requirements and the following:

1. Provision of a sidewalk connection from the clubhouse’s outdoor recreation area/dog park that crosses a private drive to link up with a proposed courtyard space in between lots “40” and “41.”
2. Verification that any proposed rooftop mechanical equipment will be screened from view from all sides.
3. Provision of additional information regarding mechanical equipment and associated screening (both ground- and roof-mounted) for the clubhouse building.
4. Provision of more-detailed materiality information for the clubhouse building that matches the standards outlined within the PUD Conceptual Plan.
5. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25; and

**WHEREAS**, the Property is legally described as follows:

OUTLOTS Y AND Z AND A PART OF LOTS 6, 7 AND 9, CENTRAL DES MOINES INDUSTRIAL PARK, AN OFFICIAL PLAT AND PART OF LOTS 80, 81, 82, 85 AND 86 IN FACTORY ADDITION, AN OFFICIAL PLAT, ALL BEING IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID LOT 9, CENTRAL DES MOINES INDUSTRIAL PARK HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHING 575161.83 AND EASTING 1602813.57; THENCE SOUTH 89° (DEGREES) 48' (MINUTES) 50" (SECONDS) EAST ALONG THE



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NORTHERLY LINE OF SAID LOT 9, A DISTANCE OF 339.93 FEET TO THE SOUTHWEST CORNER OF LOT 2, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°46'52" EAST CONTINUING ALONG SAID NORTHERLY LINE, 419.55 FEET TO THE SOUTHEAST CORNER OF SAID LOT 2 ALSO BEING A POINT ON THE WEST RIGHT OF WAY LINE OF SW 14TH STREET; THENCE SOUTH 00°01'02" EAST CONTINUING ALONG SAID NORTHERLY LINE, 69.98 FEET TO THE SOUTHWEST CORNER OF STREET LOT B, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°35'21" EAST CONTINUING ALONG SAID NORTHERLY LINE, 70.00 FEET TO THE NORTHWEST CORNER OF LOT 8, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 00°08'06" WEST CONTINUING ALONG SAID NORTHERLY LINE, 423.70 FEET TO THE SOUTHWEST CORNER OF SAID LOT 8; THENCE SOUTH 89°19'18" EAST CONTINUING ALONG SAID NORTHERLY LINE, 307.20 FEET TO THE SOUTHEAST CORNER OF SAID LOT 8, ALSO BEING THE NORTHEAST CORNER OF SAID LOT 9; THENCE NORTH 00°08'11" WEST ALONG THE WESTERLY LINE OF SAID LOT 7, CENTRAL DES MOINES INDUSTRIAL PARK, 333.39 FEET; THENCE NORTHERLY CONTINUING ALONG SAID WESTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 804.20 FEET, WHOSE ARC LENGTH IS 93.82 FEET AND WHOSE CHORD BEARS NORTH 03°29'07" WEST, 93.77 FEET; THENCE NORTH 06°58'06" WEST CONTINUING ALONG SAID WESTERLY LINE, 15.78 FEET TO THE NORTHWEST CORNER OF SAID LOT 7 ALSO BEING THE SOUTH RIGHT OF WAY LINE OF TUTTLE STREET; THENCE NORTH 81°10'49" EAST ALONG THE NORTH LINE OF SAID LOT 7, A DISTANCE OF 411.78 FEET TO THE NORTHEAST CORNER OF SAID LOT 7; THENCE NORTH 00°05'48" WEST ALONG THE WEST LINE OF SAID LOT 6, CENTRAL DES MOINES INDUSTRIAL PARK, 72.11 FEET TO THE NORTHWEST CORNER OF SAID LOT 6; THENCE NORTH 81°52'47" EAST ALONG THE NORTH LINE OF SAID LOT 6, A DISTANCE OF 347.62 FEET TO THE SOUTHEAST CORNER OF LOT 4, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE NORTH 00°31'45" WEST ALONG THE WESTERLY LINE OF SAID OUTLOT Y, CENTRAL DES MOINES INDUSTRIAL PARK, 63.43 FEET; THENCE NORTH 87°29'10" EAST CONTINUING ALONG SAID WESTERLY LINE, 10.37 FEET; THENCE NORTH 00°12'49" WEST CONTINUING ALONG SAID WESTERLY LINE, 554.99 FEET TO THE NORTHEAST CORNER OF LOT 5, SAID CENTRAL DES MOINES INDUSTRIAL PARK AND HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHING 575907.16 AND EASTING 1604699.48; THENCE SOUTH 89°04'27" EAST ALONG THE NORTH LINE OF SAID OUTLOT Y, 15.36 FEET TO THE NORTHEAST CORNER OF SAID OUTLOT Y; THENCE NORTH 00°21'12" WEST ALONG THE WEST LINE OF SAID LOT 85, FACTORY ADDITION, 8.59 FEET TO THE SOUTHERLY RIGHT OF WAY LINE OF MARTIN LUTHER KING JR PARKWAY; THENCE NORTH 86°10'40" EAST ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 100.23 FEET; THENCE NORTH 84°40'04" EAST CONTINUING ALONG SAID SOUTHERLY RIGHT OF WAY



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LINE, 199.44 FEET TO THE WESTERLY RIGHT OF WAY LINE OF SW 11TH STREET; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 173.21 FEET; THENCE SOUTH 74°42'29" WEST, 99.77 FEET; THENCE SOUTH 15°25'34" EAST, 75.29 FEET; THENCE NORTH 74°32'27" EAST, 99.81 FEET TO SAID WESTERLY RIGHT OF WAY LINE; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 344.37 FEET TO A CORNER ON THE EASTERLY LINE OF SAID LOT 81; THENCE SOUTH 39°15'32" WEST ALONG SAID EASTERLY LINE, 97.08 FEET; THENCE SOUTH 79°27'19" WEST, 73.56 FEET TO THE EASTERLY LINE OF THE WEST HALF OF SAID LOT 81; THENCE SOUTHERLY ALONG SAID EASTERLY LINE AND A CURVE CONCAVE EASTERLY WHOSE RADIUS IS 716.25 FEET, WHOSE ARC LENGTH IS 712.02 FEET AND WHOSE CHORD BEARS SOUTH 06°02'51" WEST, 683.06 FEET; THENCE SOUTH 67°48'12" WEST, 51.41 FEET TO A POINT ON THE EAST LINE OF THE WEST 100 FEET OF SAID LOT 80, FACTORY ADDITION; THENCE SOUTH 00°01'40" EAST ALONG SAID EAST LINE, 767.72 FEET; THENCE SOUTH 45°36'19" EAST, 692.69 FEET TO THE WESTERLY LINE OF SAID LOT 81; THENCE SOUTH 76°09'29" EAST, 50.00 FEET TO SAID EASTERLY LINE OF THE WEST HALF OF LOT 81, HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHING 573317.97 AND EASTING 1605461.84; THENCE SOUTHERLY ALONG SAID EASTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 740.65 FEET, WHOSE ARC LENGTH IS 76.04 FEET AND WHOSE CHORD BEARS SOUTH 17°00'55" WEST, 76.01 FEET; THENCE NORTH 70°20'25" WEST, 50.00 FEET TO SAID WESTERLY LINE OF LOT 81; THENCE NORTH 45°36'19" WEST, 663.55 FEET TO SAID EAST LINE OF THE WEST 100 FEET OF LOT 80; THENCE SOUTH 00°01'40" EAST ALONG SAID EAST LINE, 439.03 FEET TO THE NORTH BANK AND MEANDER LINE OF THE RACCOON RIVER; THENCE NORTH 71°44'57" WEST AND ALONG SAID MEANDER LINE AND THE SOUTHERLY LINE OF SAID OUTLOT Z, CENTRAL DES MOINES INDUSTRIAL PARK, 888.50 FEET; THENCE NORTH 83°01'49" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 463.82 FEET; THENCE NORTH 89°10'53" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 291.24 FEET; THENCE SOUTH 82°06'06" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 296.82 FEET; THENCE SOUTH 72°02'02" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 228.32 FEET TO THE SOUTHWEST CORNER OF SAID OUTLOT Z; THENCE NORTH 00°03'17" EAST ALONG THE WEST LINE OF SAID CENTRAL DES MOINES INDUSTRIAL PARK, 1647.05 FEET TO THE POINT OF BEGINNING AND CONTAINING 83.73 ACRES (3,647,186 SQUARE FEET) AND IS SUBJECT TO ANY AND ALL EASEMENTS OF RECORD.

**WHEREAS**, on February 20, 2023, by Roll Call No. 23-0225, it was duly resolved by the City Council that the request for approval of the PUD Final Development Plan be set down for hearing on March 6, 2023, at 5:00 p.m. at City Hall Council Chambers, 400 Robert D. Ray Dr., Des Moines, Iowa 50309; and



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**WHEREAS**, on March 6, 2023, by Roll Call No. 23-0333, the City Council opened and further continued said hearing to 5:00 p.m. on March 27, 2023, upon request by the applicants’ representative for additional review time.

**WHEREAS**, due notice of said hearing was published in the Des Moines Register, as provided by law, setting forth the time and place for hearing on said proposed PUD Final Development Plan; and

**WHEREAS**, in accordance with said notice, those interested in said proposed PUD Final Development Plan, both for and against, have been given opportunity to be heard with respect thereto and have presented their views to the City Council.

**NOW, THEREFORE, BE IT RESOLVED**, by the City Council of the City of Des Moines, Iowa, as follows:

1. Upon due consideration of the facts, and any and all statements of interested persons and arguments of counsel, any objections to the proposed “Gray’s Station Plat 5 Townhomes and Clubhouse” PUD Final Development Plan are hereby overruled, and the hearing is closed.
2. The proposed Amendment to the “Gray’s Station Plat 5 Townhomes and Clubhouse” PUD Final Plan, as on file in the Development Services Department, is hereby approved subject to compliance with all administrative review comments as well as those requirements stated above.

Moved by \_\_\_\_\_ to adopt. Second by \_\_\_\_\_

APPROVED AS TO FORM:

/s/ Chas M. Cahill  
Chas M. Cahill  
Assistant City Attorney

(SITE-2022-000153)

COUNCIL ACTION	YEAS	NAYS	PASS	ABSENT
COWNIE				
BOESEN				
GATTO				
MANDELBAUM				
SHEUMAKER				
VOSS				
WESTERGAARD				
TOTAL				

MOTION CARRIED APPROVED

\_\_\_\_\_  
Mayor

**CERTIFICATE**

I, LAURA BAUMGARTNER, City Clerk of said City hereby certify that at a meeting of the City Council of said City of Des Moines, held on the above date, among other proceedings the above was adopted.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my seal the day and year first above written.

\_\_\_\_\_  
City Clerk

February 14, 2023

Communication from the City Plan and Zoning Commission advising that at their February 2, 2023 meeting, the following action was taken regarding a request from Hubbell Realty Company (owner), represented by Caleb Smith (officer), for the following regarding property located in the vicinity of 1300 Tuttle Street:

- A) Review and approval of a 1<sup>st</sup> amendment to the Gray's Station PUD Conceptual Plan, to update the realignment of Southwest 13<sup>th</sup> Street and to allow administrative review of future development site plans in leu of public review by the Plan and Zoning Commission and City Council.
- B) Review and approval of a PUD Final Development Plan "Gray's Station Telus Condos," to allow for the construction of two condo buildings containing 84 residential units.
- C) Review and approval of a PUD Final Development Plan "Gray's Station Plat 5 Townhomes and Clubhouse," to allow for the construction of 65 townhomes and a clubhouse amenity building.

**COMMISSION RECOMMENDATION:**

After public hearing, the members voted 8-1-1 as follows.

Commission Action:	Yes	Nays	Pass	Absent
Francis Boggus				X
Dan Drendel	X			
Leah Rudolphi	X			
Carol Maher		X		
Abby Chungath				X
Kayla Berkson	X			
Chris Draper			X	
Todd Garner				X
Johnny Alcivar	X			
Justyn Lewis	X			
Carolyn Jenison				X
William Page	X			
Andrew Lorentzen				X
Emily Webb	X			
Katie Gillette	X			

**APPROVAL** of Part A) Approval of the request to amend the Gray's Station PUD Conceptual Plan to update the realignment of Southwest 13th Street.

Furthermore, Staff recommends limited approval of the request to amend the Gray's Station PUD Conceptual Plan to revise the 'Notes' section to allow administrative review of future development site plans in lieu of public review by the Plan and Zoning Commission and City Council, as follows:

- Any final development plan for a new row home type development or a minor revision to an existing development is subject to administrative (by staff) review and approval.
- Any final development plan for a mixed-use building or a multiple-household residential building is subject to review and approval by the Plan and Zoning Commission, with any appeal of that decision considered by the City Council.

Part B) Approval of the proposed PUD Final Development Plan "Gray's Station Telus Condos," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a "band" of architectural concrete masonry unit materials from grade up to 2' for the standalone garages that matches the concrete masonry units proposed for the condo buildings.
2. Provision of a sidewalk connection from the southeastern corner of "Building 2" that crosses the internal vehicular drive to link up with the north-south pedestrian pathway that bisects this development area.
3. Provision of more-detailed exterior façade materiality information that matches the standards outlined within the PUD Conceptual Plan.
4. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

Part C) Approval of the proposed PUD Final Development Plan "Gray's Station Plat 5 Townhomes and Clubhouse," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a sidewalk connection from the clubhouse's outdoor recreation area/dog park that crosses a private drive to link up with a proposed courtyard space in between lots "40" and "41."
2. Verification that any proposed rooftop mechanical equipment will be screened from view from all sides.
3. Provision of additional information regarding mechanical equipment and associated screening (both ground- and roof-mounted) for the clubhouse building.
4. Provision of more-detailed materiality information for the clubhouse building that matches the standards outlined within the PUD Conceptual Plan.

5. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

## **STAFF RECOMMENDATION TO THE P&Z COMMISSION**

Part A) Staff recommends approval of the request to amend the Gray's Station PUD Conceptual Plan to update the realignment of Southwest 13th Street.

Furthermore, Staff recommends limited approval of the request to amend the Gray's Station PUD Conceptual Plan to revise the 'Notes' section to allow administrative review of future development site plans in lieu of public review by the Plan and Zoning Commission and City Council, as follows:

- Any final development plan for a new row home type development or a minor revision to an existing development is subject to administrative (by staff) review and approval.
- Any final development plan for a mixed-use building or a multiple-household residential building is subject to review and approval by the Plan and Zoning Commission, with any appeal of that decision considered by the City Council.

Part B) Staff recommends approval of the proposed PUD Final Development Plan "Gray's Station Telus Condos," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a "band" of architectural concrete masonry unit materials from grade up to 2' for the standalone garages that matches the concrete masonry units proposed for the condo buildings.
2. Provision of a sidewalk connection from the southeastern corner of "Building 2" that crosses the internal vehicular drive to link up with the north-south pedestrian pathway that bisects this development area.
3. Provision of more-detailed exterior façade materiality information that matches the standards outlined within the PUD Conceptual Plan.
4. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

Part C) Staff recommends approval of the proposed PUD Final Development Plan "Gray's Station Plat 5 Townhomes and Clubhouse," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a sidewalk connection from the clubhouse's outdoor recreation area/dog park that crosses a private drive to link up with a proposed courtyard space in between lots "40" and "41."
2. Verification that any proposed rooftop mechanical equipment will be screened from view from all sides.

3. Provision of additional information regarding mechanical equipment and associated screening (both ground- and roof-mounted) for the clubhouse building.
4. Provision of more-detailed materiality information for the clubhouse building that matches the standards outlined within the PUD Conceptual Plan.
5. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

## STAFF REPORT TO THE PLANNING COMMISSION

### I. GENERAL INFORMATION

1. **Purpose of Request:** The applicant is requesting an amendment to the Gray's Station PUD Conceptual Plan, which would alter the alignment of Southwest 13<sup>th</sup> Street between Tuttle Street and Grays Parkway. Further, the applicant is proposing to amend the PUD's 'Notes' to remove the requirement that "All Final Development Plans are subject to review and approval by the Plan & Zoning Commission and the City Council," which would instead allow administrative review in lieu.

Additionally, the developer is seeking to subdivide the 'Gray's Station Plat 5' area within the PUD to allow the development and construction of 65 one-household residential lots containing row homes, a clubhouse amenity building, a lot containing two condo buildings with 84 residential units, and two outlots.

2. **Size of Site:** Gray's Station PUD – 84.4 acres; Plat 5 Area – 11.1 acres.
3. **Existing Zoning (site):** Gray's Station PUD, Planned Unit Development District.
4. **Existing Land Use (site):** The area is currently undeveloped land.
5. **Adjacent Land Use and Zoning:**

**North** – "DX2"; Uses are Tuttle Street, open space, and vacant industrial buildings.

**South** – "Gray's Station PUD" & "F"; Uses are open space, a city stormwater management facility, the Raccoon River, and Gray's Lake Park.

**East** – "Gray's Station PUD"; Uses are townhomes and multiple-household dwelling units.

**West** – "Gray's Station PUD" & "DX2"; Uses are industrial warehousing and undeveloped land.

6. **General Neighborhood/Area Land Uses:** The subject property is located generally to the south of West Martin Luther King, Jr. Parkway and to the west of Southwest 11<sup>th</sup> Street in the southern fringes of Downtown. The site's vicinity consists of mixed-use and medium-density residential development, parkland, and undeveloped parcels. The Raccoon River flanks the southern boundary of the site. Gray's Lake Park lies further south of the river.



- 7. Applicable Recognized Neighborhood(s):** The subject property is located within the Downtown Des Moines Neighborhood Association. All neighborhood associations were notified of the public hearing by emailing of the Preliminary Agenda on January 13, 2023, and by emailing of the Final Agenda on January 27, 2023. Additionally, separate notifications of the hearing for this specific item were mailed on January 13, 2023 (20 days prior to the public hearing) and January 23, 2023 (10 days prior to the public hearing) to the Downtown Des Moines Neighborhood Association and to the primary titleholder on file with the Polk County Assessor for each property within 250 feet of the site.

All agendas and notices are mailed to the primary contact(s) designated by the recognized neighborhood association to the City of Des Moines Neighborhood Services Department on the date of the mailing. The Sheridan Gardens Neighborhood does not have a mailing address on file. The Brook Run Neighborhood Association mailings were sent to Brandon Brown, 120 Southwest 5<sup>th</sup> Street, Unit 101, Des Moines, IA 50309.

The applicant can provide a summary of the neighborhood outreach at the public hearing.

- 8. Relevant Zoning History:** The subject property was rezoned to "PUD" on August 14, 2017, at which time a general "PUD" Conceptual Plan was adopted.
- 9. PlanDSM: Creating Our Tomorrow Plan Land Use Plan Designation:** Downtown Mixed Use and Neighborhood Mixed Use.
- 10. Applicable Regulations:** Taking into consideration the criteria set forth in Chapter 18B of the Iowa Code, the Commission reviews all proposals to amend zoning boundaries or regulations within the City of Des Moines. Such amendments must be in conformance with the comprehensive plan for the City and designed to meet the criteria in 414.3 of the Iowa Code. The Commission may make recommendations to the City Council on conditions to be made in addition to the existing regulations so long as the subject property owner agrees to them in writing. The recommendation of the Commission will be forwarded to the City Council.

The application, accompanying evidence and "PUD" Conceptual Plan shall be considered by the Plan and Zoning commission at a public hearing. The Commission shall review the conformity of the proposed development with the standards of the City Code and with recognized principles of civic design, land use planning, and landscape architecture. After the hearing, the Commission may vote to recommend either approval or disapproval of the amended "PUD" Conceptual Plan as submitted, or to recommend that the developer amend the plan or request to preserve the intent and purpose of this chapter to promote public health, safety, morals and general welfare. The recommendations of the Commission shall be referred to the City Council.

Taking into consideration the criteria set forth in Chapter 18B of the Iowa Code, the Commission shall determine if such Preliminary Plat conforms to the standards and requirements outlined in Chapter 354 of the Iowa Code, and the City Subdivision Ordinance and shall approve, conditionally approve or reject such Plat within 45 days after the date of submission to the City Permit and Development Center. Unless the applicant agrees in writing to an extension of time, the Preliminary Plat shall be

deemed approved if the Commission does not act within such 45-day period. The Commission's action for approval or conditional approval shall be null and void unless the Final Plat is submitted to the City Permit and Development Center within 270 days after the date of such action; provided, however, that the Permit and Development Administrator may grant, upon written request of the applicant, up to a 90-day extension for submittal of the Final Plat to the City Permit and Development Center.

Taking into consideration the criteria set forth in Chapter 18B of the Iowa Code, the Commission shall determine if such Preliminary Plat conforms to the standards and requirements outlined in Chapter 354 of the Iowa Code, and the City Subdivision Ordinance and shall approve, conditionally approve or reject such Plat within 45 days after the date of submission to the City Permit and Development Center. Unless the applicant agrees in writing to an extension of time, the Preliminary Plat shall be deemed approved if the Commission does not act within such 45-day period. The Commission's action for approval or conditional approval shall be null and void unless the Final Plat is submitted to the City Permit and Development Center within 270 days after the date of such action; provided, however, that the Permit and Development Administrator may grant, upon written request of the applicant, up to a 90-day extension for submittal of the Final Plat to the City Permit and Development Center.

Pursuant to Section 135-9.1.1.B of the Planning and Design Ordinance, the site plan review requirements of Chapter 135 are designed to ensure the orderly and harmonious development of property in a manner that shall:

- Promote the most beneficial relation between present and proposed future uses of land and the present and proposed future circulation of traffic throughout the city;
- Permit present development of property commensurate with fair and orderly planning for future development of other properties in the various areas of the city with respect to the availability and capacity, present and foreseeable, of public facilities and services. The factors to be considered in arriving at a conclusion concerning proposed present development of property shall include the following:
  - The maximum population density for the proposed development, the proposed density of use, and consideration of the effect the proposal will have on the capacity of existing water and sanitary sewer lines to the end that existing systems will not become overloaded or capacity so substantially decreased that site use will inhibit or preclude planned future development;
  - Zoning restrictions at the time of the proposal;
- The city's comprehensive plan;
- The city's plans for future construction and provision for public facilities and services; and

- The facilities and services already available to the area which will be affected by the proposed site use;
- Encourage adequate provision for surface and subsurface drainage, in order to ensure that future development and other properties in various areas of the city will not be adversely affected;
- Provide suitable screening of parking, truck loading, refuse and recycling disposal, and outdoor storage areas from adjacent residential districts;
- Encourage the preservation of canopied areas and mature trees and require mitigation for the removal of trees; and
- Consider the smart planning principles set forth in Iowa Code Chapter 18B.

## II. ADDITIONAL APPLICABLE INFORMATION

1. **Gray's Station PUD Conceptual Plan:** If the proposed amendment to the Gray's Station "PUD" is approved by the City Council, the applicant must submit to the Planning and Urban Design Administrator a revised version of the "PUD" Conceptual Plan that reflects any conditions of approval.
2. **Realignment of Southwest 13<sup>th</sup> Street:** The proposed amendment to the PUD incorporates a slight realignment of Southwest 13<sup>th</sup> Street. The initial alignment proposed for Southwest 13<sup>th</sup> Street was straight north-south between Tuttle Street and Grays Parkway. With the proposed realignment, the southern portion of the street curves slightly east to join Grays Parkway. The slight realignment would not change or impact the design of the PUD in any significant manner.
3. **Review and Approval:** Per the existing PUD Conceptual Plan, all final development plans within the PUD are currently subject to review and approval by the Plan and Zoning Commission and the City Council. The proposed amendment would eliminate the need for a Commission and Council review for future development plans and would allow administrative-only review. While an administrative review is appropriate for minor changes within the existing development plans and for row homes, a more-comprehensive review, including approval by the Plan and Zoning Commission, is necessary for larger mixed-use and multi-family residential developments. The PUD is well-established to allow developments such as row homes. However, there is some ambiguity relating to larger mixed-use and multi-family buildings which might be potentially sited in the southern portion of the PUD. Staff feels a public process is reasonably necessary to allow comprehensive review of those future development plans and other larger, mixed-use, and multiple-household residential buildings.
4. **Preliminary Plat:** The proposed preliminary plat would involve the creation and extension of public and private streets to serve the development. As an infill, Downtown site, the street layouts (both public and private) are proposed to complement the City street grid (in terms of both layout and construction) that already exists in this area. Murphy Street and Grays Parkway are proposed to extend to the west, with additional westward extensions of already-existing east-west private streets. A new segment of Southwest 13<sup>th</sup> Street is proposed to be constructed from Tuttle

Street to Grays Parkway. Additional east-west private streets are proposed in the southern portion of the development.

65 one-household lots would be created for clustered, rowhome-style development. Additionally, larger lots would be created for a neighborhood "clubhouse" building and open space, as well as a lot that would contain (2) 3-story residential buildings for a total of 84 dwelling units.

All new streets would be constructed with urban cross-sections generally consistent with the construction of streets within the Gray's Station area and overall Downtown. Public utilities (sanitary sewer, water service) are proposed to be extended both within public rights-of-way and within private streets. Public storm sewer is also proposed within the development, and the area's overall stormwater management has been contemplated and accounted for with the existing Gray's Station city stormwater facility that sits to the south of this development area.

On-street and off-street pedestrian connections are proposed that would provide linkages both within this development, as well as with the Gray's Station Trail to the south and the recently-platted area to the east. The Tuttle Street Cycle Track project is proposed to be extended through the northern border of this development area. A pedestrian "Paseo" is proposed to be extended from the east and bisect the northern half of the development.

- 5. Development Plan – "Gray's Station Telus Condos":** The Telus condos development plan consists of (2) 3-story residential buildings that would contain 84 dwelling units, as well three standalone garage buildings for motor vehicle parking.

The northern building ("Building 1") is proposed to frame the corner of Tuttle and Southwest 13<sup>th</sup> Streets, with the southern building ("Building 2") proposed to frame the corner of Murphy and Southwest 13<sup>th</sup> Streets. The aforementioned Paseo is proposed between the two buildings before intersecting with Southwest 13<sup>th</sup> Street. A motor vehicle parking area would sit behind the proposed residential buildings. A total of 137 motor vehicle parking spaces (53 uncovered, 84 in garages) are proposed. 20 bicycle parking spaces are proposed adjacent to the Paseo in between the residential buildings.

Building and site design is required to conform to the design criteria set forth in the Gray's Station PUD, specifically the criteria for "High Density Residential" buildings. The buildings are proposed to be clad in a mix of brick, concrete masonry units, fiber cement board, and architectural metal paneling in a manner consistent with the material palette stipulated within the PUD Conceptual Plan. Buildings are proposed to sit abutting street-facing lot lines, with street-facing entrances. Proposed floor plans demonstrate a stacking action amongst the units, with ground floor units opening to the street, and upper-floor units with street- or internal-facing balconies.

- 6. Development Plan – "Gray's Station Plat 5 Row Homes and Clubhouse":** The development plan for the row homes and clubhouse demonstrates 17 individual clusters of 3-5 row homes, as well as a larger area within the southern portion of the development area for the clubhouse and outdoor recreation areas. Each rowhome would sit on its own lot.

Row homes are proposed to be oriented toward either a public street or a designated pedestrian way. Rear-loading garages for motor vehicle parking are proposed for each rowhome. These garages would have space for up to 2 vehicles and are proposed to be accessed from the development's private drives that would function as alleys. Additional motor vehicle parking facilities are proposed to be either on street in a parallel fashion or off of a private drive directly abutting the clubhouse/outdoor recreation area. 20 bicycle parking spaces are demonstrated in a clumped fashion oriented around the outdoor recreation area.

The outdoor recreation area is proposed to consist of a swimming pool, dog park, pickleball courts, landscaping arrangements, and other unprogrammed open space. The clubhouse is designed as a flexible, general-use entertainment space that would open up to the pool area.

Building and individual site design is required to conform to the design criteria set forth in the Gray's Station PUD, specifically the criteria for "Low-Medium Residential" buildings. With the exception of the clubhouse, all buildings that are a part of this development plan are proposed to be at least 2 stories. Variations of 5 different rowhome "product types" are proposed to be placed in clusters throughout the area of this development plan. These product types differ in their building heights, exterior façade materials mixes, color palettes, entryway configurations, and proposed floor plans. Similar to the condo buildings, a mix of fiber cement board paneling, fiber cement board lap siding, architectural metal paneling, brick, stone, and finished concrete surfaces are proposed for the exterior of the row homes. Material placement and percentages differ by specific product type. Individual townhouse clusters are positioned between approximately 5 to 12 feet from the edges of streets or other pedestrian ways. Row homes would contain stoops/porches that would orient toward their respective streets/pedestrian ways.

## **SUMMARY OF DISCUSSION**

Nick Tarpey presented staff report and recommendation.

Chris Draper asked how large a project would need to be before it would come before the commission if the staff recommendation is adopted.

Nick Tarpey stated multi-family or mixed-use projects. Any one household residential project would be administratively reviewed.

Chris Draper asked if there are any future concerns with connectivity to broadband.

Jason Van Essen stated the question of broadband is outside the scope of this PUD. Amendments before the commission would not have an impact.

Will Page asked if the provision "to allow administrative review of future development site plans in lieu of public review by the Plan and Zoning Commission and City Council" is applicable to this project only.

Jason Van Essen stated there is an understood development pattern for the rowhomes and that staff supports reviewing those administratively. Other new project types should

have a public hearing, but staff believes that could be limited to just the Commission, eliminating the need to go before the City Council. These amendments would take effect for future projects.

Johnny Alcivar asked if there was a threshold for rowhome development being administratively reviewed.

Jason Van Essen stated if the staff recommendation is adopted, rowhome development would only be reviewed administratively.

Chris Draper asked if there were thoughts of how different a rowhome development would need to be before its reviewed publicly.

Jason Van Essen stated within the PUD, the developer has created several different designs. City staff feels comfortable reviewing proposed changes given the existing standards within the PUD and past negotiations on previous iterations.

Carol Maher asked if that would give city staff the ability to approve type 2 deign alternatives.

Jason Van Essen stated no, the development will need to match the standards within the PUD. If city staff and the developer disagreed on those standards, it would come before the commission.

Dan Drendel asked if it would be developed under the same PUD if another developer bought out future phases.

Jason Van Essen stated it is possible for another builder to follow the standards within the current PUD. If they wanted to go a different direction, amendments to the PUD would be needed to accommodate new designs.

Chris Draper asked if the existing powerlines would be undergrounded.

Jason Van Essen stated there are a few major lines that will remain.

Caleb Smith, Hubbell Realty Company, 6900 Westown Parkway, WDM stated after a simple restriping plan for the LINC, they thought it would make sense to suggest removing the clause that requires review by the Plan and Zoning Commission and City Council. City staff has come forward with a reasonable approach given the size, style and scope of the townhomes have been well established. It does make sense for the commission to review multi-family projects due to the variety they will have. The overhead utility line is a massive transmission line that would be a multi-million dollar project and something Mid-American Energy isn't supportive of. These plans do include utility easement corridors that will provide all units with telecom communication.

Will Page asked if they could explain their thoughts of darker colors being more appropriate for urban settings.

Caleb Smith stated they learned a lot during the first phase of this development and noticed those darker colors sold better. They also have a network of builders across the country that allows them to see these types of trends.

Carol Maher asked if these are all rental units.

Calen Smith stated there are no rental properties in this phase of the project.

Carol Maher asked about parking around the multi-family development.

Ryan Hardisty, Civil Design Advantage, 4121 NW Urbandale Drive, Urbandale, IA stated the TELUS project has 84 total units with 137 parking spaces. There are 84 total garages with a few being detached.

Carol Maher asked why the parking ratio is more than 1 to 1.

Ryan Hardisty stated given the success of the LINC project, parking issues started to occur once the building was fully rented. Based on those issues, they revamped parking for TELUS.

Carol Maher asked if a parking space is included with purchase of a unit.

Michael Bialas, Hubbell Realty Company, 6900 Westown Parkway, WDM stated the parking spaces and garages will be sold separately. If they are not sold, those spaces would be turned over to the association.

Carol Maher asked if they would offer indoor bike storage.

Michael Bialas stated that is something they are exploring. If all the garages are not sold, they would likely convert one to bike storage.

Carol Maher stated they are only focused on how to fit more cars, instead of encouraging other means of transportation. They should be more forward thinking and include adequate bike parking and EV charging stations.

Emily Webb stated she understands the need for more parking. She lives in an urban rowhouse setting and just having a garage makes it challenging for others to visit you.

Justyn Lewis stated there are parking issues around this area, but he would like to see this development promote bicycling and walkability.

Ryan Hardisty stated they are providing 20 bike parking spaces at the main entrance.

Justyn Lewis asked if that's inside the building.

Ryan Hardisty stated they're exterior, located off SW 13<sup>th</sup> Street.

Will Page asked if they agree with staff recommendations.

Caleb Smith stated yes.

## **CHAIRPERSON OPENED THE PUBLIC HEARING**

Troy Hall, 2530 University Avenue stated he is generally supportive of this development and would like to continue seeing densification of downtown. He would encourage the developer to embrace architectural diversity and encourage different means of transportation.

## **CHAIRPERSON CLOSED THE PUBLIC HEARING**

Chris Draper stated he hopes we aren't straying away from the underlying strategies within tax abatement that would reduce parking ratios.

## **COMMISSION ACTION:**

Will Page made a motion for:

Part A) Approval of the request to amend the Gray's Station PUD Conceptual Plan to update the realignment of Southwest 13th Street.

Furthermore, Staff recommends limited approval of the request to amend the Gray's Station PUD Conceptual Plan to revise the 'Notes' section to allow administrative review of future development site plans in lieu of public review by the Plan and Zoning Commission and City Council, as follows:

- Any final development plan for a new row home type development or a minor revision to an existing development is subject to administrative (by staff) review and approval.
- Any final development plan for a mixed-use building or a multiple-household residential building is subject to review and approval by the Plan and Zoning Commission, with any appeal of that decision considered by the City Council.

Part B) Approval of the proposed PUD Final Development Plan "Gray's Station Telus Condos," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a "band" of architectural concrete masonry unit materials from grade up to 2' for the standalone garages that matches the concrete masonry units proposed for the condo buildings.
2. Provision of a sidewalk connection from the southeastern corner of "Building 2" that crosses the internal vehicular drive to link up with the north-south pedestrian pathway that bisects this development area.
3. Provision of more-detailed exterior façade materiality information that matches the standards outlined within the PUD Conceptual Plan.
4. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.



Part C) Approval of the proposed PUD Final Development Plan "Gray's Station Plat 5 Townhomes and Clubhouse," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a sidewalk connection from the clubhouse's outdoor recreation area/dog park that crosses a private drive to link up with a proposed courtyard space in between lots "40" and "41."
2. Verification that any proposed rooftop mechanical equipment will be screened from view from all sides.
3. Provision of additional information regarding mechanical equipment and associated screening (both ground- and roof-mounted) for the clubhouse building.
4. Provision of more-detailed materiality information for the clubhouse building that matches the standards outlined within the PUD Conceptual Plan.
5. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

Motion passed: 8-1-1

Respectfully submitted,



Jason Van Essen, AICP  
Planning & Urban Design Administrator

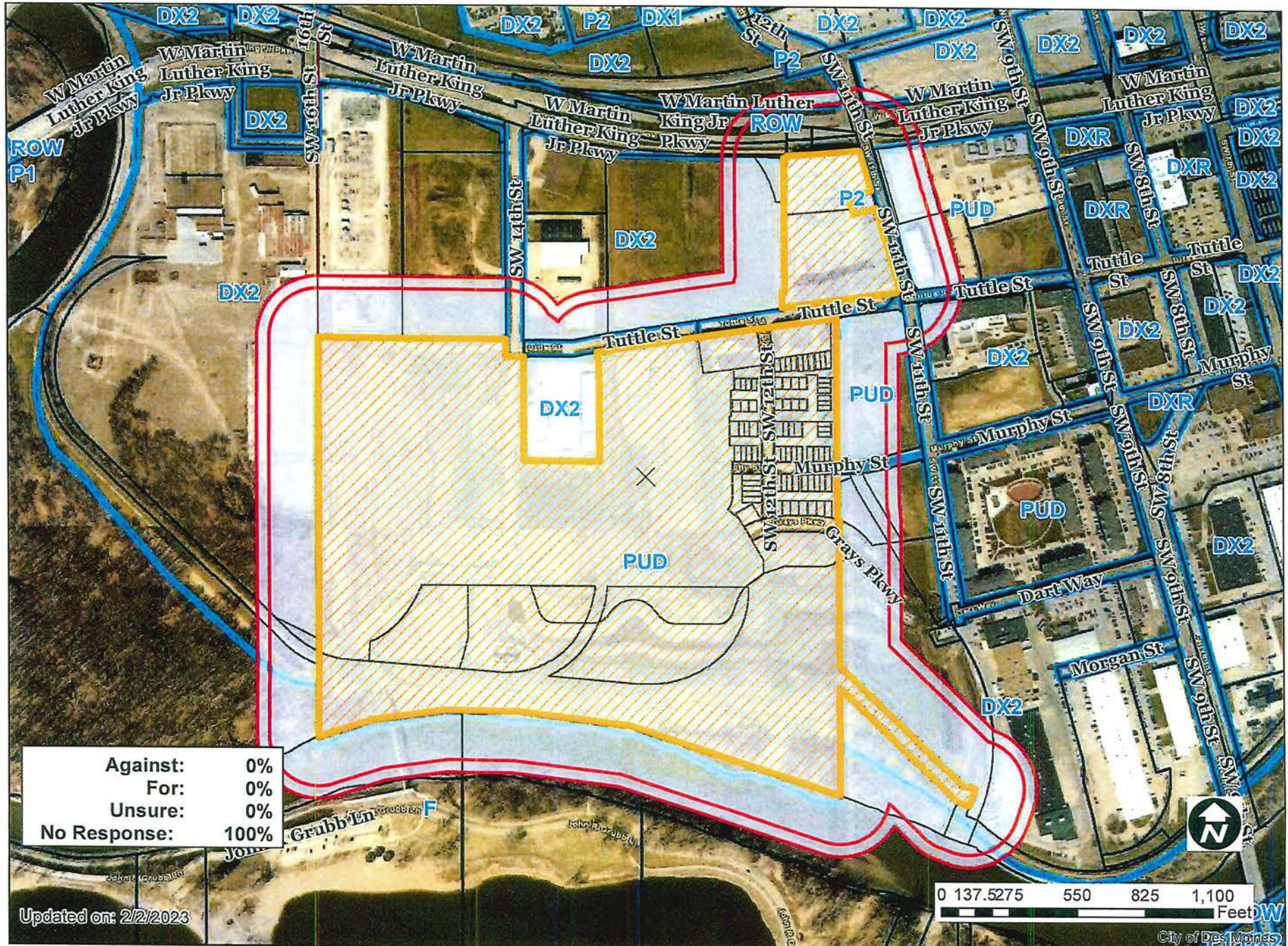
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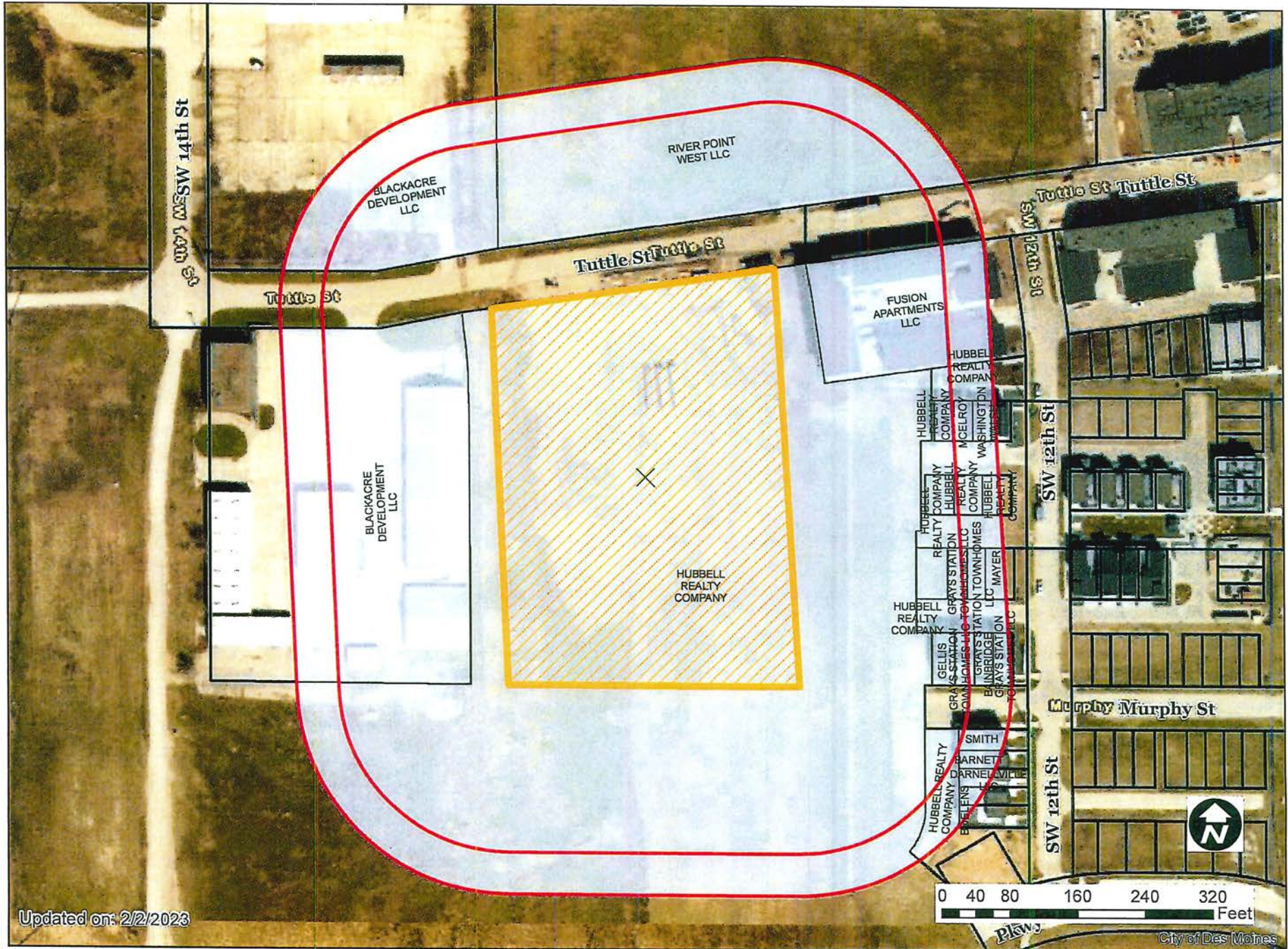
Updated on: 1/12/2023

1 inch = 517 feet

23-0333

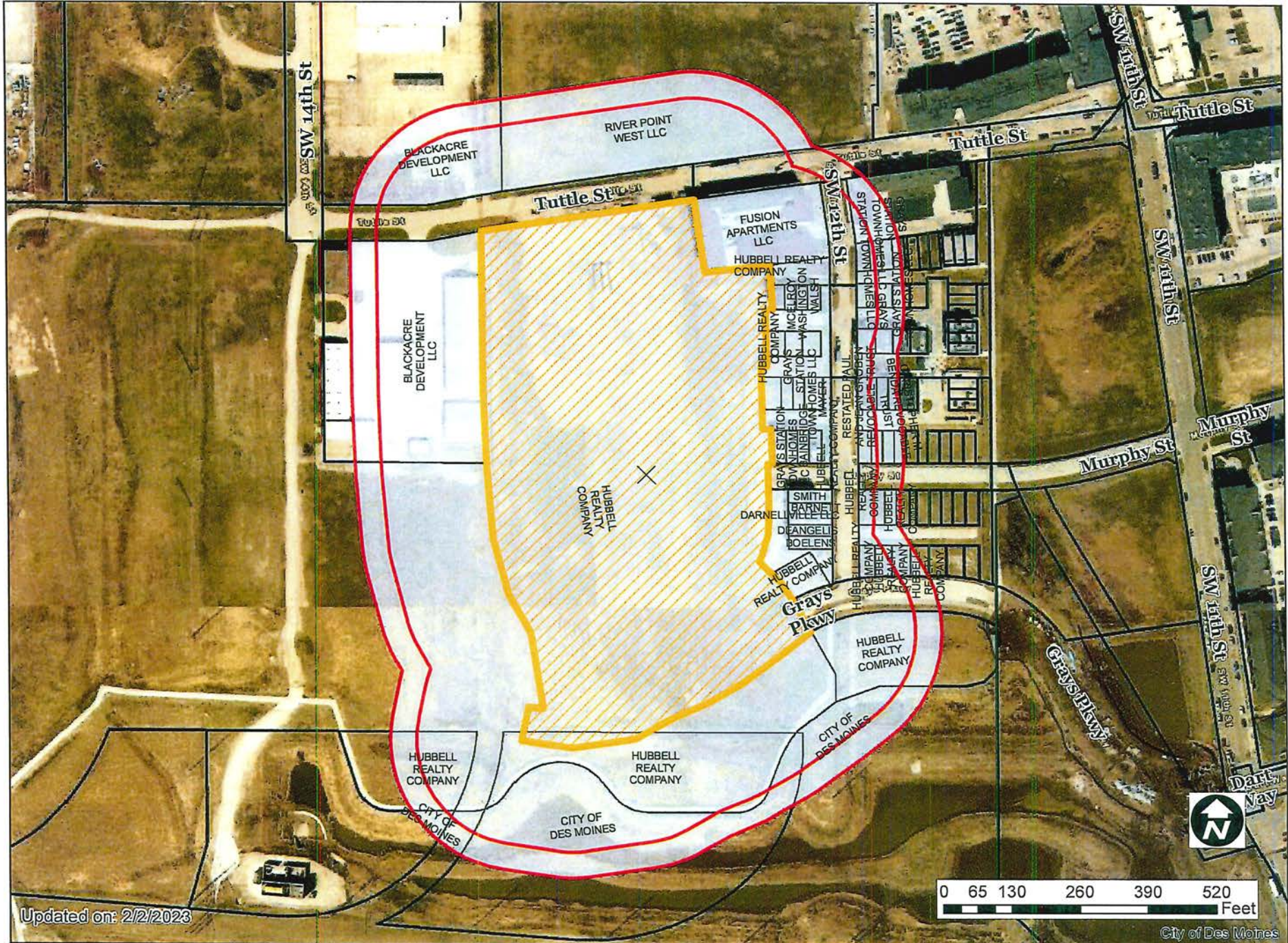


1 inch = 517 feet

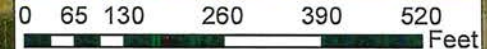


Updated on: 2/2/2023

1 inch = 151 feet

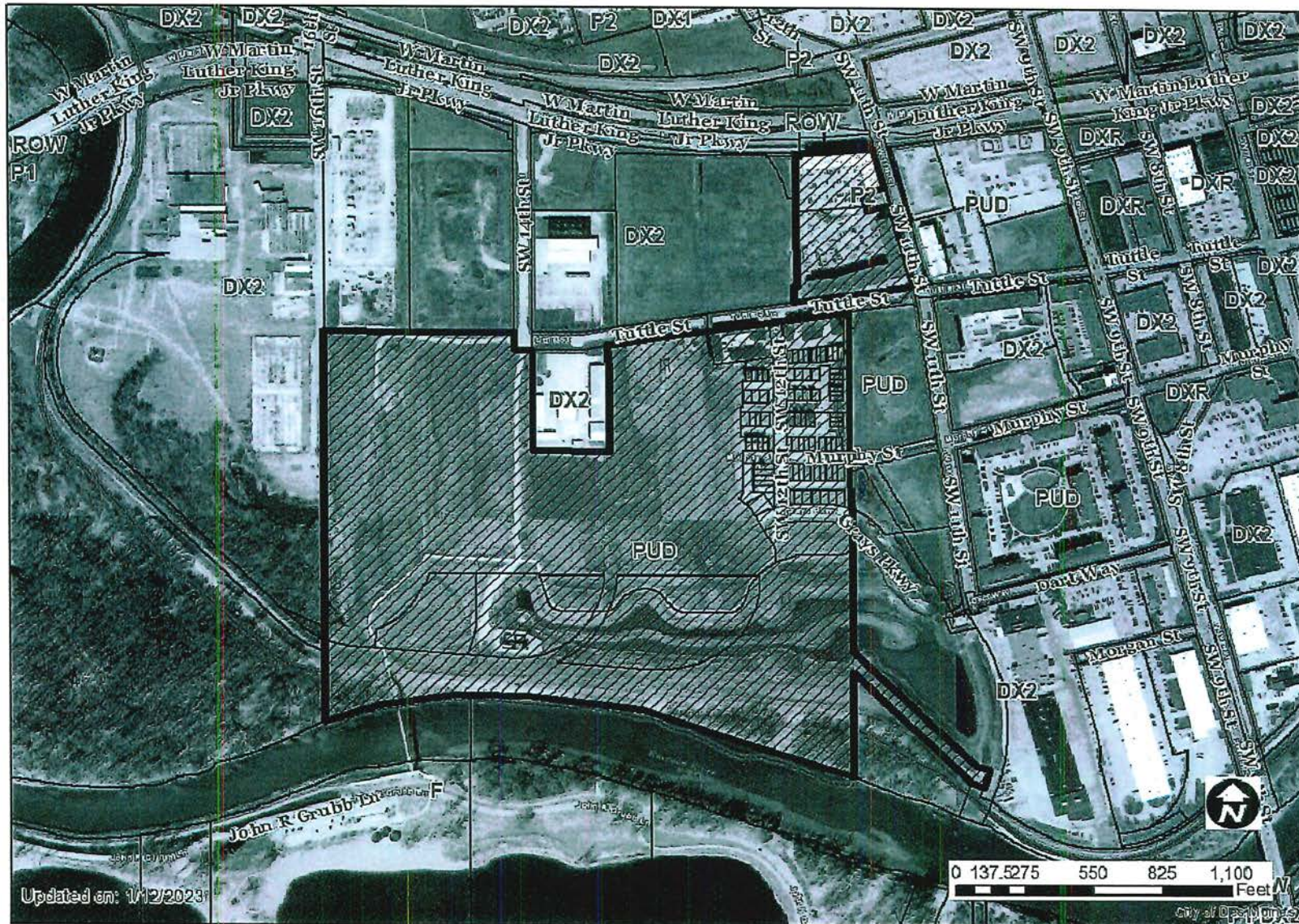


Updated on: 2/2/2023



City of Des Moines

1 inch = 243 feet



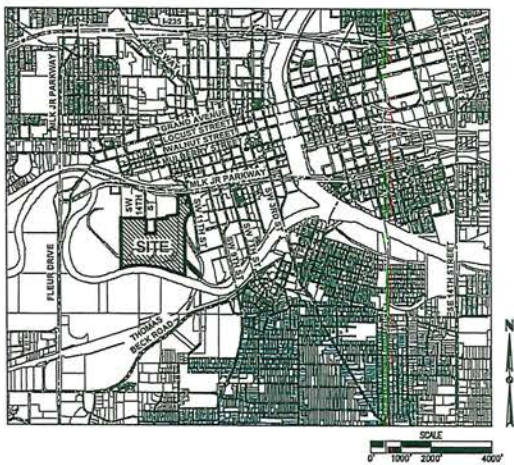
1 inch = 517 feet



# CONCEPTUAL PLAN, FIRST AMENDMENT GRAY'S STATION

A PLANNED UNIT DEVELOPMENT IN THE CITY OF DES MOINES, COUNTY OF POLK, STATE OF IOWA

VICINITY MAP (1"=2000')



**DEVELOPER**

HUBBELL REALTY COMPANY  
CONTACT: JOE PIETRUSZYNSKI  
3890 WESTOWN PARKWAY  
WEST DES MOINES, IOWA 50326  
PH. (515) 243-3228

**OWNER**

NORFOLK SOUTHERN RY  
TAX ID#1  
3 COMMERCIAL PLACE, SUITE 23  
NORFOLK, VIRGINIA 23510  
CITY OF DES MOINES  
400 ROBERT D RAY DRIVE  
DES MOINES, IOWA 50309

**SHEET INDEX**

- 1 PLANNED UNIT DEVELOPMENT (COVER SHEET)
- 2 PLANNED UNIT DEVELOPMENT (BASE MAP)

**CITY PRE-APPLICATION MEETING DATE**

MAY 2, 2017

**ZONING**

- EXISTING:**
- C-3B CENTRAL BUSINESS MIXED USE DISTRICT
  - FW FLOODWAY DISTRICT
  - DOWNTOWN OVERLAY DISTRICT
  - GAMBLING GAMES PROHIBITION OVERLAY DISTRICT

**PROPOSED:**

- P.U.D.

**PLAN DSM LAND USE**

DOWNTOWN MIXED USE

**ENGINEER**

CIVIL DESIGN ADVANTAGE, LLC  
CONTACT: RYAN HARDESTY  
4121 NW URBANDALE DRIVE  
URBANDALE, IOWA 50322  
PH. (515) 389-4400

**NEIGHBORHOOD MEETINGS & MEETING DATE**

The Gray's Station site is located such that it does not have many citizen neighbors to speak of. Hubbell Realty Company determined that the best way to solicit input related to the potential value and development of the Gray's Station neighborhood was to invite community leaders to participate in a Steering Committee for the neighborhood. This Steering Committee included members of City Staff, members of the Urban Land Institute Iowa District Council, downtown neighborhood associations, a City Council member, and other individuals whose input would assist Hubbell Realty Company to create a unique new neighborhood for Des Moines. This group met twice in the Summer of 2016 to engage in setting the vision for the neighborhood and to review early concepts for Gray's Station. Their participation has influenced the resulting Conceptual Development Plan for Gray's Station.

**Steering Committee Meeting #1 - June 14, 2016**

- o Location: RDC Planning & Design, 301 and Avenue Des Moines, Iowa 50309
- o Time: 2 PM - 4 PM

**Steering Committee Meeting #2 - August 15, 2016**

- o Location: RDC Planning & Design, 301 and Avenue Des Moines, Iowa 50309
- o Time: 2 PM - 4 PM
- o Neighborhood Open House - August 15, 2016
- o Location: Cityville on 8th/ Common Room, 950 SW 8th/ ST Des Moines, Iowa 50309
- o Time: 5:30 PM - 8:30 PM

**P.U.D. PROPERTY DESCRIPTION**

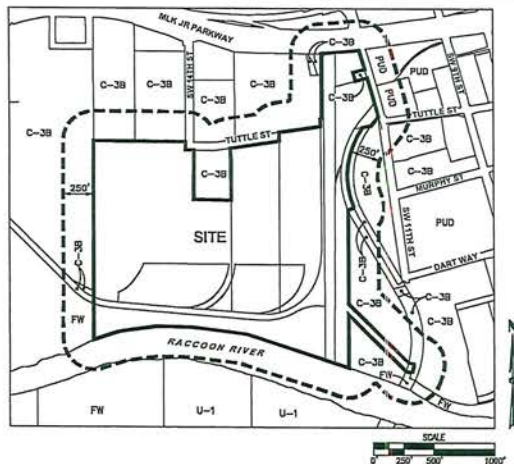
OUTLOTS Y AND Z AND A PART OF LOTS 6, 7 AND 8, CENTRAL DES MOINES INDUSTRIAL PARK, AN OFFICIAL PLAT AND PART OF DES 50, 81, 82, 85 AND 86 IN FACTORY ADDITION, AN OFFICIAL PLAT, ALL BEING IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID LOT 9, CENTRAL DES MOINES INDUSTRIAL PARK HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHING 575161.83 AND EASTING 1602813.07; THENCE SOUTH 89° (DEGREES) 48' (MINUTES) 50" (SECONDS) EAST ALONG THE NORTHERLY LINE OF SAID LOT 9, A DISTANCE OF 338.63 FEET TO THE SOUTHWEST CORNER OF LOT 2, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°46'52" EAST CONTINUING ALONG SAID NORTHERLY LINE, 418.55 FEET TO THE SOUTHEAST CORNER OF SAID LOT 9 ALSO BEING A POINT ON THE WEST RIGHT OF WAY LINE OF SW 14TH STREET; THENCE SOUTH 00°01'02" EAST CONTINUING ALONG SAID NORTHERLY LINE, 89.98 FEET TO THE SOUTHWEST CORNER OF STREET LOT 8, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°35'21" EAST CONTINUING ALONG SAID NORTHERLY LINE, 70.00 FEET TO THE NORTHWEST CORNER OF LOT 8, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 00°08'00" WEST CONTINUING ALONG SAID NORTHERLY LINE, 423.70 FEET TO THE SOUTHWEST CORNER OF SAID LOT 8; THENCE SOUTH 89°18' EAST CONTINUING ALONG SAID NORTHERLY LINE, 307.20 FEET TO THE SOUTHEAST CORNER OF SAID LOT 8; ALSO BEING THE NORTHEAST CORNER OF SAID LOT 8; THENCE NORTH 00°01'11" WEST ALONG THE WESTERLY LINE OF SAID LOT 7, CENTRAL DES MOINES INDUSTRIAL PARK, 333.39 FEET; THENCE NORTHERLY CONTINUING ALONG SAID WESTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 604.30 FEET, WHOSE ARC LENGTH IS 93.82 FEET WESTERLY LINE, 15.78 FEET TO THE NORTHWEST CORNER OF SAID LOT 7 ALSO BEING THE SOUTH RIGHT OF WAY LINE OF TUTTLE STREET; THENCE NORTH 81°04'48" EAST ALONG THE NORTH LINE OF SAID LOT 7, A DISTANCE OF 411.78 FEET TO THE NORTHEAST CORNER OF SAID LOT 7; THENCE NORTH 00°05'48" WEST ALONG THE WEST LINE OF SAID LOT 8, CENTRAL DES MOINES INDUSTRIAL PARK, 72.11 FEET TO THE NORTHWEST CORNER OF SAID LOT 6; THENCE NORTH 81°52'47" EAST ALONG THE NORTH LINE OF SAID LOT 6, A DISTANCE OF 347.62 FEET TO THE SOUTHEAST CORNER OF LOT 4, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE NORTH 00°14'40" WEST ALONG THE WESTERLY LINE OF SAID OUTLOT Y, CENTRAL DES MOINES INDUSTRIAL PARK, 63.43 FEET; THENCE NORTH 87°28'10" EAST CONTINUING ALONG SAID WESTERLY LINE, 10.37 FEET; THENCE NORTH 00°12'49" WEST CONTINUING ALONG SAID WESTERLY LINE, 554.98 FEET TO THE NORTHEAST CORNER OF LOT 5, SAID CENTRAL DES MOINES INDUSTRIAL PARK AND HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHING 575907.16 AND EASTING 1604689.46; THENCE SOUTH 89°04'27" EAST ALONG THE NORTH LINE OF SAID OUTLOT Y, 16.36 FEET TO THE NORTHEAST CORNER OF SAID OUTLOT Y; THENCE NORTH 00°21'12" WEST ALONG THE WEST LINE OF SAID LOT 85, FACTORY ADDITION, 8.59 FEET TO THE SOUTHERLY RIGHT OF WAY LINE OF MARTIN LUTHER KING JR PARKWAY; THENCE NORTH 89°10'40" EAST ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 100.23 FEET; THENCE NORTH 84°40'04" EAST CONTINUING ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 184.44 FEET TO THE WESTERLY RIGHT OF WAY LINE OF SW 11TH STREET; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 173.21 FEET; THENCE SOUTH 74°42'59" WEST, 99.77 FEET; THENCE SOUTH 15°25'34" EAST, 75.29 FEET; THENCE NORTH 74°32'27" EAST, 99.61 FEET TO SAID WESTERLY RIGHT OF WAY LINE; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 87.08 FEET; THENCE SOUTH 78°27'19" WEST, 73.56 FEET TO THE WESTERLY RIGHT OF WAY LINE OF SAID LOT 81; THENCE SOUTHERLY EASTERLY LINE OF SAID LOT 81; THENCE SOUTH 38°14'07" WEST ALONG SAID EASTERLY LINE, 87.08 FEET; THENCE EASTERLY LINE OF SAID LOT 81; THENCE SOUTH 81°14'07" WEST ALONG THE WEST HALF OF SAID LOT 81; THENCE SOUTHERLY 00°14'00" EAST ALONG SAID EAST LINE, 787.72 FEET; THENCE SOUTH 45°38'19" EAST, 662.69 FEET TO THE WESTERLY 00°14'00" EAST ALONG SAID EAST LINE, 787.72 FEET; THENCE SOUTH 50.00 FEET TO SAID EASTERLY LINE OF THE WEST HALF OF LOT 81, HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHING 573317.97 AND EASTING 1605481.84; THENCE SOUTHERLY ALONG SAID EASTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 746.55 FEET, WHOSE ARC LENGTH IS 78.04 FEET AND WHOSE CHORD BEARS SOUTH 17°00'55" WEST, 78.01 FEET; THENCE NORTH 70°20'29" WEST, 50.00 FEET TO SAID WESTERLY LINE OF LOT 81; THENCE NORTH 45°38'19" WEST, 663.55 FEET TO SAID EAST LINE OF THE WEST 100 FEET OF LOT 81; THENCE SOUTH 00°14'00" EAST ALONG SAID EAST LINE, 438.00 FEET TO THE NORTH BANK AND MEANDER LINE OF THE RACCOON RIVER; THENCE NORTH 71°44'57" WEST AND ALONG SAID MEANDER LINE AND THE SOUTHERLY LINE OF SAID OUTLOT Z, CENTRAL DES MOINES INDUSTRIAL PARK, 882.50 FEET; THENCE NORTH 83°01'48" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 463.82 FEET; THENCE NORTH 89°10'53" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 291.24 FEET; THENCE SOUTH 82°00'06" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 298.82 FEET; THENCE SOUTH 72°02'02" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 228.32 FEET TO THE SOUTHWEST CORNER OF SAID OUTLOT Z; THENCE NORTH 00°01'07" EAST ALONG THE WEST LINE OF SAID CENTRAL DES MOINES INDUSTRIAL PARK, 1647.00 FEET TO THE POINT OF BEGINNING AND CONTAINING 53.73 ACRES (3,447,188 SQUARE FEET) AND IS SUBJECT TO ANY AND ALL EASEMENTS OF RECORD.

**NOTES**

1. THIS SITE IS SUBJECT TO THE TREE REMOVAL AND MITIGATION ORDINANCE OF THE CITY CODE. HUBBELL REALTY COMPANY IS ENTERING INTO A DEVELOPMENT AGREEMENT WITH THE CITY OF DES MOINES THAT OUTLINE A CUSTOM APPROACH TO TREE REMOVAL AND MITIGATION FOR THE GRAY'S STATION PLANNED UNIT DEVELOPMENT. NO TREE SURVEY OR CANOPY AREA METHOD INFORMATION WAS SUBMITTED FOR REVIEW; THIS INFORMATION WILL BE PROVIDED WITH THE FINAL DEVELOPMENT PLAN.
2. ALL REQUIRED STORM-WATER MANAGEMENT CAN BE ACHIEVED UNDER THE PROPOSED CONCEPT. (NO DESIGN SUBMITTED)
3. ANY DEVELOPMENT PLAN REVIEW IS SUBJECT TO PROVIDING A TRAFFIC IMPACT ANALYSIS REVIEWED BY THE CITY TRAFFIC ENGINEER.
4. THE SITE SHALL BE PLATTED TO PRESERVE PUBLIC TRANSPORTATION CORRIDORS IN GENERAL CONFORMANCE WITH THE MASTER PLAN.
5. ALL SANITARY SEWER CONNECTIONS ARE AVAILABLE TO SERVICE THE PROPOSED CONCEPT.
6. NO LESS THAN SEVENTY-FIVE PERCENT (75%) OF THE LOW-MEDIUM DENSITY RESIDENTIAL UNITS SHALL HAVE A MINIMUM OF 2 STORIES.
7. NO MORE THAN TWENTY-FIVE PERCENT (25%) OF THE LOW-MEDIUM DENSITY RESIDENTIAL UNITS SHALL HAVE A MINIMUM HEIGHT OF 1 STORY.
8. NO MORE THAN TWO 1-STORY LOW-MEDIUM DENSITY RESIDENTIAL UNITS SHALL BE CONSTRUCTED ON ADJOINING SITES.
9. THE 75%-25% MINIMUM HEIGHT RATIO FOR THE LOW-MEDIUM DENSITY RESIDENTIAL UNITS SHOULD BE ACHIEVED WITH EACH PHASE OF THE PUD PROJECT. SOME VARIATION CAN BE CONSIDERED WITH EACH DEVELOPMENT PLAN.
10. AN EMPHASIS ON TALLER BUILDING HEIGHTS SHALL BE PROVIDED FOR LOW-MEDIUM DENSITY RESIDENTIAL UNITS THAT FRONT PRIMARY STREETS.

ZONING OF ADJACENT PROPERTIES (1"=500')



**PUD CONCEPTUAL PLAN APPROVAL**

REVIEWED BY THE PLAN & ZONING COMMISSION ON JULY 6, 2017, AND JULY 20, 2017.

REVIEWED BY THE CITY COUNCIL ON JULY 24, 2017 (ROLL CALL NUMBER 17-1295), AND AUGUST 2, 2017 ROLL CALL NUMBER 17-1307).

APPROVED BY THE CITY COUNCIL ON AUGUST 14, 2017, BY ROLL CALL NUMBER 17-1407, ORDINANCE NO. 15,800.

IN ACCORDANCE WITH SECTION 134-895 OF THE DES MOINES CODE, AS AMENDED.

DATE \_\_\_\_\_ PLANNING DIRECTOR \_\_\_\_\_

I HEREBY CERTIFY THAT THIS ENGINEERING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF IOWA.

RYAN A. HARDESTY, P.E. DATE \_\_\_\_\_

MY LICENSE RENEWAL DATE IS DECEMBER 31, 2023  
PAGE # OF SHEETS COVERED BY THIS SEAL: \_\_\_\_\_  
SHEETS 1 & 6



4121 NW URBANDALE DRIVE  
 URBANDALE, IOWA 50322  
 PHONE: (515) 389-4400 FAX: (515) 969-4410  
 CIVIL DESIGN ADVANTAGE  
 DES MOINES, IOWA

PREPARED BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 ENGINEER: \_\_\_\_\_  
 TECH: \_\_\_\_\_

**GRAY'S STATION**  
 PLANNED UNIT DEVELOPMENT (COVER SHEET)

1/2  
 2202.1-13





# Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa Sheet 2B of 20



16. Sign, on-premise advertising means a sign which identifies or directs attention to:
- A prohibition, business, service, activity, product, category or situation manufactured, sold, offered or conducted upon the premises where such sign is located, and/or
  - The ideological or noncommercial views of an occupant of the premises.
17. Sign, on-premise or sign, on-site means a sign or signs at or adjacent to the site and displayed at an angle, which is not permanently anchored or secured.
18. Sign, on-site means a sign identifying business and other service facilities.
19. Sign, temporary means a sign intended for a limited period of display.
20. Sign, type A means a sign which shall conform to all aspects with the following:
- It shall have no more than two sign faces, such sign faces to be parallel.
  - Sign, type B means a sign which shall conform to all of the requirements for type A signs, except that type B signs may be internally illuminated.
21. Sign, type C includes type A and type B signs and shall conform to all aspects with the following:
- It may contain exposed wiring or lead pipe tubes or any combination thereof, provided that the exposed lamp does not exceed 11 watts or the lead pipe tube does not exceed 60 millibars. Where inside heated lamps or exposed lamps with a diffusing screen are used, no lamp shall exceed 22 watts.
  - Installation of combination by reduplicated lamps or by mirrored reflectors is prohibited.
22. Sign, window means a sign within a building which is located and placed so as to entitle an intent for the sign to be viewed from a public right-of-way.
- B. Downtown Mixed Use & High Density Residential Signs**
- Signs shall be in accordance with the following:
    - Any combination of two signs types A, type B or type C identification and/or advertising signs per occupant of a building not to exceed one and one-half square feet in area per linear foot of building facade per occupant, and not to exceed a maximum of 100 square feet per occupant, shall be permitted. Building facade consists of 25 percent or less of the total lot coverage, the amount or amount of that building shall be permitted. In the case above, one square foot in area per linear foot of lot coverage per lot and not to exceed a maximum of 100 square feet per occupant. Such signs shall be permitted on public or private concrete attached to and supported by a building.
    - All building or roof signs shall project no more than seven feet from the building, unless such projection is over the street right-of-way, in which case such sign shall project no more than 30 inches. Such projecting signs shall be no less than ten feet from the ground in areas in which there is only pedestrian traffic and not less than 15 feet from the ground in areas in which there is vehicular traffic. Signs shall be no more than 20 square feet in area per occupant and shall not extend more than four feet above the skyline. Projecting signs may be anchored to the roof or building.
    - The permitted types of free-standing signs are non-illuminated and sandwich board signs. Pole signs and portable signs other than sandwich board signs are not permitted.
    - One sandwich board sign shall be permitted per building front for identification and on-premise advertising. Such signs shall comply with ingress and egress requirements outlined in the city building code. Sandwich board signs are permitted upon the sidewalk within the public right-of-way, subject to the following:
      - Such signs are permitted only during the hours the business is open to the public, and must be located adjacent to the premises.
      - Such signs shall be placed no closer than two feet from the back of the street curb.
      - Such signs shall be no more than ten square feet in area on each side, no more than five feet in height, and no more than three feet in width.
      - There shall be no electrical service to such signs.
    - One non-illuminated building or wall identification sign per occupant, not to exceed five square feet in area, shall be permitted on buildings where occupant signs are occupied buildings, and shall be located immediately adjacent to the principal entrance to said occupant's premises.
    - Two on-premise type B illumination signs shall be permitted on building marquees. The total sign area per marquee shall not exceed 100 square feet.

17. Sign, on-premise advertising means a sign which identifies or directs attention to:
- A prohibition, business, service, activity, product, category or situation manufactured, sold, offered or conducted upon the premises where such sign is located, and/or
  - The ideological or noncommercial views of an occupant of the premises.
18. Sign, on-premise or sign, on-site means a sign or signs at or adjacent to the site and displayed at an angle, which is not permanently anchored or secured.
19. Sign, on-site means a sign identifying business and other service facilities.
20. Sign, temporary means a sign intended for a limited period of display.
21. Sign, type A means a sign which shall conform to all aspects with the following:
- It shall have no more than two sign faces, such sign faces to be parallel.
  - Sign, type B means a sign which shall conform to all of the requirements for type A signs, except that type B signs may be internally illuminated.
22. Sign, type C includes type A and type B signs and shall conform to all aspects with the following:
- It may contain exposed wiring or lead pipe tubes or any combination thereof, provided that the exposed lamp does not exceed 11 watts or the lead pipe tube does not exceed 60 millibars. Where inside heated lamps or exposed lamps with a diffusing screen are used, no lamp shall exceed 22 watts.
  - Installation of combination by reduplicated lamps or by mirrored reflectors is prohibited.
23. Sign, window means a sign within a building which is located and placed so as to entitle an intent for the sign to be viewed from a public right-of-way.
- B. Downtown Mixed Use & High Density Residential Signs**
- Signs shall be in accordance with the following:
    - Any combination of two signs types A, type B or type C identification and/or advertising signs per occupant of a building not to exceed one and one-half square feet in area per linear foot of building facade per occupant, and not to exceed a maximum of 100 square feet per occupant, shall be permitted. Building facade consists of 25 percent or less of the total lot coverage, the amount or amount of that building shall be permitted. In the case above, one square foot in area per linear foot of lot coverage per lot and not to exceed a maximum of 100 square feet per occupant. Such signs shall be permitted on public or private concrete attached to and supported by a building.
    - All building or roof signs shall project no more than seven feet from the building, unless such projection is over the street right-of-way, in which case such sign shall project no more than 30 inches. Such projecting signs shall be no less than ten feet from the ground in areas in which there is only pedestrian traffic and not less than 15 feet from the ground in areas in which there is vehicular traffic. Signs shall be no more than 20 square feet in area per occupant and shall not extend more than four feet above the skyline. Projecting signs may be anchored to the roof or building.
    - The permitted types of free-standing signs are non-illuminated and sandwich board signs. Pole signs and portable signs other than sandwich board signs are not permitted.
    - One sandwich board sign shall be permitted per building front for identification and on-premise advertising. Such signs shall comply with ingress and egress requirements outlined in the city building code. Sandwich board signs are permitted upon the sidewalk within the public right-of-way, subject to the following:
      - Such signs are permitted only during the hours the business is open to the public, and must be located adjacent to the premises.
      - Such signs shall be placed no closer than two feet from the back of the street curb.
      - Such signs shall be no more than ten square feet in area on each side, no more than five feet in height, and no more than three feet in width.
      - There shall be no electrical service to such signs.
    - One non-illuminated building or wall identification sign per occupant, not to exceed five square feet in area, shall be permitted on buildings where occupant signs are occupied buildings, and shall be located immediately adjacent to the principal entrance to said occupant's premises.
    - Two on-premise type B illumination signs shall be permitted on building marquees. The total sign area per marquee shall not exceed 100 square feet.
- C. Median Density Residential Signs**
- Signs shall be in accordance with the following:
    - One on-premise type A identification sign not to exceed one square foot in area shall be permitted for each of the following uses: Single-family detached dwellings, unimproved group residences, and family homes.
    - One on-premise type B identification sign not to exceed 24 square feet in area per sign face shall be permitted for each of the following uses: Churches, synagogues and temples, and similar places of worship, public and parochial schools (including boarding schools, nursery schools, and child care centers), city-owned fire stations, and publicly owned parks, playgrounds, golf courses, and recreation areas.
    - One traditional bulletin board, individually or internally illuminated, not to exceed 24 square feet in area, shall be permitted per individual. The sign shall extend higher than eight feet above ground level.
    - One on-premise type B identification sign not to exceed one square foot in area shall be permitted for each of the two dwelling units of a two-family residence.
    - One on-premise type B identification and/or advertising sign per lot building, not to exceed four square feet in area per sign face, shall be permitted for each multiple dwelling.
    - In addition to the signage permitted by subsections a) through d) of this section, a home occupation may be permitted one additional square foot of signage.
    - In lieu of the identification sign and the traditional bulletin board, a church or school may combine into one sign type A or type B identification sign and an illuminated bulletin board. This sign may be internally or internally illuminated. It shall not exceed 24 square feet per sign face nor extend more than eight feet above the ground. It shall not be located in any required view or side yard.
- VI. Parking Requirements**
- A. Residential Areas**
- Surface parking spaces shall not exceed 3.75 spaces per 1000 GSF of commercial area.
  - Surface parking shall have an emphasis of trees, shrubs and other plantings placed around the perimeter of any parking area and within the parking lot to create a more attractive area. The landscape plan should generally enhance the visual appearance of the building, parking area, and any pedestrian areas. Details of surface parking landscape enhancements to be reviewed by the Planning & Zoning Commission and/or City Council.
  - Vehicular use areas with 25 or more car stalls require interior landscape area as detailed below:
    - Interior landscaping shall be a minimum amount of one planting bed no smaller than 8' x 17' for every 25 interior spaces containing 1 tree/shrub tree and 2 shrubs.
    - Required landscaping and planting beds may be clustered but there may be no more than 25 spaces in a single row, or 30 spaces in a double row without an intervening planting bed.
    - Trees planted in Parking lot Interior Plantings are to be a minimum of 1" 1/2' tall.
    - Use of existing trees is required to increase shade and reduce "urban heat island" effect.
    - A minimum of one (1) tree/shrub tree and two (2) shrubs for every twenty-five (25) spaces exclusive of tree planting required by perimeter landscape area.
    - Center islands require that 7' perimeter landscape sign may be located toward interior planting requirements even though they are contiguous with perimeter landscape areas.
    - The 7' perimeter landscape area may not be located toward interior planting requirements.
    - Perimeter landscape areas in excess of 7' may partially be located toward interior planting requirements.
    - Interior planting islands that are at least 17' x 17' should be spaced no farther than 20 parking spaces or 100' apart.
  - Vehicular use areas require perimeter landscape area as detailed below:
    - Vehicular use areas holding up to 30 can require a 7' perimeter landscape area (7' car stacking, 7' landscape area) measured from the right-of-way line along parking lot facing public right-of-way. (Public land cannot be located toward sidewalk of the sidewalk).

- Vehicular use areas of 40 car stalls and over require a 7' perimeter landscape area (7' car stacking, 7' landscape area) measured from the right-of-way line along parking lot facing public right-of-way. (Public land cannot be located toward sidewalk of the sidewalk).
  - Barrier curbs of concrete or stone shall be a minimum of 12" in height and a minimum of 12" in width, and shall be securely installed around the perimeter landscape area and be maintained in good condition.
  - Soil shall be placed the wide width of perimeter landscape area, including the car stacking area.
  - The landscape area shall contain 1 tree and 10 shrubs (24" DBH) maximum mature residential height, and be no less than 18" tall per 40 linear feet of perimeter landscape area with groundcover or a decorative well or lawn made of grass, brick, cobblestone concrete block, pavement (cannot include gravel paths, gravel pits, or a combination of these materials, located at the lot line. Wood is not allowed except for an in-lot matured. Chain link fences are not allowed. Walls shall be no taller than 30", and no shorter than 24".
  - Groundcover or the planting required for the car stacking area where matured will be permitted. Christmas trees, or burlwood divided mulch, is required.
  - Plantings are recommended every 100' throughout the perimeter landscape area.
  - Trees shall be planted a minimum of 30' from any curb.
  - Unequal tree spacing is acceptable with groupings of lighter spacing and larger spacings between groupings.
  - The spacing or offsetting of perimeter area trees with parking trees is encouraged.
  - Trees planted in the perimeter landscape area are a separate requirement, and do not count toward other the parking planting or the parking lot interior plantings.
- E. Parking lots adjacent to public streets shall be screened by walls, fences, hedges, landscaping or a combination thereof. Unimproved sections shall be landscaped to meet the Median Landscape Standards for zoning districts C-1, C-2, C-3, Residential Areas**
- B. Streets Designated as "Local" or "Collector" shall include parallel parking as depicted in the approved conceptual development plan.**
- C. Streetwide Parking**
- Function of above grade structured parking (i.e., surface parking beneath commercial or residential uses) adjacent to any public right-of-way shall be architecturally and aesthetically coordinated with the remainder of the building they support.
- VI. Mass Transit Integration**
- Development shall accommodate long term transit connections within and between neighborhoods.
  - The Des Moines Area Regional Transit (DART) Authority shall review all development plans and designate locations for transit stops and shelters.
  - Developers and DART shall work together to establish feasible transportation linkages on a case by case basis.
- VI. Public Amenities**
- A. Bicycle and Pedestrian Facilities:**
- Development shall provide on-street bike lanes and off-street trails that connect to existing urban system.
  - Development will include a Pedestrian Bridge connecting Gray's Station to the Racoon River.
  - Pedestrian bridge is anticipated to be located near the southeast corner of the identified PUD boundary and west of the existing parking system associated with the reference stadium area.
  - Development shall provide the inclusion of bicycle sharing services (e.g., B-Cycle).
  - Stairways along streets, in common areas, and between buildings shall maintain a minimum width of 5 feet.
- B. Public Open Space**
- Greenways
    - Greenways shall maintain an approximate width of 40' to 100'. Exceptions may occur where lot/structure requirements necessitate and will be reviewed with the Final Development Plan.
    - Greenways shall include landscaping.
    - Greenways shall include pedestrian pathways with a minimum width of 7'.
    - Greenways may include structural (e.g., Pergola, Chain Structures, etc.) and/or artistic elements where deemed appropriate by the developer and approved in the Final Development Plan.
  - Neighborhood Parks
    - Neighborhood Parks shall be generally located per the approved conceptual development plan.
    - Neighborhood Parks shall include landscaping.
    - Neighborhood Parks shall include pedestrian pathways with a minimum width of 7'.
    - Neighborhood Parks may include structural (e.g., Shelter, Chainways, Pergola, etc.) and/or artistic elements where deemed appropriate by the developer.
  - Pedestrian Bridge Landing Plaza
    - The Pedestrian Bridge Landing Plaza shall generally be located as depicted on the approved conceptual development plan.
    - The Plaza shall be designed to accommodate pedestrian and bicycle traffic associated with daily use of the Pedestrian Bridge facility.
    - The Plaza shall be designed as an inviting public space that is flexible enough to accommodate neighborhood events.
  - Enhanced Stormwater Basin
    - The Enhanced Stormwater Basin (ESB) shall generally be located as depicted on the approved conceptual development plan on Street 15.
    - The ESB shall be designed to detain stormwater from the watershed which it serves.
    - The ESB shall be designed to improve the quality of stormwater that flows through it.
    - The ESB shall be designed to be architecturally pleasing.
    - The ESB shall be designed by the developer. The ESB design shall be approved by the City of Des Moines. ESB shall be constructed and owned by the City of Des Moines.

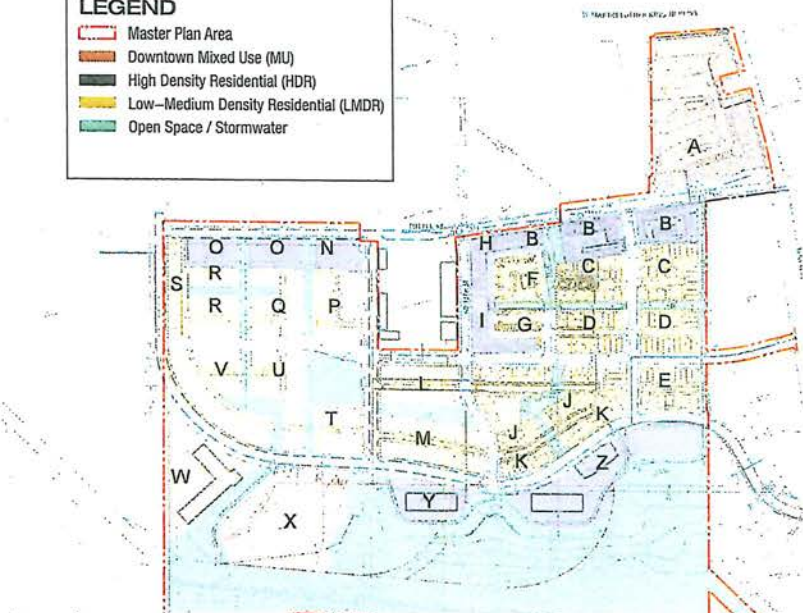
# Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa

## Sheet 3 of 20



**LEGEND**

- Master Plan Area
- Downtown Mixed Use (MU)
- High Density Residential (HDR)
- Low-Medium Density Residential (LMDR)
- Open Space / Stormwater



Principal Uses	Plan Subarea																							
	MU	HDR	MDR	MDR	MDR	MDR	MDR	HDR	MDR	MDR	MDR	MDR	MDR	HDR	MDR	MDR	MDR	MDR	MDR	MDR	MDR	MU	HDR	HDR
<b>Residential Use Category</b>																								
1-unit dwelling		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
2-unit dwelling		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
3-unit dwelling		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Multi-unit dwelling (4+) units	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Efficiency/Micro Units	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Bed & Breakfast	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Group Living	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Group Living for Health Reasons	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Hotel/Apartment Hotel	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Residential Care, Large	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Residential Care, Small	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
<b>Civic &amp; Institutional Category</b>																								
Assembly, Neighborhood (<10,000 sf)	X	X						X														X	X	X
Assembly, General (>10,000 sf)	X	X						X														X	X	X
Government/Higher Education/Hospital	X	X						X														X	X	X
Library/Museum	X	X						X														X	X	X
Police/Fire	X	X						X														X	X	X
School: Pre-K, elementary, Intermediate	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Transit Station	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
<b>Open Space Category</b>																								
Community Garden	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Intensive Park Uses	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Park	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Urban Farm	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
<b>Retail Use Category</b>																								
Neighborhood Retail	X	X						X														X	X	X
General Retail	X	X						X														X	X	X
Beer/Wine/Liquor Sales	X	X						X														X	X	X
Convenience Store	X	X						X														X	X	X
<b>Service Use Category</b>																								
Neighborhood Service	X	X						X														X	X	X
General Service	X	X						X														X	X	X
Adult Day Care	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Child Day Care (>12 children)	X	X						X														X	X	X
Community Service	X	X						X														X	X	X
Drinking Places	X	X						X														X	X	X
Eating Places	X	X						X														X	X	X
Entertainment Assembly	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Private Club	X	X						X														X	X	X
Tattoo/Piercing Parlor	X	X						X														X	X	X
<b>Employment Use Category</b>																								
Office	X	X						X														X	X	X
Craftsman/Studio	X	X						X														X	X	X

X Permitted  
All uses not listed on this sheet are prohibited.

Accessory & Temporary Uses	Plan Subarea																							
	MU	HDR	MDR	MDR	MDR	MDR	MDR	HDR	MDR	MDR	MDR	MDR	MDR	HDR	MDR	MDR	MDR	MDR	MDR	MDR	MU	HDR	HDR	
<b>Residential Accessory Category</b>																								
Accessory Dwelling Unit			X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Home Child Care			X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Home Occupation	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Room Rental - Long-Term (> 3 months)	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Room Rental - Short-Term (< 21 days)	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
<b>Commercial Accessory Category</b>																								
Drive-Through Facility	X	X																						
Outdoor Cafe	X	X				X								X	X							X	X	X
Outdoor Display	X	X				X								X	X							X	X	X
Parking	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
<b>Temporary Use Category</b>																								
Volting Place	X	X												X	X							X	X	X
Temporary Outdoor Event	X	X												X	X							X	X	X
Farmers Market	X	X												X	X							X	X	X

X Permitted  
All uses not listed on this sheet are prohibited.

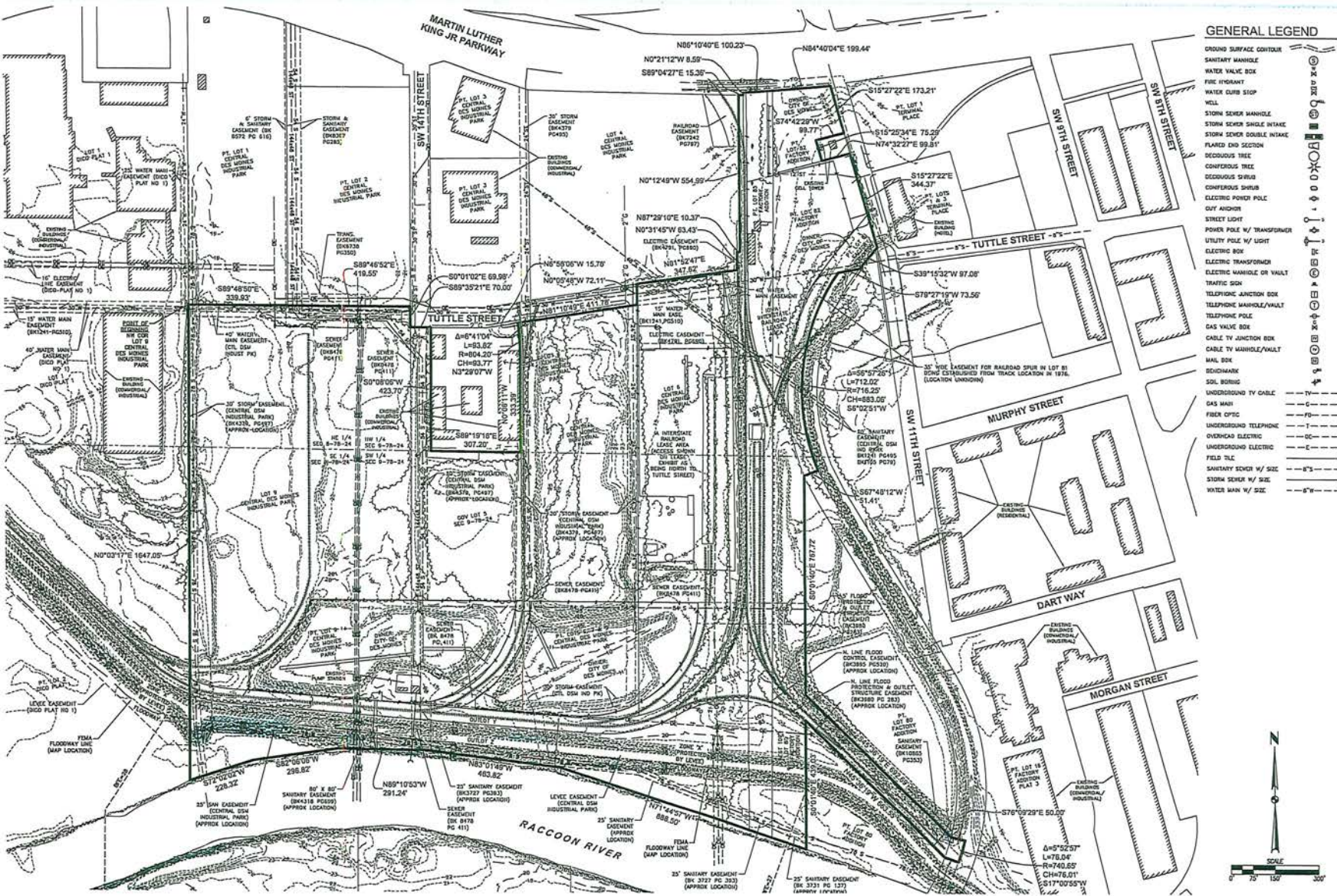
PERMITTED LAND USES  
OCTOBER 19, 2022



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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GRAY'S STATION



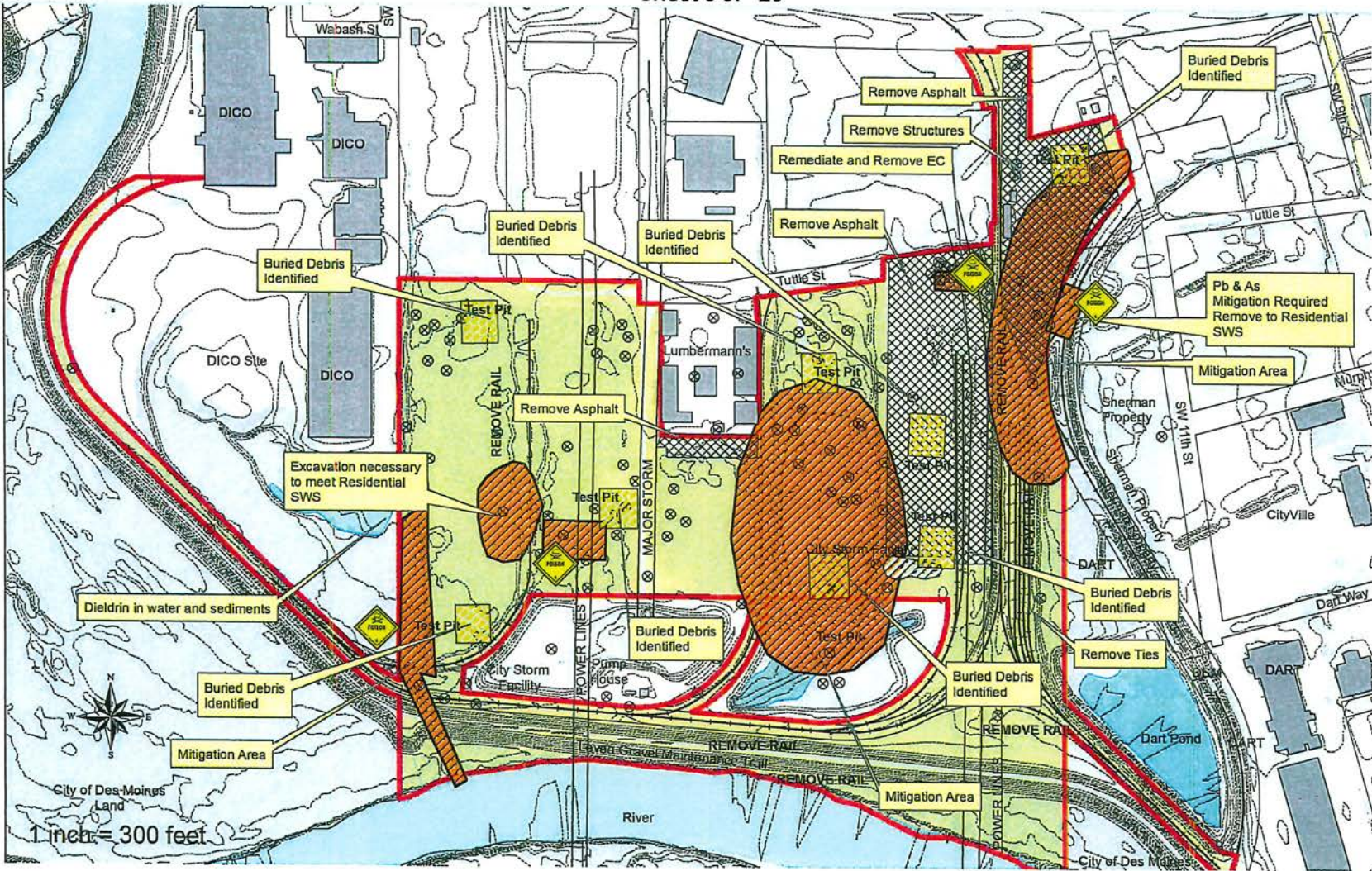
**GENERAL LEGEND**

- GROUND SURFACE CONTOUR
- SANITARY MANHOLE
- WATER VALVE BOX
- FIRE HYDRANT
- WATER CURB STOP
- WELL
- STORM SEWER MANHOLE
- STORM SEWER SINGLE INTAKE
- STORM SEWER DOUBLE INTAKE
- PLANNED END SECTION
- DECIDUOUS TREE
- CONIFEROUS TREE
- DECIDUOUS SHRUB
- CONIFEROUS SHRUB
- ELECTRIC POWER POLE
- GUY AND/OR
- STREET LIGHT
- POWER POLE W/ TRANSFORMER
- UTILITY POLE W/ LIGHT
- ELECTRIC BOX
- ELECTRIC TRANSFORMER
- ELECTRIC MANHOLE OR VAULT
- TRAFFIC SIGN
- TELEPHONE JUNCTION BOX
- TELEPHONE MANHOLE/VAULT
- TELEPHONE POLE
- GAS VALVE BOX
- CABLE TV JUNCTION BOX
- CABLE TV MANHOLE/VAULT
- MAIL BOX
- SEWERSHANK
- SOIL BORING
- UNDERGROUND TV CABLE
- GAS MAIN
- FIBER OPTIC
- UNDERGROUND TELEPHONE
- OVERHEAD ELECTRIC
- UNDERGROUND ELECTRIC
- FIELD TILE
- SANITARY SEWER W/ SIZE
- STORM SEWER W/ SIZE
- WATER MAIN W/ SIZE

PLAN MAP:  
 EXISTING CONDITIONS  
 MAY 3, 2017



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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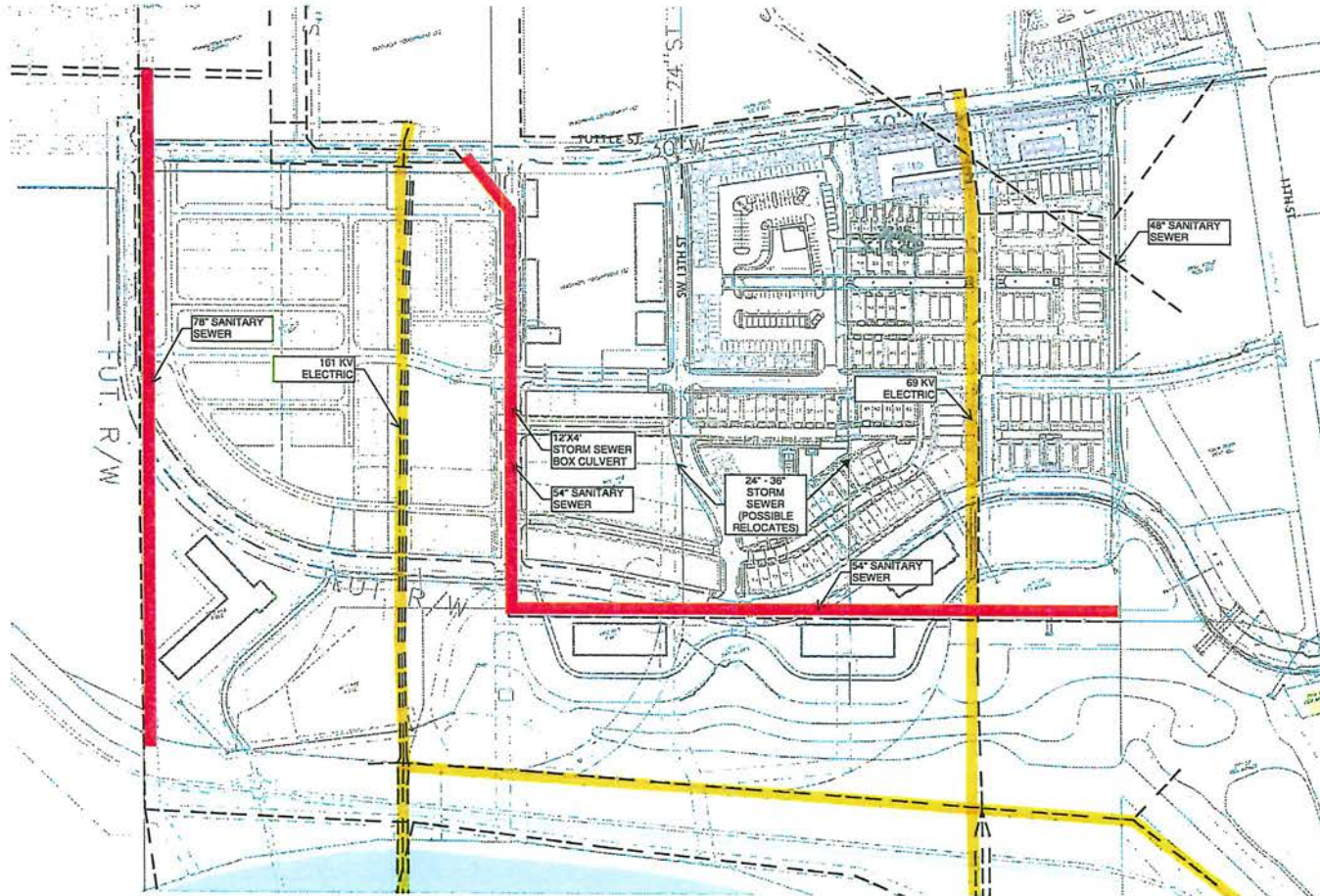


NOTES:  
 1. Refer to supplemental digital content provided to the City of Des Moines for additional environmental conditions findings.

ENVIRONMENTAL CONDITIONS  
 OCTOBER 19, 2022



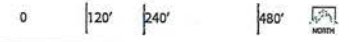
Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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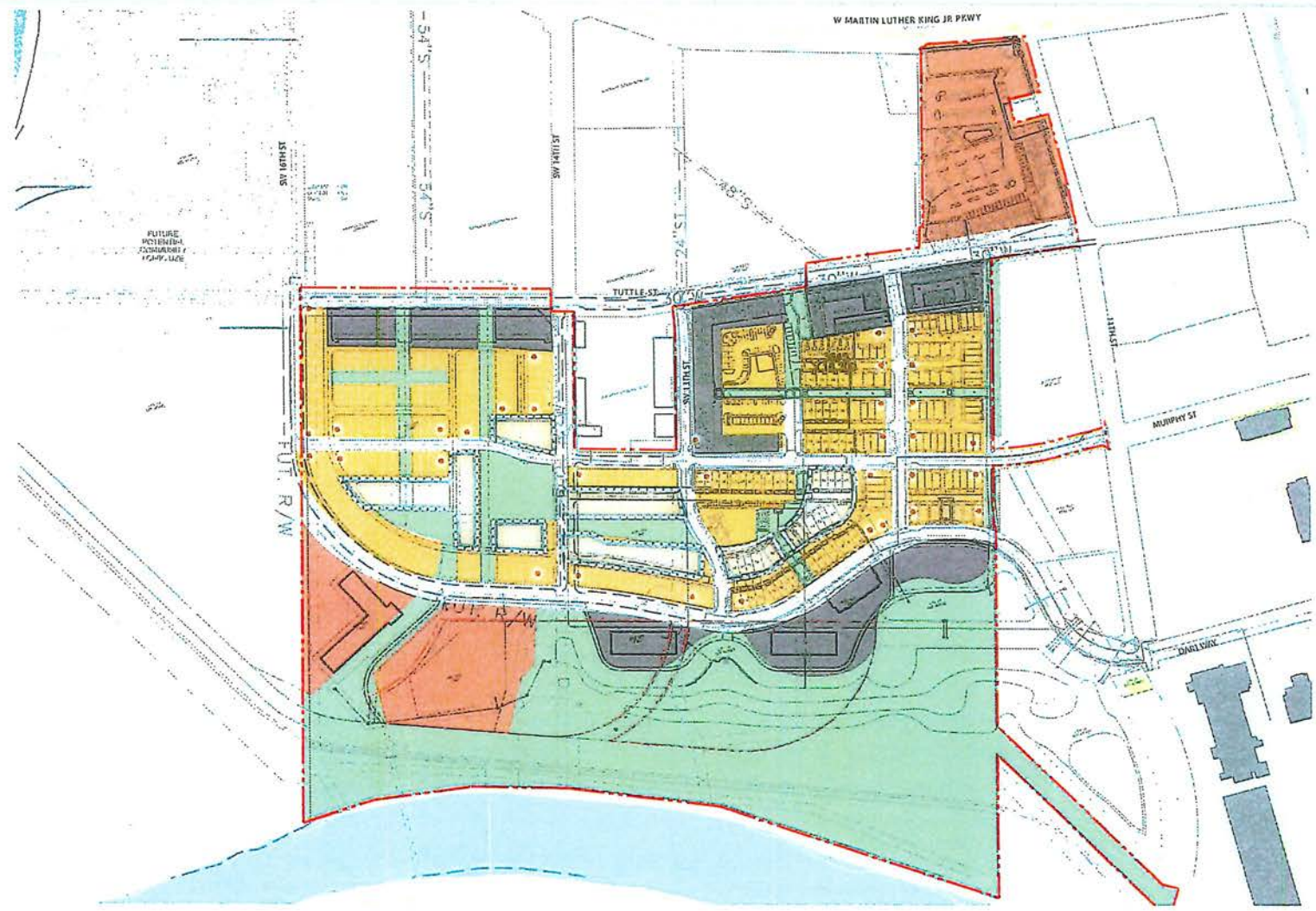
**LEGEND:**  
 EXISTING SANITARY LINE TO REMAIN  
 EXISTING OVERHEAD POWER LINES TO REMAIN

**NOTES:**  
 1. The conceptual plan takes into account significant existing infrastructure features that are currently infeasible to move based on economic factors. Unless economic conditions change, future development plans should respect and sensitively integrate the existing infrastructure highlighted in the adjacent diagram and attached legend.

EXISTING INFRASTRUCTURE CONSTRAINTS  
 OCTOBER 19, 2022



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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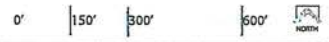


**NOTES:**  
 1. The master plan at left is for illustrative purposes only and is provided to show general, conceptual intent and character of the development. Individual parcels, building footprints, landscape design, open space design and parking design will be determined and designed as a part of future development plans for each phase.

**LEGEND**

- Master Plan Area
- Downtown Mixed Use (MU)
- High Density Residential (HDR)
- Low-Medium Density Residential (LMDR)
- Open Space / Stormwater
- 2 story minimum building height at corner lots where indicated with orange circle
- Detached Urban Townhomes Subarea (Other uses permitted as per the Permitted Land Uses Chart)

CONCEPTUAL ILLUSTRATIVE MASTER PLAN  
 OCTOBER 19, 2022





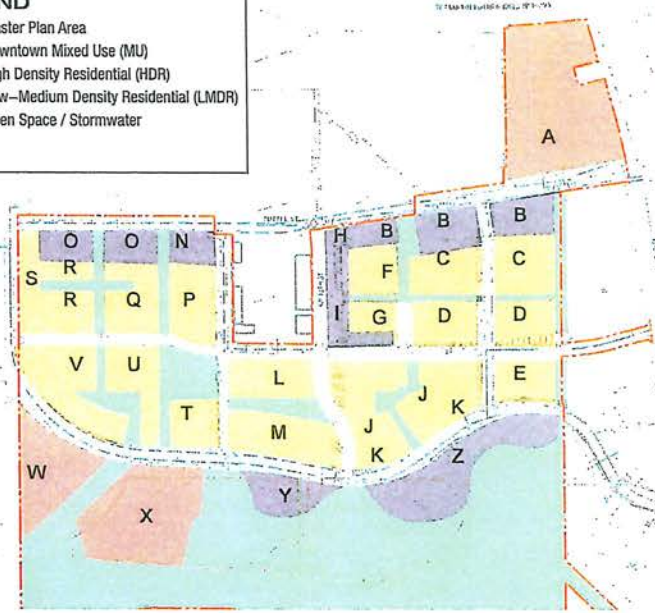
Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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GRAY'S STATION

**LEGEND**

- Master Plan Area
- Downtown Mixed Use (MU)
- High Density Residential (HDR)
- Low-Medium Density Residential (LMDR)
- Open Space / Stormwater



**Hubbell Gray's Station**  
Master Plan Concept  
Conceptual Land Use Summary & Estimated Residential Units

AREA NAME	LAND USE	ACRES	MINIMUM DENSITY (DU/AD)	MAXIMUM DENSITY (DU/AD)
A	Mixed Use	5.3	30	50
B	HDR	1.9	30	50
C	MDR	2.0	8	35
D	MDR	1.9	8	35
E	MDR	1.0	8	35
F	MDR	0.7	8	35
G	MDR	0.7	8	35
H	HDR	0.1	18	30
I	MDR	0.8	8	35
J	MDR	1.2	7	22
K	MDR	1.7	7	22
L	MDR	1.6	8	35
M	MDR	1.6	8	35
N	HDR	0.4	18	30
O	HDR	1.2	40	55
P	MDR	1.0	8	35
Q	MDR	1.2	8	35
R	MDR	1.1	8	35
S	MDR	0.7	8	35
T	MDR	0.7	8	35
U	MDR	1.1	8	35
V	MDR	2.0	8	35
W	Mixed Use	2.0	30	50
X	Mixed Use	3.5	30	50
Y	HDR	1.0	50	70
Z	HDR	2.7	50	70
<b>TOTAL</b>		<b>39.2</b>		

**Illustrative Estimate**

Illustrated Units	Illustrated Net Density	Assumed Multi-family residential floors	City Classification
193	37	4	High
79	41	3	High
34	17		High
34	18		High
16	16		Med
10	14		Med
11	16		Med
5	35	3	High
12	16		Med
13	11		Low-Med
22	13		Med
32	19		High
32	20		High
15	33	3	High
65	54	3	High
22	22		High
22	18		High
24	21		High
16	22		High
10	14		Med
18	16		Med
38	19		High
85	42	4	High
142	41	4	High
55	54	5	High
148	55	5	High
<b>TOTAL</b>	<b>1151</b>	<b>293</b>	

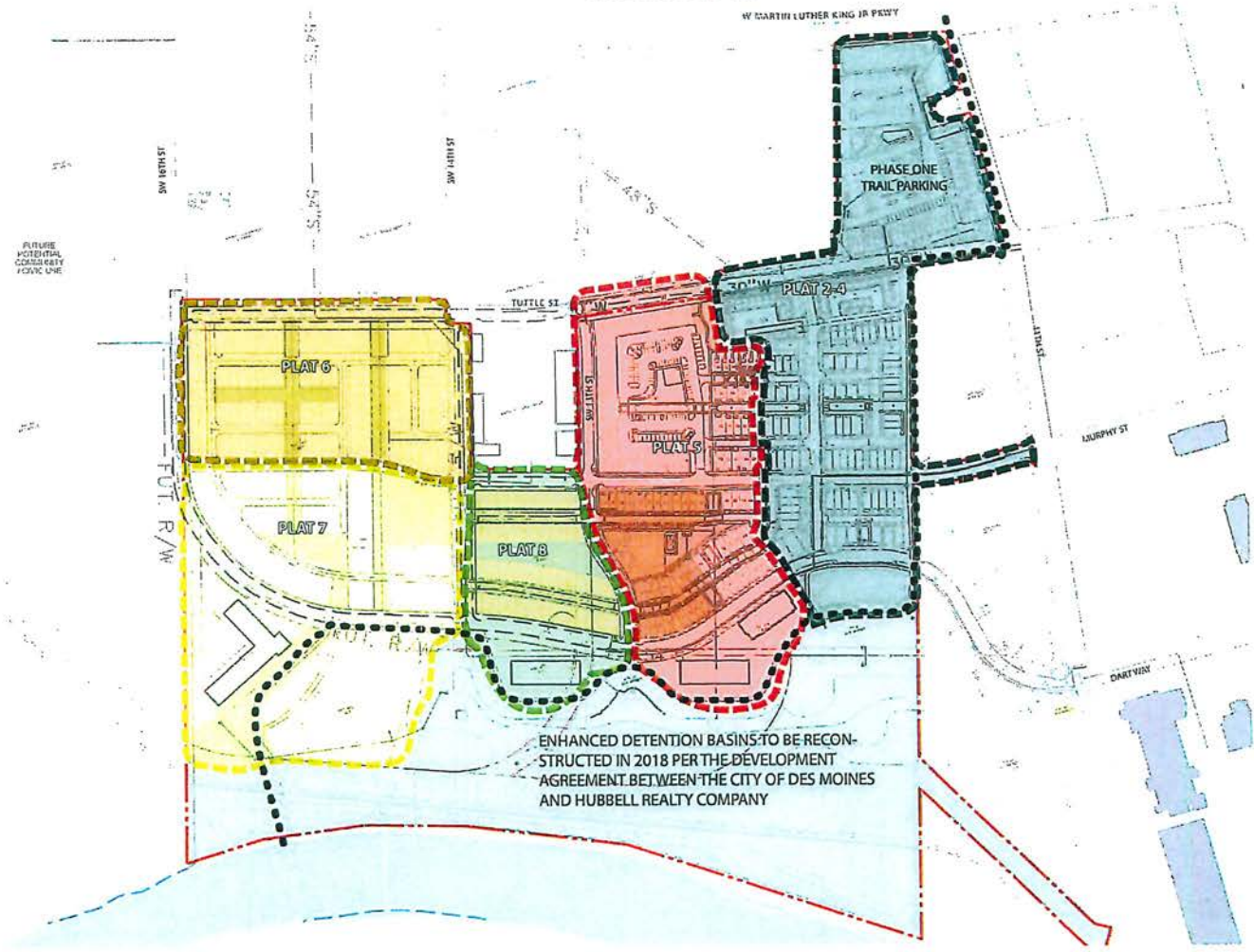
Gray's Station will achieve a minimum of 1,100 dwelling units

**MINIMUM NET DENSITY (EXCLUDING SIGNIFICANT OPEN SPACE, ROADS) 28**

**Notes:** 1. The assumed number of multi-family units is based on an average 1000 sf dwelling unit.



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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ENHANCED DETENTION BASINS TO BE RECONSTRUCTED IN 2018 PER THE DEVELOPMENT AGREEMENT BETWEEN THE CITY OF DES MOINES AND HUBBELL REALTY COMPANY

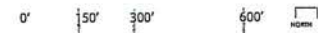
NOTES:  
 Variables that will impact the anticipated phasing of the Gray's Station plan include the timing of:  
 1) Redevelopment of the DICO site, and  
 2) Infrastructure, improvements and street extensions completed by the City of Des Moines.

**LEGEND**

- Master Plan Area
- Phase One Trail - Conceptual Alignment

Anticipated Phasing:

- Plat 2-4
- Plat 5
- Plat 8
- Plat 6
- Plat 7



CONCEPTUAL PHASING DIAGRAM  
 OCTOBER 19, 2022

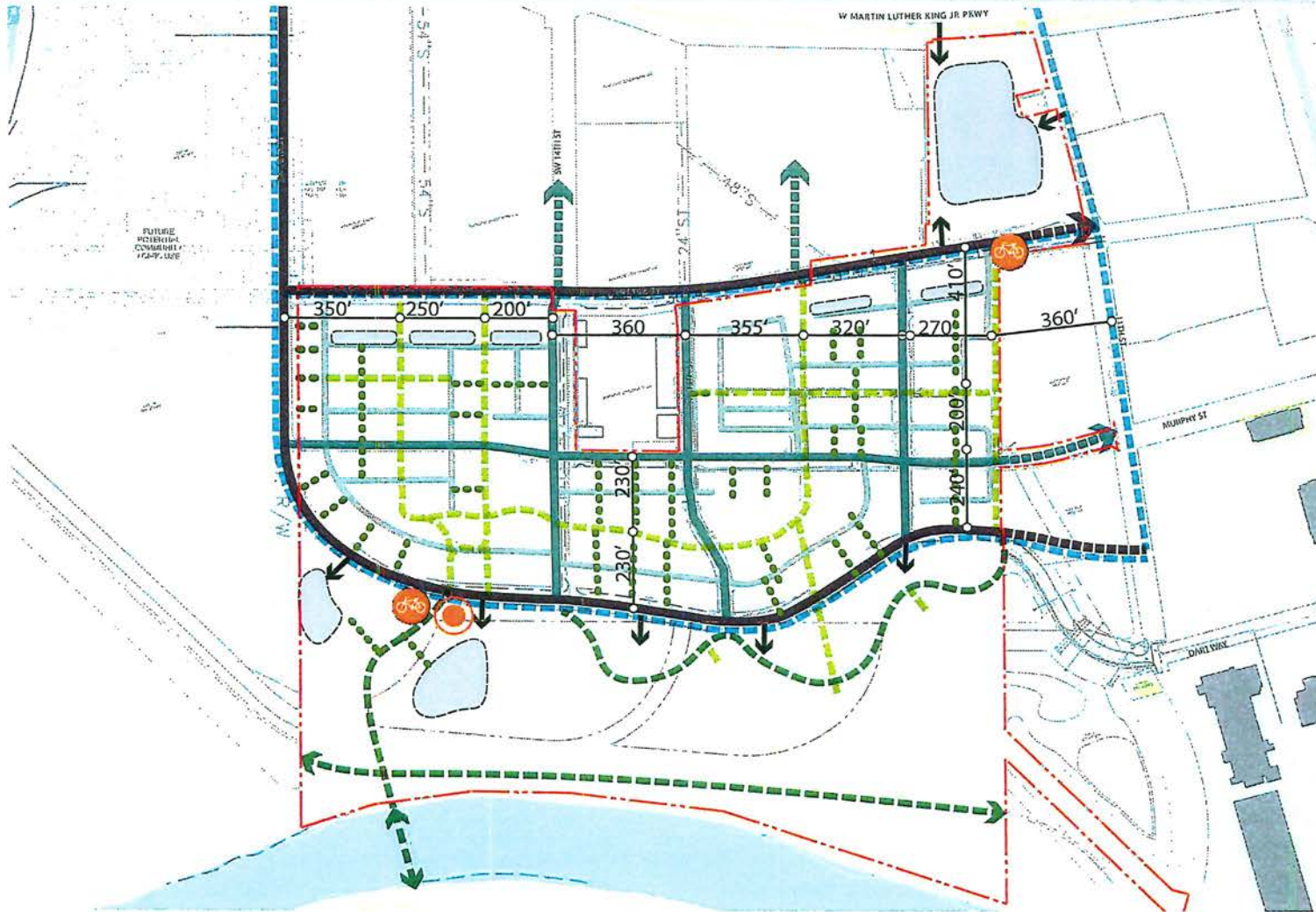
Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
Sheet 11 of 20



GRAY'S STATION

NOTES:

- The location of the following items are approximate and provided to show general plan intent of the circulation network:
  - Block lengths between streets and greenways;
  - Greenway locations;
  - Road centerlines;
  - Alley network;
  - Access drive location;
  - Bus stop locations;
  - Bike share locations;
  - Off-street parking areas.
- The proposed circulation network outside of the master plan area is shown for illustrative purposes and to demonstrate the development's intent to tie into future improvements outside the scope of this master plan.
- The Conceptual Plan was completed while the City's Transportation Master Plan (MOVE DSM) was under development. The intent of this Conceptual Plan is to comply with the MOVE DSM plan based on the preliminary goals that have been publicly presented such as:
  - Support other City goals of safety, quality of life, and economic development.
  - Ensure that Des Moines is best positioned to provide transportation options for all citizens in every stage of life.



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
 Sheet 12 of 20



GRAY'S STATION

CONCEPTUAL STREET SECTION: SOUTHERN ROAD/16TH STREET (COLLECTOR STREET)



CONCEPTUAL STREET SECTIONS: TUTTLE STREET - CYCLE TRACK (COLLECTOR STREET)



CONCEPTUAL ALLEY SECTION



CONCEPTUAL STREET SECTION: LOCAL STREET AT 14TH STREET



CONCEPTUAL STREET SECTION: LOCAL STREETS (EXCLUDING 14TH STREET)

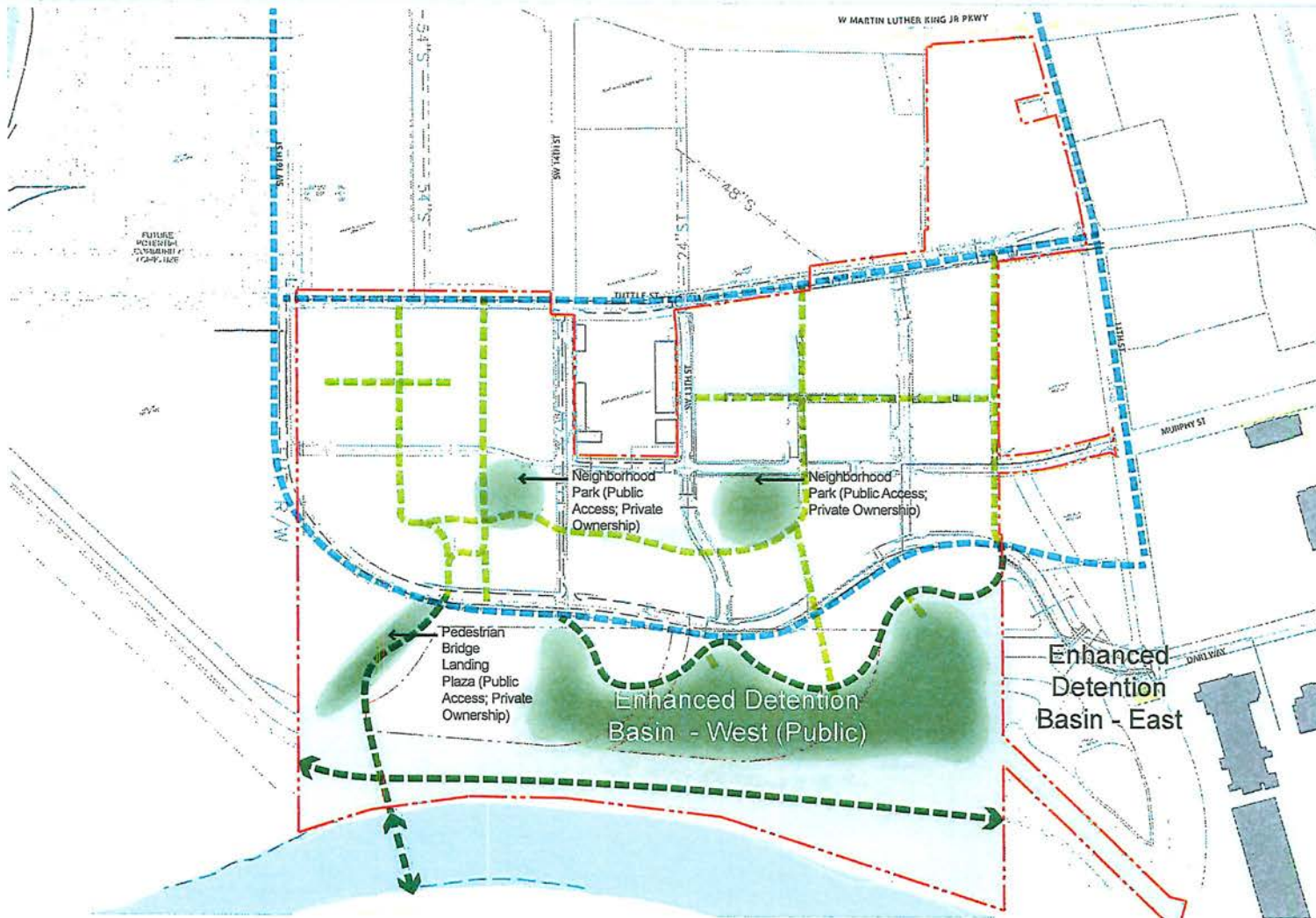


NOTE: BUILDINGS SHOWN IN ALL SECTIONS FOR GENERAL MASSING ONLY. ARCHITECTURAL DESIGN IS YET TO BE DETERMINED. FINAL DEVELOPMENT PLANS TO BE REVIEWED AND

CONCEPTUAL STREET AND ALLEY SECTIONS  
 OCTOBER 19, 2022



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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**LEGEND**

- Master Plan Area
- Proposed Public or Public Access Area
- Proposed Bicycle/Pedestrian Route – Primary
- Proposed Greenways
- Proposed On-Street Bike Facility
- Existing Regional Trail
- Existing Local Trail

NOTE: See Sheet 2A for land dedication and maintenance plans for open space areas.

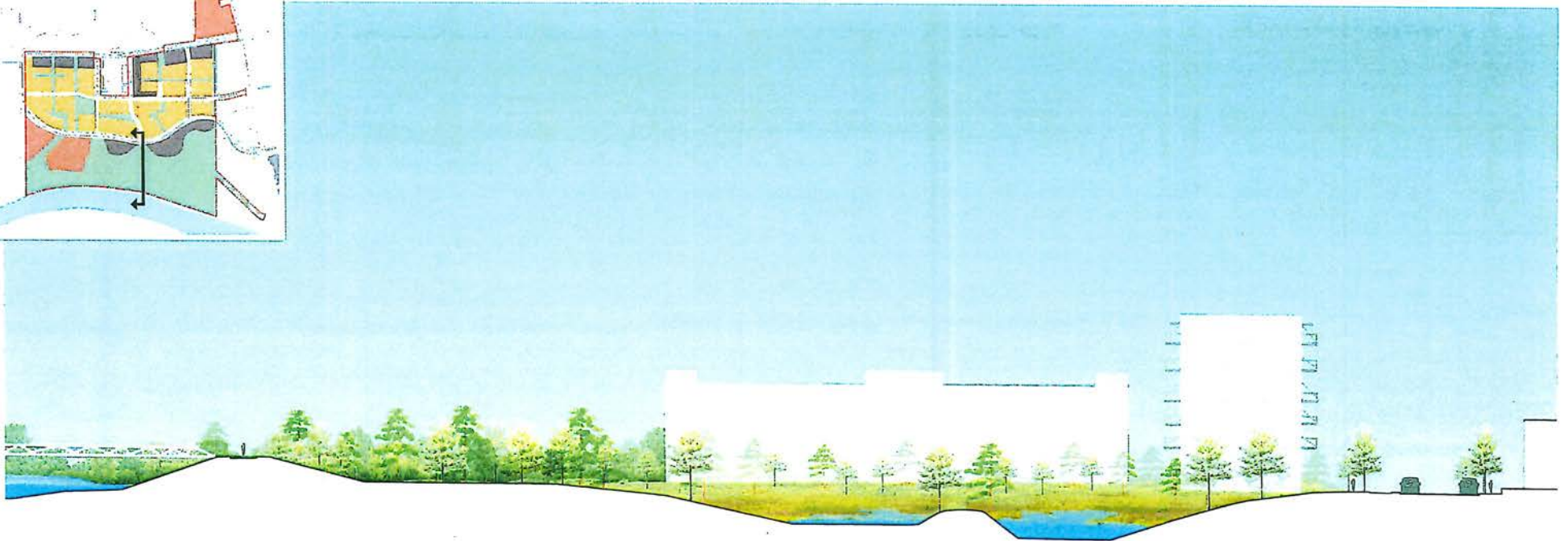
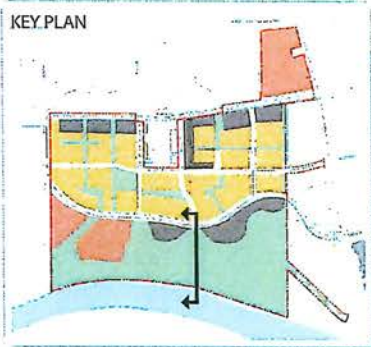
CONCEPTUAL OPEN SPACE PLAN  
 OCTOBER 19, 2022



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
Sheet 14 of 20



GRAY'S STATION



NOTE: BUILDINGS SHOWN FOR GENERAL MASSING ONLY. ARCHITECTURAL DESIGN IS YET TO BE DETERMINED. SUBJECT TO INDIVIDUAL FINAL DEVELOPMENT PLANS TO BE REVIEWED AND APPROVED BY THE PLANNING & ZONING COMMISSION AND CITY COUNCIL.

CONCEPTUAL SECTION ELEVATION THROUGH ENHANCED DETENTION BASIN  
OCTOBER 19, 2022



# Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa

## Sheet 15 of 20



GRAY'S STATION

### OPEN SPACE CHARACTER

The open space system at Gray's Station is intended to support a diverse, urban population with a focus on wellness; opportunities for social and physical connections; and areas for connections to nature and solitude.

### PEDESTRIAN BRIDGE LANDING PLAZA

The following images and conceptual programming represent the intended character and feel of the plaza area anticipated in the Gray's Station neighborhood, but detailed programming and design of the park systems will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Gathering • People-Watching • Outdoor Dining • Festival Space • Bike Connectivity



### NEIGHBORHOOD PARKS

The following images and conceptual programming represent the intended character and feel of the neighborhood parks anticipated in the Gray's Station neighborhood, but detailed programming and design of the park systems will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Dog Walking • Playing • Learning • Picnicking • Gathering • Community Gardening • Community Space



### GREENWAYS

The following images and conceptual programming represent the intended character and feel of the greenways anticipated in the Gray's Station neighborhood, but detailed programming and design of the park systems will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Connecting • Dog Walking • Playing • Biking • Neighborhood Gatherings



### ENHANCED STORMWATER DETENTION BASINS

The following images and conceptual programming represent the intended character and feel of the enhanced regional stormwater detention basins anticipated in the Gray's Station neighborhood, but detailed programming and design of the park systems will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Restoring • Connecting • Learning • Unpaved Trails • Signage • Seating

The proposed enhanced detention basins will provide an attractive, educational water feature with public access and improved water quality within the Raccoon River. It is proposed to have sufficient volume to meet the water quality volume (WQv) requirements for the proposed Gray's Station development and have enough volume to address the 100-year storm event for the upstream watershed areas under full buildout conditions.

- The East Detention Basin area, formerly known as DART pond, is off-site, but is being planned for improvements concurrently with this redevelopment area. WQv requirements for areas within the proposed development will be met through permanent pool storage in the pool and marsh zones of the basin.
- The West Detention Basin area has sufficient volume to meet WQv requirements for areas within the proposed development through permanent pool storage in the pool and marsh zones of the basin.
- Permanent and well-maintained temporary access will be provided to the stormwater pump station at all times and coordinated with the City of Des Moines to ensure operation and maintenance of the station.
- A new security fence will be constructed around the stormwater pump station.

Though its primary purpose is to improve water quality, the enhanced detention basins also serve as a place to enhance the health of the community and to offer educational opportunities for the larger community. At a Master Plan level, the area features paved and unpaved trails, wetland overlook areas, and seating areas. The proposed inclusion of interpretive signage and art would allow people of all ages to understand the role of the area for the region. The use of micro-grading for the basins allows the area to be carefully shaped. This shaping breaks down the scale of the regional detention, creates places that are enjoyable, and removes the utilitarian feel of the existing basins.



PROPOSED OPEN SPACE CHARACTER IMAGERY  
OCTOBER 19, 2022

# Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa

## Sheet 16 of 20



GRAY'S STATION

### PRECEDENT HOUSING CHARACTER IMAGERY

As a large neighborhood, the proposed housing character for Gray's Station is intended to be diverse and urban in form/character. The following images represent the intended character and feel of the multi-family and mixed-use residential housing types. The design of the buildings including architectural details, materials, final massing/heights and colors will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Each character image represents varied features as represented by the following categories that correspond to the colors under each image:

- Relationship to exterior open space (plaza, park, roof deck, etc.)
- Relationship to street
- Relationship to alley
- Building articulation/massing
- Architectural character
- Landscape character

### DOWNTOWN MIXED USE (NE CORNER)



### RESIDENTIAL

### MULTI-FAMILY / MEDIUM-HIGH DENSITY



### DOWNTOWN MIXED USE (BRIDGE LANDING)



PROPOSED BUILDING CHARACTER IMAGERY  
OCTOBER 19, 2022





Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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GRAY'S STATION

**PRECEDENT HOUSING CHARACTER IMAGERY**

As a large neighborhood, the proposed housing character for Gray's Station is intended to be diverse and urban in form/character. The following images represent the intended character and feel of the medium density housing types. The design of the buildings including architectural details, materials, final massing/heights and colors will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Each character image represents varied features as represented by the following categories that correspond to the colors under each image:

- Relationship to exterior open space (plaza, park, roof deck, etc.)
- Relationship to street
- Relationship to alley
- Building articulation/massing
- Architectural character
- Landscape character

**MEDIUM DENSITY RESIDENTIAL**



PROPOSED BUILDING CHARACTER IMAGERY  
 OCTOBER 19, 2022



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa  
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GRAY'S STATION



DOWNTOWN MIXED USE - PROTOTYPICAL BUILDING

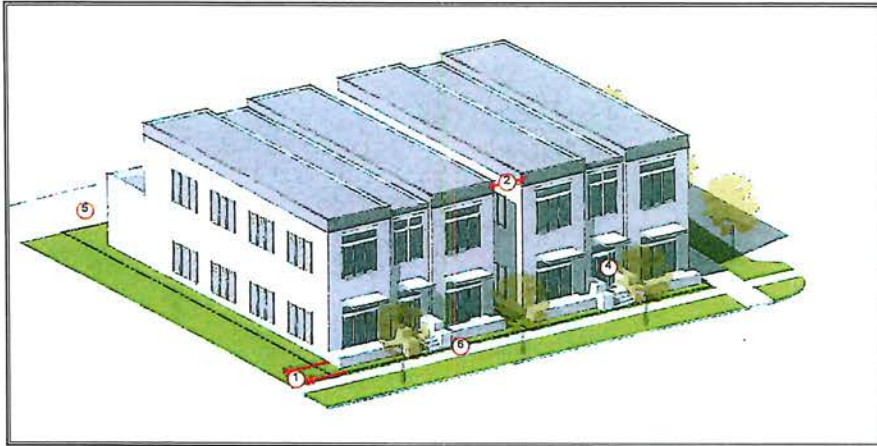
1. Primary facades of mixed-use and commercial structures shall be primarily parallel to the public right-of-way except in the subareas of W and X.
2. Buildings shall have entrances oriented toward primary street(s) and public plaza(s).
3. At least one building entrance for residential uses should directly access the street or public plaza when a residential use is located above street-level retail or commercial uses.
4. All buildings should have frontage on principal street(s), public plaza, park or greenway of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).
5. For commercial and mixed-use buildings, at least 70 percent of the building frontage should be within one foot of the property line (with exceptions as outlined in the Design Guidelines).
6. Facades of above grade structured parking (e.g., podium parking beneath commercial or residential uses) adjacent to any public right-of-way shall be architecturally and aesthetically consistent with the remainder of the building they support.
7. Minimum height for all Downtown Mixed Use buildings shall be four stories.
8. All Downtown Mixed Use buildings shall have a parapet or flat roof. Tower elements are allowed.



HIGH DENSITY RESIDENTIAL - PROTOTYPICAL BUILDING

1. For residential buildings, a maximum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
2. Buildings shall have entrances oriented toward primary street(s) and public plaza(s).
3. All buildings should have frontage on principal street(s), public plaza, park or greenway of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).
4. Facades of above grade structured parking (e.g., podium parking beneath commercial or residential uses) adjacent to any public right-of-way shall be architecturally and aesthetically consistent with the remainder of the building they support.
5. Minimum height for all uses shall be as follows:
  - a) Three stories for Subareas B, H, N, and O
  - b) Eight stories for Subareas Y and Z
6. All Downtown Mixed Use buildings shall have a parapet or flat roof. Tower elements are allowed.

PROPOSED PROTOTYPICAL FORMS  
OCTOBER 19, 2022



MEDIUM DENSITY RESIDENTIAL - PROTOTYPICAL MULTI-UNIT DWELLING UNIT

1. For all residential buildings, a maximum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
2. Attached and Detached residential buildings shall have side-yard building separation(s) (i.e., building face to building face) that are at minimum 5 feet and at maximum 10 feet except where walking paths occur between buildings.
  - a. Where walking paths occur between buildings the maximum width between buildings shall increase above the maximum separation by the width of the walking path.
3. Greenways shall include pedestrian pathways with a minimum width of 5'
4. Buildings shall have entrances oriented toward primary street(s), public plaza(s), parks, greenways or alleys.
5. All garages shall be oriented to an alley.
6. All buildings should have frontage on principal street(s), public plaza, park, greenways or alleys of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).



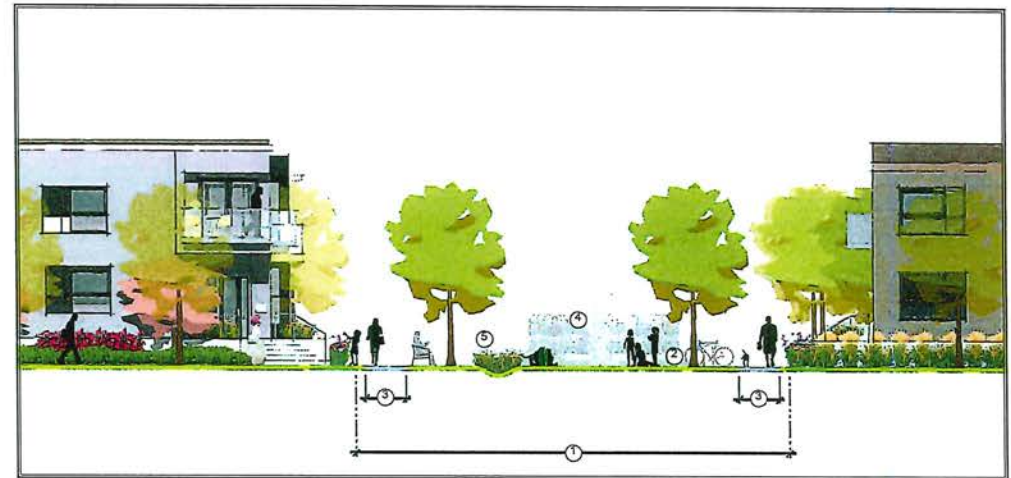
MEDIUM DENSITY RESIDENTIAL - PROTOTYPICAL DETACHED FORM (WITH FOURPLEX)

1. For all residential buildings, a maximum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
2. Attached and Detached residential buildings shall have side-yard building separation(s) (i.e., building face to building face) that are at minimum 5 feet and at maximum 10 feet except where walking paths occur between buildings.
  - a. Where walking paths occur between buildings the maximum width between buildings shall increase above the maximum separation by the width of the walking path.
3. Greenways shall include pedestrian pathways with a minimum width of 5'
4. Buildings shall have entrances oriented toward primary street(s), public plaza(s), parks, greenways or alleys.
5. All garages shall be oriented to an alley.
6. Accessory Dwelling Units shall be allowed.
7. All buildings should have frontage on principal street(s), public plaza, park, greenways or alleys of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).



MEDIUM DENSITY RESIDENTIAL - PROTOTYPICAL ATTACHED FORM

1. For all residential buildings, a maximum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
2. Attached and Detached residential buildings shall have side-yard building separation(s) (i.e., building face to building face) that are at minimum 5 feet and at maximum 10 feet except where walking paths occur between buildings.
  - a. Where walking paths occur between buildings the maximum width between buildings shall increase above the maximum separation by the width of the walking path.
3. Greenways shall include pedestrian pathways with a minimum width of 5'
4. Buildings shall have entrances oriented toward primary street(s), public plaza(s), parks, greenways or alleys.
5. All garages shall be oriented to an alley.
6. Accessory Dwelling Units shall be allowed.
7. All buildings should have frontage on principal street(s), public plaza, park, greenways or alleys of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).



GREENWAY - PROTOTYPICAL FORM

Green connections are encouraged to minimize impact of storm water, provide urban pathways for wildlife, and promote recreation, wellness, and pedestrian scale mobility.

1. Greenways shall maintain an approximate width of 40' to 50'. Exceptions may occur where infrastructure requirements necessitate and will be reviewed with the Final Development Plan.
2. Greenways shall include landscaping.
3. Greenways shall include pedestrian pathways with a minimum width of 5'. Pathway location may vary depending on context.
4. Greenways may include structural (e.g., Pergola, Shade Structure, Community Garden Features, etc.) and/or artistic elements where deemed appropriate by the developer.
5. Greenways may include stormwater quality areas, where appropriate.

# SITE DEVELOPMENT PLAN FOR: GRAY'S STATION TELUS

## DES MOINES, IOWA

### VICINITY - MAP

NOT TO SCALE



DES MOINES, IOWA

### OWNER

HMC NFS I LLC  
6900 WESTOWN PKWY  
WEST DES MOINES, IA 50288

### APPLICANT

HUBBELL REALTY COMPANY  
CONTACT: KRIS SANDORIS  
6900 WESTOWN PKWY  
WEST DES MOINES, IA 50266  
PH: (515) 243-3228

### ENGINEER

CIVIL DESIGN ADVANTAGE  
4121 NW URBANDALE DRIVE  
URBANDALE, IA 50322  
CONTACT: RYAN HARDESTY  
EMAIL: RYANH@CDA-ENG.COM  
PH: (515) 389-4400

### SURVEYOR

CIVIL DESIGN ADVANTAGE, LLC  
4121 NW URBANDALE DRIVE  
URBANDALE, IA 50322  
CONTACT: CHARLIE MCLOTHLEN  
EMAIL: CHARLIE@CDA-ENG.COM  
PH: (515) 389-4400

### ARCHITECT

SIMONSON & ASSOCIATES, INC.  
1717 INGERSOLL AVE SUITE #117  
DES MOINES, IA 50319  
CONTACT: STEPHANIE POOLE  
EMAIL: SPOOLE@SIMONSONASSOC.COM  
PH: (515) 440-5628

### SUBMITTAL DATES

FIRST SUBMITTAL: 10/19/2022  
SECOND SUBMITTAL: 01/04/2023

### ZONING

GRAY'S STATION PUD - ZON2017-00087

### EXISTING/ PROPOSED USE

EXISTING: UNDEVELOPED  
PROPOSED: HOUSEHOLD LIVING - 8+ HOUSEHOLDS PER LOT

### DEVELOPMENT SUMMARY

TOTAL SITE AREA: 3.48 ACRES (191,395 SF)  
EXISTING IMPERVIOUS AREA: 0.00 ACRES (0 SF)  
OPEN SPACE CALCULATION:  
TOTAL SITE: 191,395 SF (3.48 AC)  
BUILDINGS: 47,725 SF  
PARKING AREAS: 66,172 SF  
SIDEWALK: 14,844 SF  
OPEN SPACE PROVIDED: 22,554 SF (15.0%)  
UNITS: 84 UNITS (24.14 UNITS PER ACRE)  
PARKING:  
STANDARD: 48 SPACES  
GARAGE: 84 SPACES  
ADA: 5 SPACES  
TOTAL: 137 SPACES

### DATE OF SURVEY

08/05/2022

### BENCHMARKS

- CITY BENCHMARK #725: BRASS CAP IN THE NORTHEAST TRAFFIC SIGNAL BASE AT THE SOUTHWEST CORNER OF MARTIN LUTHER KING DRIVE AND SOUTHWEST 11TH STREET. ELEVATION=25.81
- CITY BENCHMARK #5581: BRASS TABLET IN THE CONCRETE WALL 43.6 FEET WEST OF THE EAST FACE NEAR THE SANITARY SEWER PUMP STATION. ELEVATION=25.48

### LEGAL DESCRIPTION

LOT 87, GRAY'S STATION PLAT 5, AN OFFICIAL PLAT IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND CONTAINING 3.48 ACRES (191,395 S.F.).

### TAX ABATEMENT - SUSTAINABILITY

- BIKE RACKS FOR PUBLIC USE THAT PROVIDE A MINIMUM OF 10% OF THE AUTOMOBILE PARKING PROVIDED.
- 50% INCREASE IN REQUIRED LANDSCAPING.
- PRIMARY ENTRY WITHIN A MILE OF A DART TRANSIT STOP.
- REDEVELOPMENT OF A PREVIOUSLY DEVELOPED SITE.

### INDEX OF SHEETS

NO.	DESCRIPTION
CO.1	COVER SHEET
C1.0-1.2	DIMENSION PLAN
C2.0-2.6	GRADING PLAN
C3.0-3.2	UTILITY PLAN
C4.0	DETAILS
L02.02 & L02.04	RDG LANDSCAPE PLAN - SITE LAYOUT PLAN
L04.02 & L04.04	RDG LANDSCAPE PLAN - SITE PLANTING PLAN
L05.01	RDG LANDSCAPE PLAN - DETAILS

### GENERAL LEGEND

PROPOSED	EXISTING
PROPERTY BOUNDARY	SANITARY MANHOLE
SECTION LINE	WATER VALVE BOX
CENTER LINE	FIRE HYDRANT
RIGHT OF WAY	WATER CURB STOP
BUILDING SETBACK	WELL
PERMANENT EASEMENT	STORM SEWER MANHOLE
TEMPORARY EASEMENT	STORM SEWER SINGLE INTAKE
TYPE SW-501 STORM INTAKE	STORM SEWER DOUBLE INTAKE
TYPE SW-502 STORM INTAKE	FLARED END SECTION
TYPE SW-503 STORM INTAKE	DECIDUOUS TREE
TYPE SW-505 STORM INTAKE	CONIFEROUS TREE
TYPE SW-506 STORM INTAKE	DECIDUOUS SHRUB
TYPE SW-512 STORM INTAKE	CONIFEROUS SHRUB
TYPE SW-513 STORM INTAKE	ELECTRIC POWER POLE
TYPE SW-401 STORM MANHOLE	GUY ANCHOR
TYPE SW-402 STORM MANHOLE	STREET LIGHT
FLARED END SECTION	POWER POLE W/ TRANSFORMER
TYPE SW-301 SANITARY MANHOLE	UTILITY POLE W/ LIGHT
STORM/SANITARY CLEANOUT	ELECTRIC BOX
WATER VALVE	ELECTRIC TRANSFORMER
FIRE HYDRANT ASSEMBLY	ELECTRIC MANHOLE OR VAULT
SIGN	TRAFFIC SIGN
DETECTABLE WARNING PANEL	TELEPHONE JUNCTION BOX
WATER CURB STOP	TELEPHONE MANHOLE/VAULT
SANITARY SEWER	TELEPHONE POLE
SANITARY SERVICE	GAS VALVE BOX
STORM SEWER	CABLE TV JUNCTION BOX
STORM SERVICE	CABLE TV MANHOLE/VAULT
WATERMAIN WITH SIZE	MAIL BOX
WATER SERVICE	BENCHMARK
SAWCUT (FULL DEPTH)	SOIL BORING
SILT FENCE	UNDERGROUND TV CABLE
USE AS CONSTRUCTED	GAS MAIN
MINIMUM PROTECTION ELEVATION	FIBER OPTIC
	UNDERGROUND TELEPHONE
	OVERHEAD ELECTRIC
	UNDERGROUND ELECTRIC
	FIELD TILE
	SANITARY SEWER W/ SIZE
	STORM SEWER W/ SIZE
	WATER MAIN W/ SIZE



### UTILITY WARNING

ANY UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY AND RECORDS OBTAINED BY THIS SURVEYOR. THE SURVEYOR MAKES NO GUARANTEE THAT THE UTILITIES SHOWN COMPRISE ALL THE UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UTILITIES SHOWN ARE IN THE EXACT LOCATION SHOWN.



**CIVIL DESIGN ADVANTAGE**

4121 NW URBANDALE DRIVE, URBANDALE, IA 50322  
PH: (515) 389-4400

PROJECT NO. 2205.350

**SITE PLAN APPROVAL:**

APPROVED  APPROVED WITH CONDITIONS - SEE EXHIBIT "A"

IN ACCORDANCE WITH SECTION 155-9, 2019 DES MOINES MUNICIPAL CODE, AS AMENDED.

NO CHANGES TO THIS PLAN UNLESS APPROVED BY WRITING FROM THE DEVELOPMENT SERVICES DIRECTOR.

DEVELOPMENT SERVICES DIRECTOR: \_\_\_\_\_ DATE: \_\_\_\_\_

THE PROJECT REQUIRES AN IOWA NPDES PERMIT #2 AND CITY OF DES MOINES GRADING PERMIT. CIVIL DESIGN ADVANTAGE WILL PROVIDE THE PERMITS AND THE INITIAL STORM WATER POLLUTION PREVENTION PLAN (SWPPP) FOR THE CONTRACTORS USE DURING CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR UPDATING THE SWPPP THROUGHOUT CONSTRUCTION AND MEETING LOCAL, STATE AND FEDERAL REQUIREMENTS.

ALL CONSTRUCTION MATERIALS, DUMPSTERS, DETACHED TRAILERS OR SIMILAR ITEMS ARE PROHIBITED ON PUBLIC STREETS OR WITHIN THE PUBLIC R.O.W.

THE 2022 EDITION OF THE SDAS, STANDARD SPECIFICATIONS, THE PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) AND ALL CITY SUPPLEMENTALS, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.

I HEREBY CERTIFY THAT THIS ENGINEERING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A QUALIFIED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF IOWA.

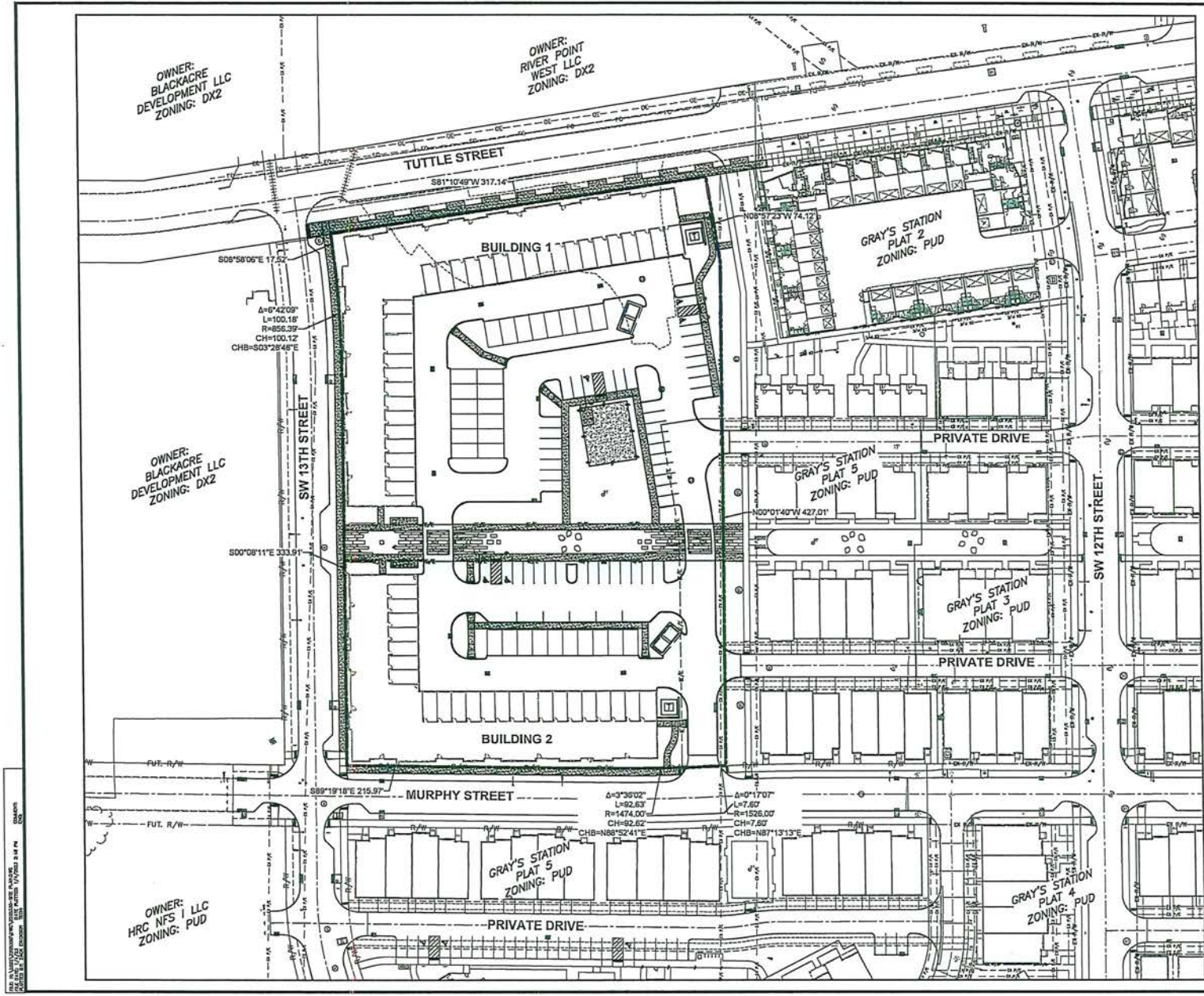
**PRELIMINARY**

DATE: \_\_\_\_\_

BY: \_\_\_\_\_

ALL C SERIES SHEETS

DATE PLOTTED: 1/11/2023 10:45 AM BY: RYAN HARDESTY



OWNER:  
BLACKACRE  
DEVELOPMENT LLC  
ZONING: DX2

OWNER:  
RIVER POINT  
WEST LLC  
ZONING: DX2

OWNER:  
BLACKACRE  
DEVELOPMENT LLC  
ZONING: DX2

OWNER:  
HRC NFS LLC  
ZONING: PUD

**GENERAL NOTES**

1. THE 2022 EDITION OF THE SDAS STANDARD SPECIFICATIONS AND ALL CITY SUPPLEMENTALS, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.
2. ALL WORK SHALL COMPLY WITH ADA ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES.
3. ALL WORK SHALL BE IN ACCORDANCE WITH OSHA CODES AND STANDARDS, NOTHING INDICATED ON THE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLIANCE WITH ANY APPLICABLE SAFETY REGULATIONS.
4. PRIOR TO ANY WORK AT THE SITE, CONTRACTOR SHALL EXAMINE ANY APPLICABLE DRAWINGS AVAILABLE FROM THE OWNER, ENGINEER, AND/OR ARCHITECT, AND CONSULT WITH OWNER'S PERSONNEL AND UTILITY COMPANY REPRESENTATIVES. NO COMPENSATION WILL BE ALLOWED FOR DAMAGE FROM FAILURE TO COMPLY WITH THIS REQUIREMENT.
5. ONE WEEK PRIOR TO CONSTRUCTION WITHIN CITY BULK OR ANY CONNECTION TO PUBLIC SEWER, CONTRACTOR SHALL NOTIFY THE CITY'S CONSTRUCTION DIVISION. CONTRACTOR SHALL NOTIFY CITY OF DES MOINES ENGINEERING DEPT. AT LEAST 48 HOURS PRIOR TO COMMENCEMENT OF CONSTRUCTION TO COORDINATE UTILITIES. CALL 515-281-4177 OR 515-281-4177.
6. ALL CONSTRUCTION WITHIN PUBLIC RIGHT-OF-WAY, EASEMENTS, AND/OR ANY CONNECTION TO PUBLIC SEWERS AND STREETS, SHALL COMPLY WITH THE CITY STANDARDS. NO STEPS ARE ALLOWED IN SANITARY MAINS.
7. ALL EASEMENTS ARE TO BACK OF EXIST. BUILDING FACE OR PROPERTY LINE UNLESS OTHERWISE NOTED.
8. CONTRACTOR SHALL NOTIFY BUILDING DEPARTMENT WITH ARCHITECTURAL PLANS.
9. PLACE 3/4" HIGH EXPANSION JOINT BETWEEN ALL PAVEMENT/SEWERMAKS AND BUILDING. PLACE 1/2" HIGH EXPANSION JOINT BETWEEN SEWERMAK AND PAVEMENT OR STORAGE OF MATERIALS.
10. REMOVE ALL DEBRIS SPILLED INTO BULK AT THE END OF EACH HOUR.
11. ALL PROPERTY PINS SHALL BE PROTECTED FROM GRADING OR OTHER OPERATIONS. ANY PINS DESTROYED SHALL BE RESET AT THE CONTRACTOR'S EXPENSE.
12. DO NOT STORE CONSTRUCTION MATERIALS AND EQUIPMENT IN THE RIGHT-OF-WAY.
13. THE CONTRACTOR SHALL NOT DESTROY IDENTIFIABLE CHASE AREAS AND SIGNALS, SIGNS OUTSIDE THE CONSTRUCTION AREA. THE CONTRACTOR WILL NOT BE PERMITTED TO PARK OR SERVICE VEHICLES AND EQUIPMENT OR USE THESE AREAS FOR STORAGE OF MATERIALS, SUPPLIES, FUELING AND SERVICE AREAS WILL BE SUBJECT TO THE APPROVAL OF THE ENGINEER.
14. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY AREAS OF PAVEMENT OR SIGNALS NOT TO BE DESTROYED THAT IS DAMAGED DUE TO CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE TO PLACE TEMPORARY WARNING SIGNS AT THE LOCATION OF ANY DAMAGE TO THE ROADWAY. REPLACEMENT FEATURES ARE NOT INSTALLED THE SAME DAY, AS DIRECTED BY THE ENGINEER AND THE CITY.
15. THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR THE COORDINATION OF WORK BETWEEN SUPPLIERS AND SUBCONTRACTORS INVOLVED IN THE PROJECT, INCLUDING STAGING OF CONSTRUCTION MATERIALS.
16. CONCRETE REMOVAL FOR DRIVEWAY APPROACHES SHALL BE REMOVED TO THE NEAREST TRANSVERSE JOINT. CONTRACTOR SHALL VERIFY REMOVAL LIMITS WITH CITY'S CONSTRUCTION INSPECTOR PRIOR TO ANY CONCRETE REMOVAL.
17. ALL FORMS AND ADDITIONAL FEES REQUIRED TO COMPLETE THE WORK SHALL BE INCLUDED IN THE CONTRACTOR BIDDING.
18. ALL WORK WITHIN THE CITY RIGHT-OF-WAY REQUIRES A RIGHT-OF-WAY PERMIT.
19. CONTRACTOR SHALL APPLY FOR A SEWER/DRIVEWAY APPROACH PERMIT IN ADVANCE OF WORK.

**CITY OF DES MOINES NOTES**

1. ANY AMENDMENTS OR CHANGES TO THE PROJECT SITE THAT DO NOT MEET WHAT IS SHOWN ON THE SITE PLAN NEED TO BE APPROVED WITH THE PERMIT AND DEVELOPMENT CENTER PRIOR TO INSTALLATION/CONSTRUCTION.
2. ALL LIGHTING SHALL CONSIST OF LOW-GLARE FULL CUT-OFF DOWN-DIRECTED TYPE FIXTURES TO REDUCE THE GLARE OR LIGHT POLLUTION ON SURROUNDING PROPERTIES.
3. THE REQUIRED LANDSCAPING, BOTH EXISTING AND PROPOSED, SHALL BE MAINTAINED FOR THE LIFE OF THE CERTIFICATE OF OCCUPANCY OR CERTIFICATE OF ZONING COMPLIANCE.
4. NO MECHANICAL OR UTILITY EQUIPMENT OVER 3" IN HEIGHT SHALL BE ALLOWED IN ANY UNPAVED FRONT YARD SEWERMAK AREA.
5. ALL MECHANICAL EQUIPMENT MUST BE SCREENED ON ALL SIDES WITH ARCHITECTURAL SCREENING EQUAL TO THE HEIGHT OF THE EQUIPMENT.
6. ALL DISTURBED AREAS SHALL BE RESTORED BY SEEDING OR SOILING.
7. THIS SITE SHALL BE MAINTAINED IN COMPLIANCE WITH ALL CITY CODES APPLICABLE ON THE DATE OF SITE PLAN APPROVAL.
8. ANY GROUND-MOUNTED OR WALL-MOUNTED UTILITIES AND METERS SHALL BE SCREENED AND NOT VISIBLE FROM A PUBLIC STREET.
9. MECHANICAL EQUIPMENT SHALL NOT BE LOCATED IN THE FRONT YARD OR SIDE YARD AND SHALL BE SCREENED FROM VIEW FROM ANY PUBLIC RIGHT-OF-WAY WITH LANDSCAPING, FENCING, OR WALL CONSISTENT WITH THE BUILDING DESIGN, COLORS, AND MATERIALS.
10. ALL UTILITIES SHALL BE UNDERGROUND.
11. STORAGE SHALL BE MOVEMENT OR DISCREET BUT IN NO CASE SHALL BE POLE STORAGE.
12. ALL PLAYING SURF SHALL INCLUDE NON-LIVING PERMEABLE MATERIALS SUCH AS SAND, NOT BELONGING ROCK.

**PAVEMENT THICKNESS**

1. SIDEWALKS 4" P.C.C.
2. CLASS "A" SIDEWALK 4" P.C.C.
3. PRIVATE DRIVE 6" P.C.C.
4. SIDEWALK RAMP 8" P.C.C.



DATE	
REVISIONS	
SECOND SUBMITTAL	04/10/2022
FIRST SUBMITTAL	03/10/2022

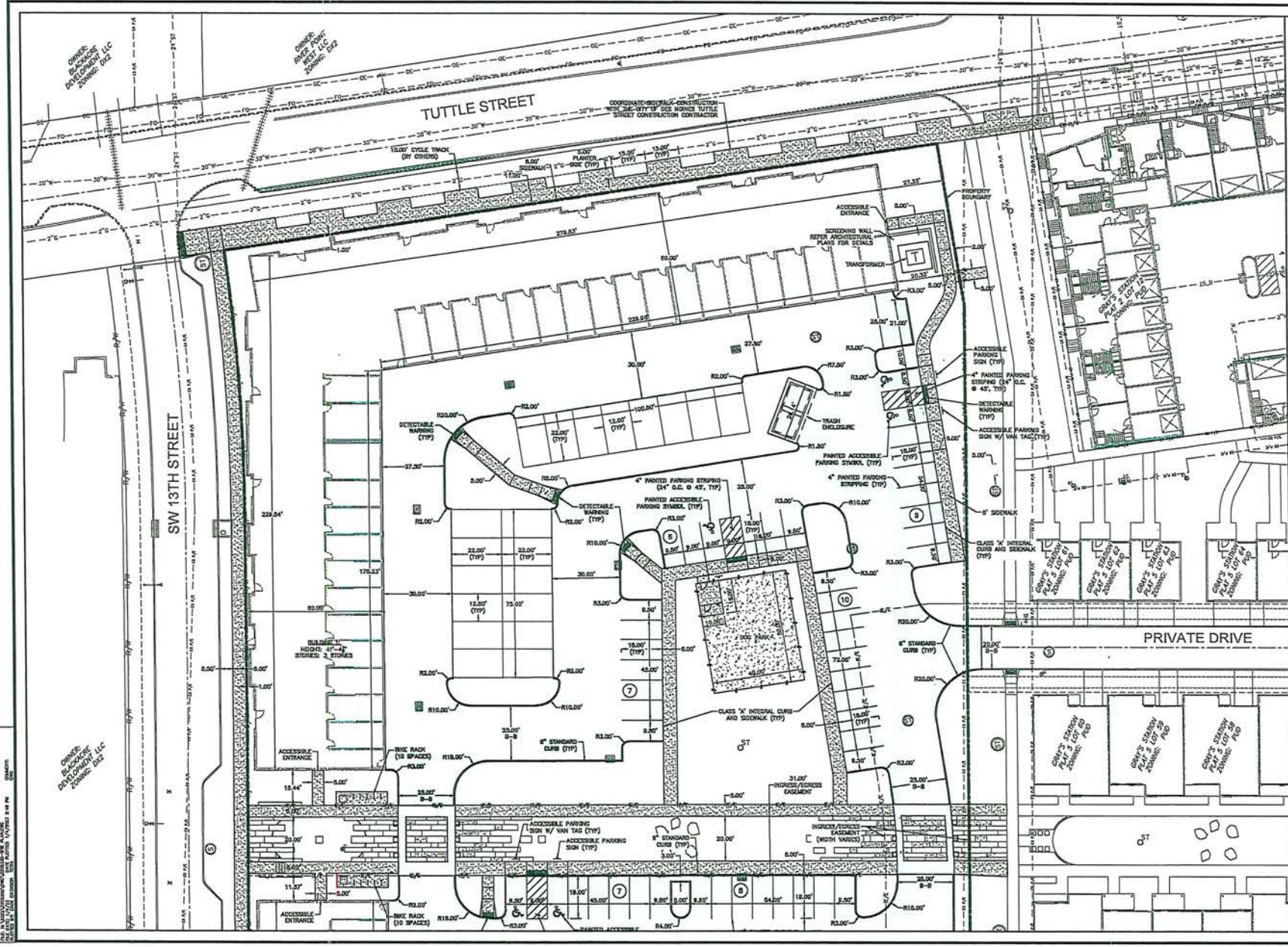
4121 NW URBANDALE DRIVE  
URBANDALE, IA 50322  
PHONE: (515) 385-4400

TECH: [blank]  
ENGINEER: [blank]

**GRAY'S STATION TELUS**  
OVERALL DIMENSION PLAN

**C1.0**  
2205.350

DES MOINES, IOWA



OWNER: GRAY'S STATION DEVELOPMENT ZONING: D2Z  
 OWNER: BLANKENHORN DEVELOPMENT ZONING: D2Z  
 OWNER: RALPH POPE, JR. WEST LLC ZONING: D2Z

DATE	REVISIONS
01/14/2025	SECOND SUBMITTAL
09/16/2024	FIRST SUBMITTAL

4121 NW URBANDALE DRIVE  
 URBANDALE, IA 50322  
 PHONE: (515) 369-4400



**GRAY'S STATION TELUS**  
 DIMENSION PLAN

**C1.1**

2205.350

TECH:

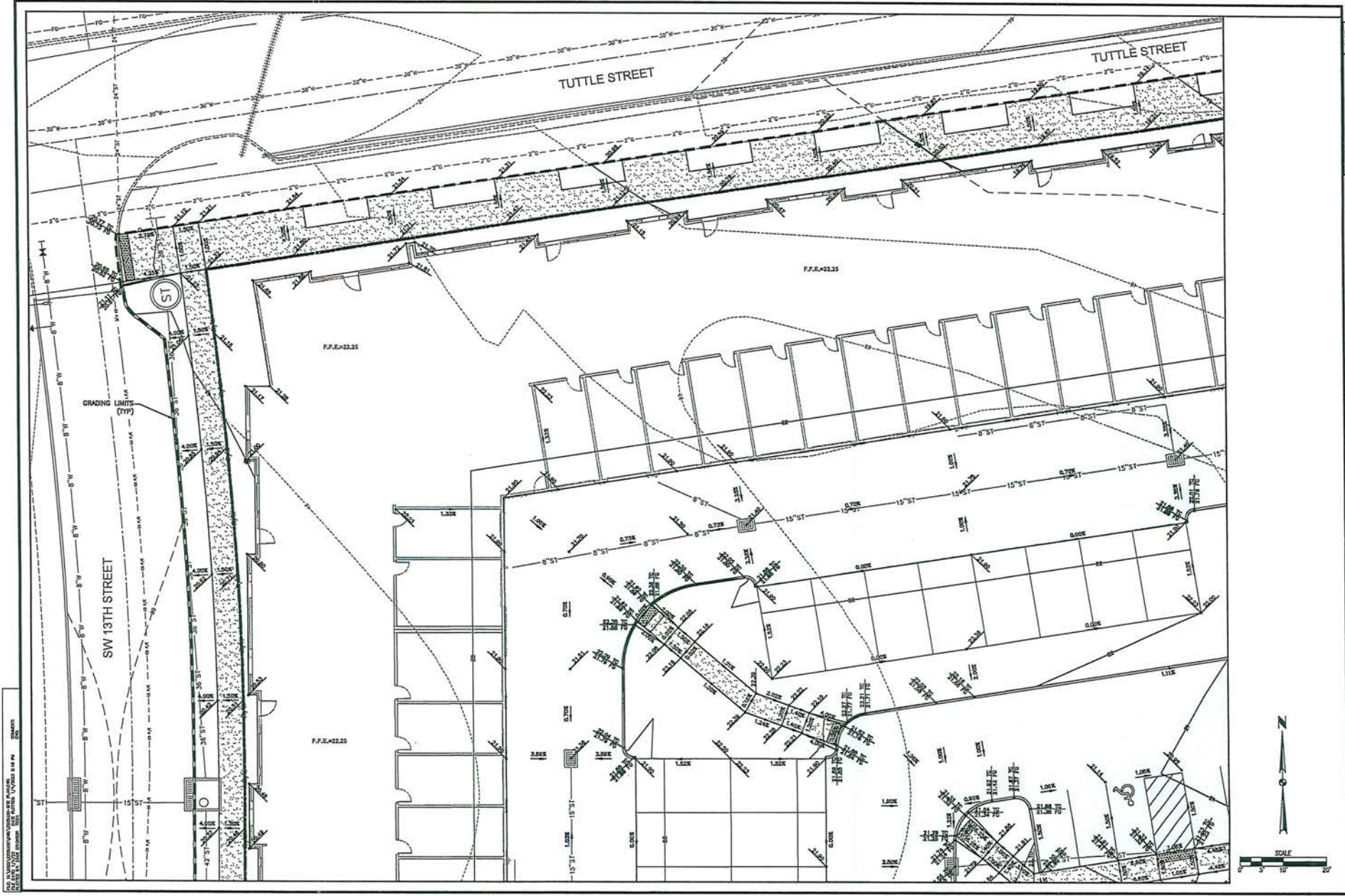
ENGINEER:

DES: HONNES, ICWA

CIVIL DESIGN ADVANTAGE







ALL IN UNITS UNLESS OTHERWISE SPECIFIED.  
 DATE: 05/11/2010 10:52 AM  
 PROJECT: 2205.360  
 DRAWING: GRADING PLAN

REVISIONS	DATE

4121 NW URBANDALE DRIVE  
 URBANDALE, IA 50322  
 PHONE: (515) 269-4400



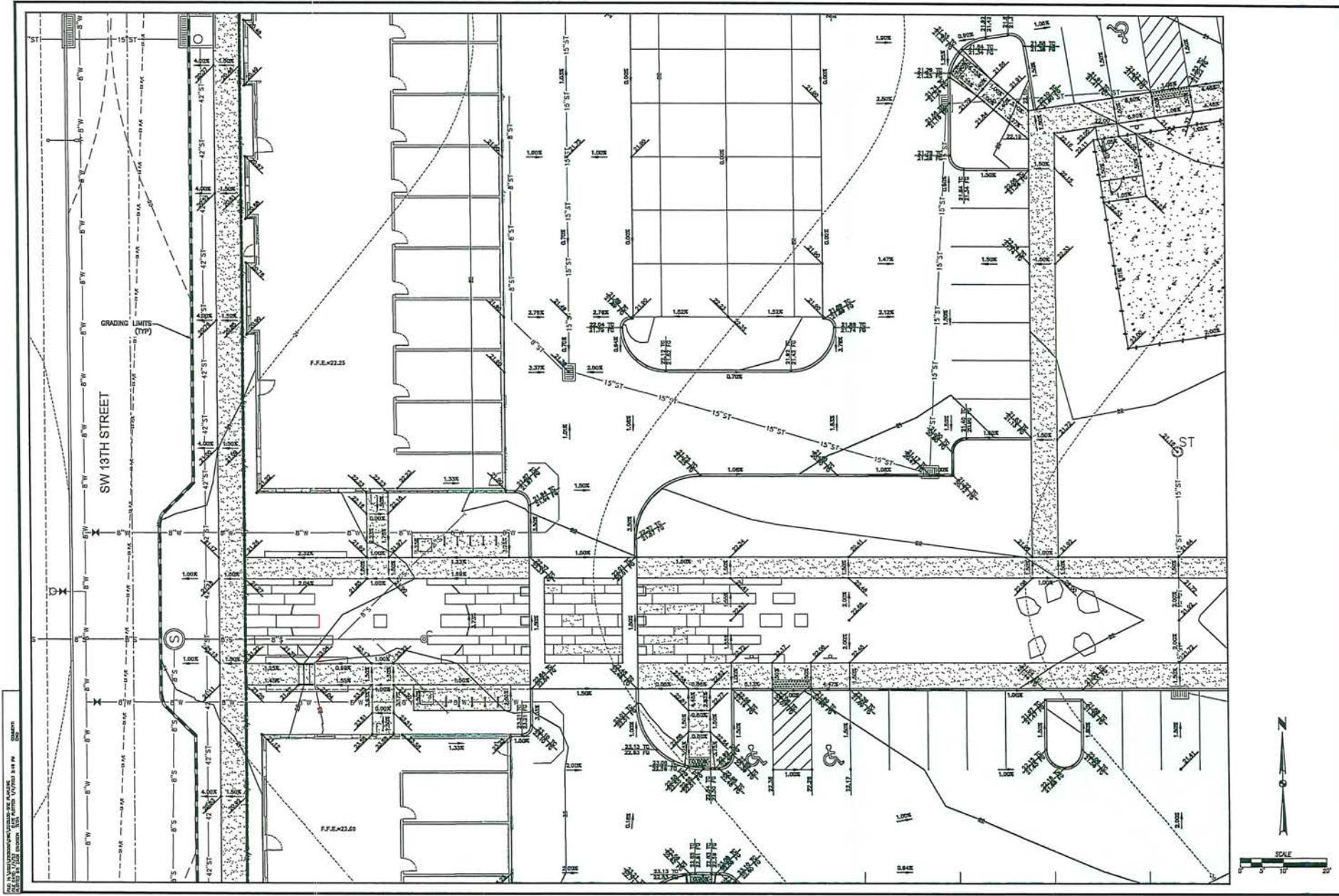
DES MOINES, IOWA

**GRAY'S STATION TELUS**  
 GRADING PLAN

**C2.1**  
 2205.360

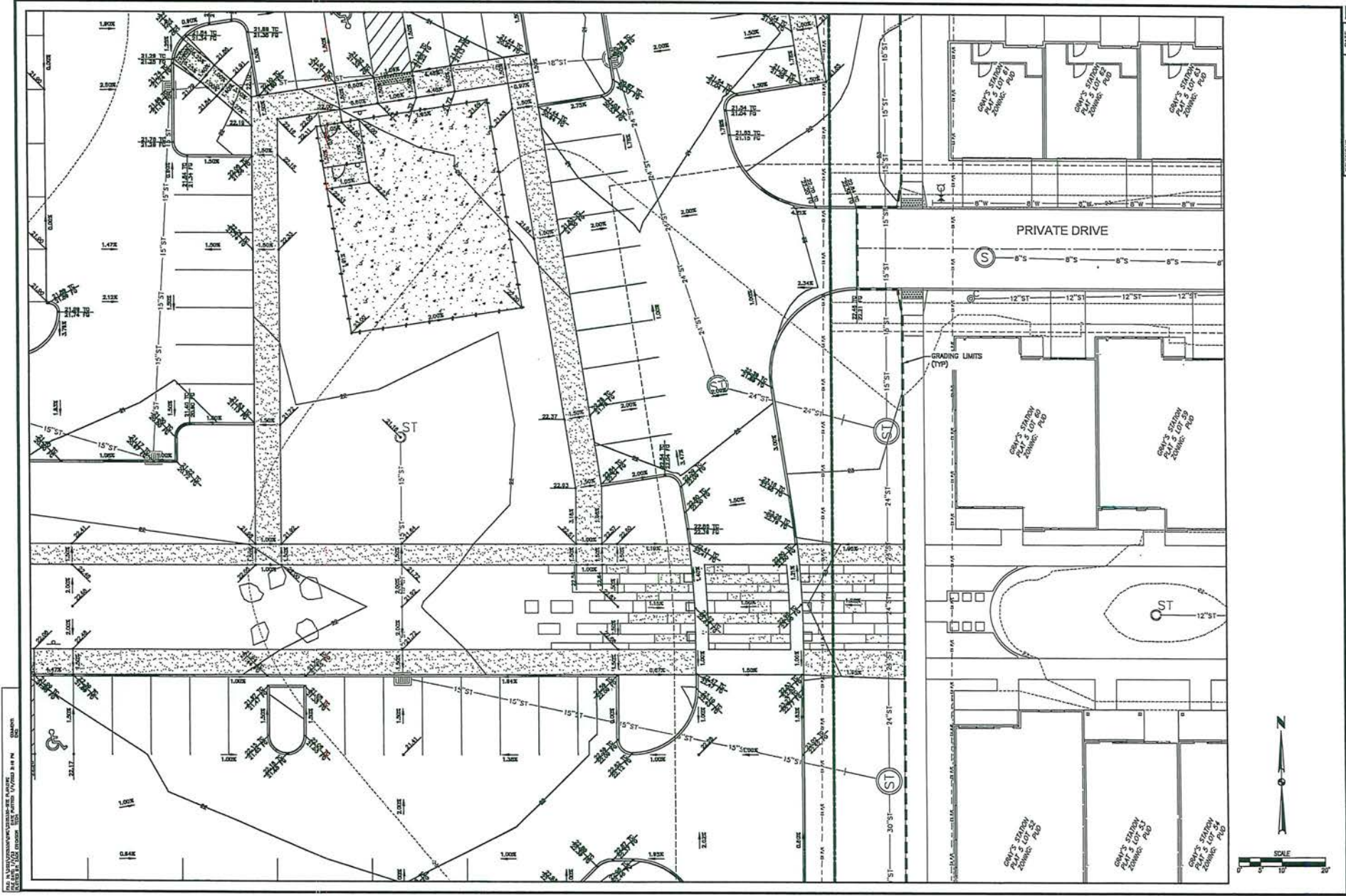
TECH: \_\_\_\_\_ ENGINEER: \_\_\_\_\_





DATE: 10/15/2012  
 TIME: 10:00 AM  
 PROJECT: GRAY'S STATION TELUS  
 DRAWING: GRADING PLAN  
 SHEET: 2205.350

	4121 NW URBANDALE DRIVE URBANDALE, IA 50322 PHONE: (515) 389-4000	TECH:
	ENGINEER:	
	REVISIONS 01/04/2012 SECOND SUBMITTAL 01/16/2012 FIRST SUBMITTAL	DATE:
<b>GRAY'S STATION TELUS</b> <b>GRADING PLAN</b>	<b>C2.3</b> 2205.350	



REVISIONS	DATE

4121 NW URBANDALE DRIVE  
 URBANDALE, IA 50322  
 PHONE: (515) 365-4400



DES MOINES, IOWA  
 CIVIL DESIGN ADVANTAGE

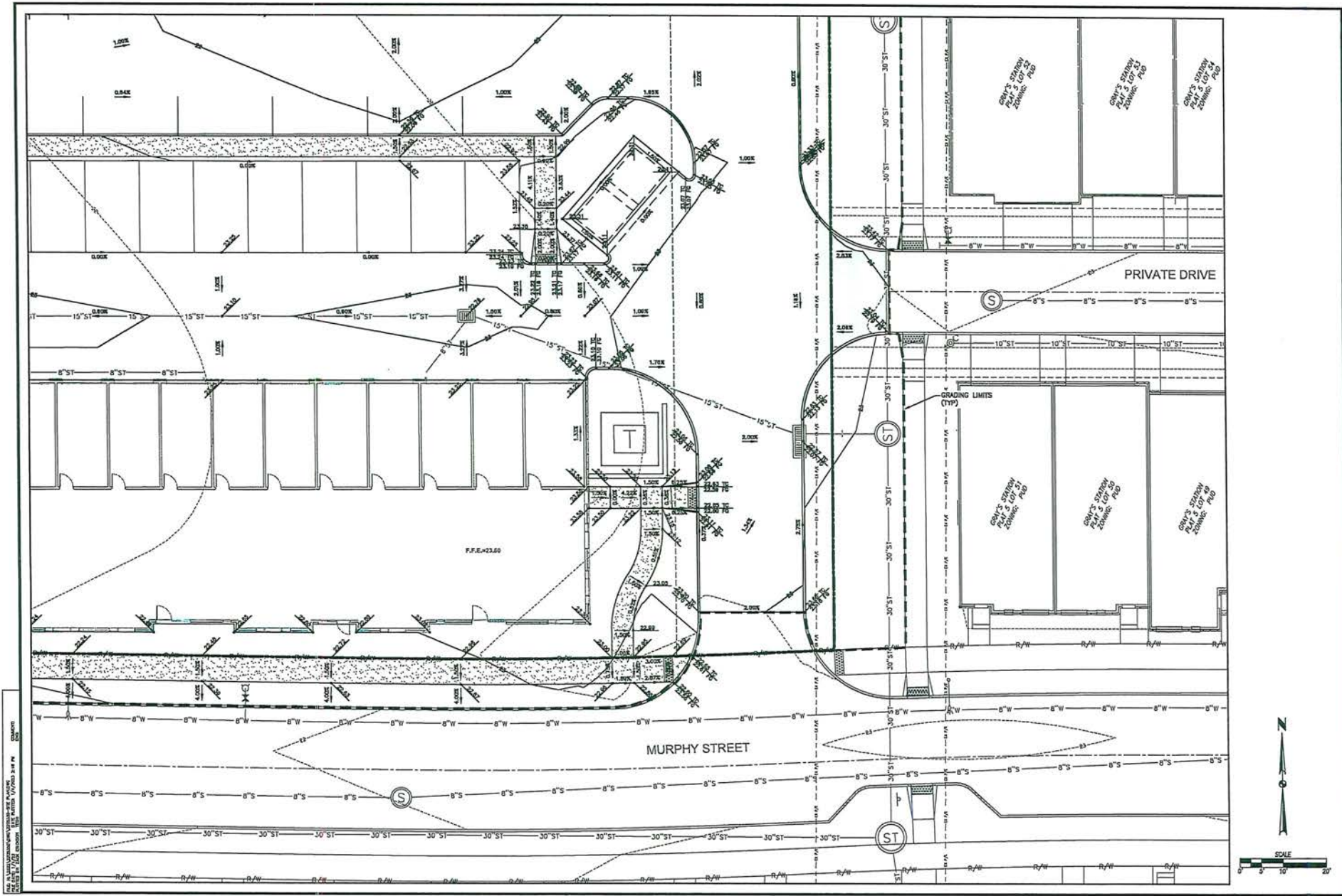
**GRAY'S STATION TELUS**  
 GRADING PLAN

**C2.4**  
 2205.350

TECH: \_\_\_\_\_  
 ENGINEER: \_\_\_\_\_

2205.350





DATE: 10/15/2022  
 TIME: 10:00 AM  
 DRAWN BY: J. W. HARRIS  
 CHECKED BY: J. W. HARRIS  
 PROJECT: GRADING PLAN

DATE	10/15/2022
REVISIONS	
SECOND SUBMITTAL	
FIRST SUBMITTAL	

4121 NW URBANDALE DRIVE  
 URBAN PLATE: 18-9022Z  
 PHONE: (515) 389-4400

TECH: [Blank]  
 ENGINEER: [Blank]

**EA** CIVIL DESIGN ADVANTAGE  
 DES. MOINES, IOWA

**GRAY'S STATION TELUS**  
 GRADING PLAN

**C2.6**  
 2205.350













REVIEWED BY: J.P.  
 HUBBELL REALTY



**B1** DUO BENCH - FORMS AND SURFACES  
 NOT TO SCALE



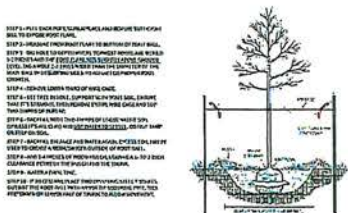
**B2** LIMESTONE SEAT BOULDERS  
 NOT TO SCALE



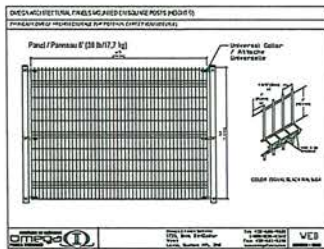
**B3** TRASH RECEPTACLE - FORMS AND SURFACES  
 NOT TO SCALE



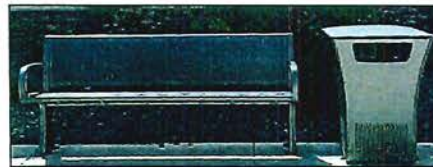
**B4** BIKE RACK - LANDSCAPE FORMS  
 NOT TO SCALE



**A1** CITY OF DES MOINES PLANTING SPECIFICATION  
 NOT TO SCALE



**A2** FENCE  
 NOT TO SCALE



**A3** BENCH AND TRASH RECEPTACLES - FORMS AND SURFACES  
 NOT TO SCALE

GRAY'S STATION PLAT 5 PRELIMINARY - NOT FOR CONSTRUCTION DESIGN DEVELOPMENT

ARCHITECT: RDG... PLANNING • DESIGN

LANDSCAPE ARCHITECT: HUBBELL REALTY

KEY PLAN

REVISION	DATE

RDG Planning & Design

PROJECT: GRAY'S STATION PLAT 5

DATE: 08/20/2014

PROJECT NO: 14-001

SCALE: 1/8" = 1'-0"

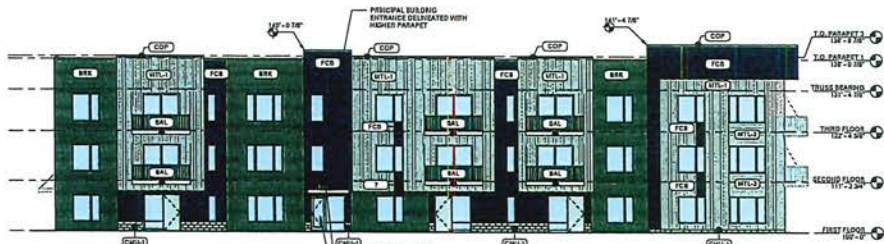
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DATE: 08/20/2014

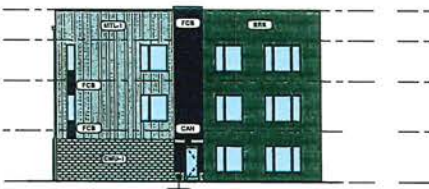
PROJECT NO: 14-001

SCALE: 1/8" = 1'-0"

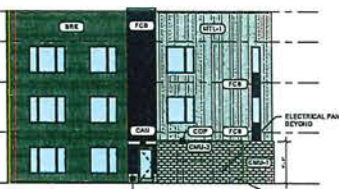




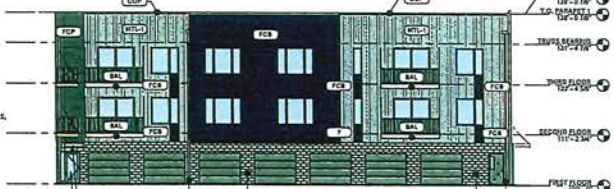
1 BUILDING #2 SW 13TH STREET  
SCALE 3/8" = 1'-0"



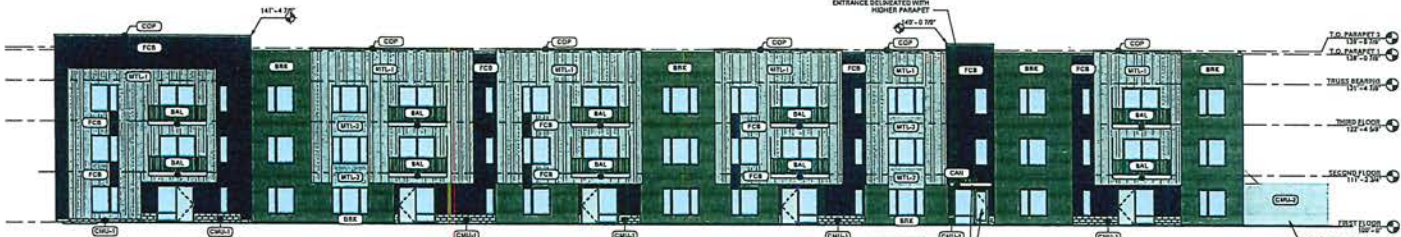
6 BUILDING #2 - SIDE (GREEN BELT)  
SCALE 3/8" = 1'-0"



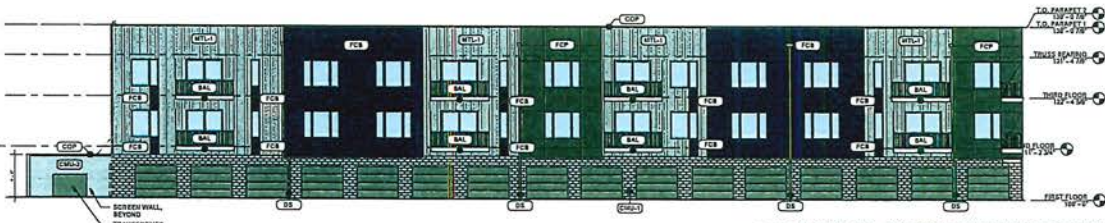
4 BUILDING #2 SIDE (EAST END)  
SCALE 3/8" = 1'-0"



3 BUILDING #2 - INTERNAL ELEVATION (EAST)  
SCALE 3/8" = 1'-0"



2 BUILDING #2 - MURPHY STREET  
SCALE 3/8" = 1'-0"



5 BUILDING #2 - INTERNAL ELEVATION (SOUTH)  
SCALE 3/8" = 1'-0"

EXTERIOR MATERIAL LEGEND	
MAT. LABEL	MATERIAL DESCRIPTION
GLC	PREFABRICATED BALCONY, CLEAR ANODIZED ALUMINUM
BRK	PAINT BRICK MODULAR BRICK, NATURAL, VARIETAL, COLOR - GRAY
BRN	PREFABRICATED CANOPY, MAPLE, OF EQUAL COLOR, CLEAR ANODIZED ALUMINUM
CMU	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW2	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW3	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW4	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW5	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW6	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW7	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW8	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW9	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW10	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW11	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW12	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW13	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW14	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW15	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW16	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW17	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW18	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW19	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING
CMW20	PREFABRICATED CMU BLOCK, CONCRETE, WITH VERTICAL BUSHING

**GENERAL EXTERIOR ELEVATION NOTES**

- REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINE DOCUMENTS.
- ALL CONDUIT RUNS & SERVICE LINES SHALL BE GROUPED TOGETHER IN A HEAT & COLDLY MANNER AND SHOULD PENETRATE THE WALL AT THE LOWEST POINT AND ARE NOT TO RUN OVER THE TOP OF PARAPET. ALL OTHER CONDUITS & JUNCTION BOXES FOR EXTERIOR EQUIPMENT SHALL BE FULLY CONCEALED WITHIN THE BUILDING CONSTRUCTION.
- ALL HORIZONTAL TRANSITIONS BETWEEN DIFFERENT MATERIALS SHALL BE FINISHED CONTINUOUSLY FROM TOP TO BOTTOM AND EXTENDING OVER TO EXTERIOR FACE OF LOWER MATERIAL WITH DEEP EDGE.
- ANY RETAINING OR BLIND ELEVATIONS NOT SHOWN SHALL BE SIMILAR IN MATERIAL AND FINISH TO EXTERIOR CONDITIONS OR OTHER SIMILAR CONDITIONS.
- NO EXPOSED MASONRY SHALL BE USED. MASONRY RETURNING AT WINDOW/DOOR OPENINGS OR FRAMING IS NOT DEEP ENOUGH TO PROTECT CLOSURE AT THE BACKSIDE OF MASONRY FOR AN EXPOSED SEALED JOINT.
- MATERIALS ABOVE CANOPIES & AWNINGS TO BE CONTINUED DOWN BEHIND THEM USUAL, TYP.
- ANY EXPOSED ROOFING MEMBRANE VISIBLE FROM A PUBLIC RIGHT AWAY MUST MATCH THE PRIMARY WALL MATERIAL SOLOID.
- INSTALL VERTICAL EXPANSION JOINTS FOR STRUCTURAL OR MINIMAL AT COLUMN/PIERS LINES AND AT THE HEAD OF DOOR JAMBSCOPINGS OR AS OTHERWISE NOTED OR INVERSE REQUIRED PER STANDARD PRACTICE TO REDUCE STRESS CRACKING. NO CONTROL JOINTS/PANESION JOINT AT MAIN STRUCTURAL BEARING LOCATIONS. VERIFY JOINTS WITH ARCHITECT.
- SCALE & SEAL ALL TRANSITION-CONTROL-EXPANSION AT ALL EXTERIOR MATERIALS.
- PROVIDE CAST REGLETS, JUST IN FROM THE OUTER EDGE ON UNDERSIDE OF FACE MATERIAL OR DRIP EDGE AT ALL OVERHANGS & ROFFETS.
- ISOLATE AND SEAL TO ENSURE ADEQUATE AIR INFILTRATION BARRIER AROUND ALL EXTERIOR PENETRATIONS TO PREVENT WATER AND SPERMAL PIPE FREZING.
- WHERE EXTERIOR FINISH ADJACENT TO BUILDING, TOP OF FINISH TO BE 1/4" LOWER THAN INTERIOR FINISH FLOOR. SLOPING CONTINUOUSLY AWAY FROM BUILDING A MINIMUM OF 2% BEYOND EXTERIOR 4" OUT FROM DOOR + WEIR. MAX. SLOPE, AFTER 4" AWAY FROM DOOR 1/8" MAXIMUM SLOPE TO PUBLIC WALK, (REFER TO CODE).
- WHERE NO EXTERIOR FINISH ADJACENT TO BUILDING, TOP OF FINISH LANDSCAPE A MINIMUM OF 4" BELOW INTERIOR FINISH FLOOR AND 4" BELOW EXTERIOR WALK, (REFER TO CODE). SLOPING CONTINUOUSLY AWAY FROM BUILDING A MINIMUM OF 2% (REFER TO CODE).
- ADDRESS SHALL BE PROVIDED ON STREET SIDE OF BUILDING AND ON ADJACENT SIDE. NUMBERS A MINIMUM 3" HIGH CONTRASTING COLOR NUMBERS WITH MINIMUM 1/4" STROKE.
- IF ANY SIGNAGE OR ECHO BASE IS SHOWN IT IS FOR REFERENCE ONLY. FINAL DESIGN AND/OR ALLOWANCES SHALL BE DETERMINED WITH APPROVED SIGNAGE PERMITS.

**MISC EXTERIOR MATERIAL FINISHES**

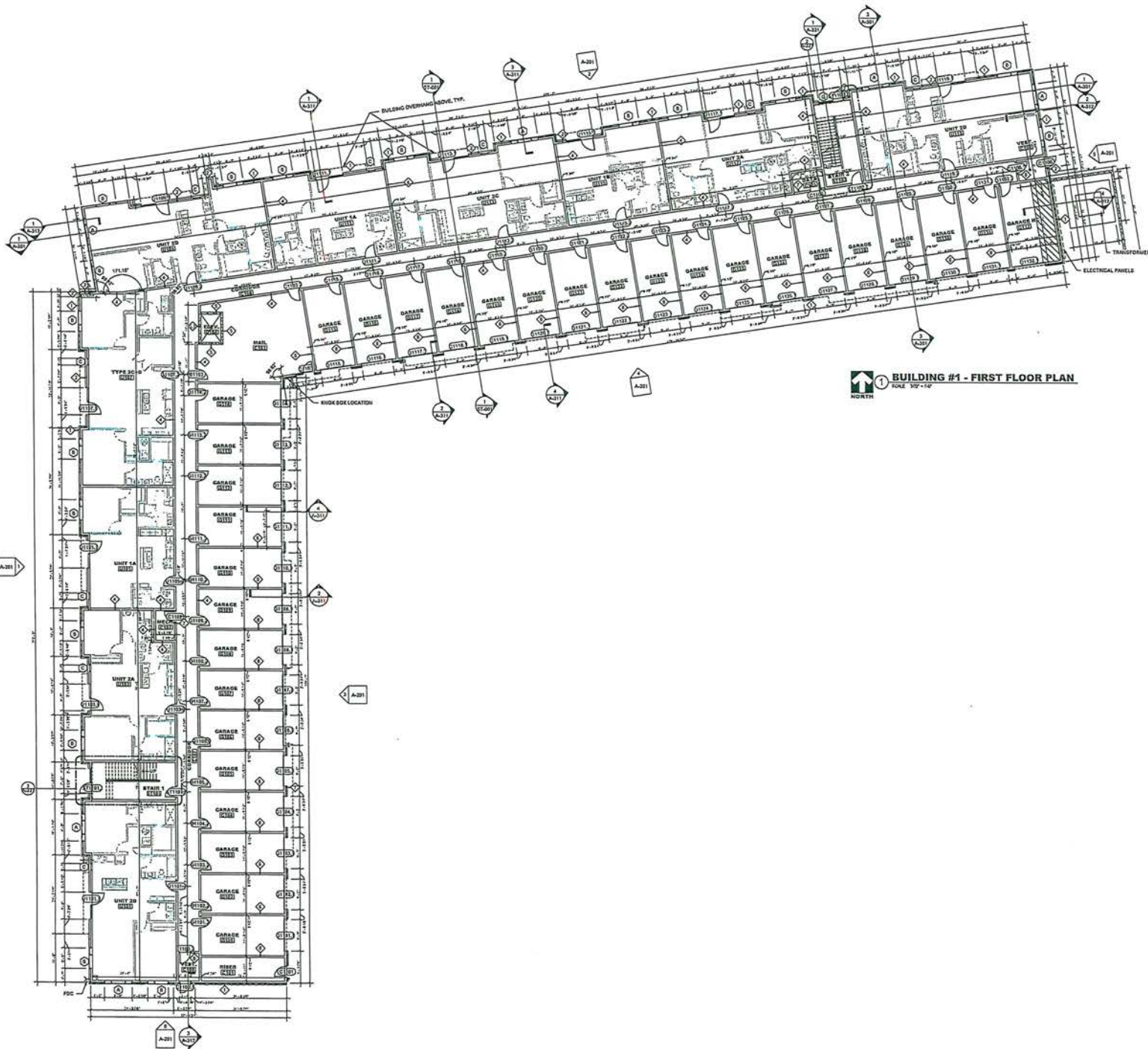
- TYPICAL FINISHES:**
- EXPOSED CONCRETE: SAND-SMOOTH & BUSHCOAT WITH CEMENT SLURRY.
  - EXPOSED UNITS: GALVANIZED & PAINTED TO MATCH ADJACENT MATERIAL ABOVE.
  - METAL BALCONIES & RAILINGS: GALVANIZED & PAINTED WHITE.
  - FENCES & GATES: METAL, PREFERRED TO MATCH ADJACENT MATERIAL.
  - EQUIPMENT, GUTTERS & DOWNSPUTS: METAL, PREFERRED TO MATCH ADJACENT MATERIAL.
  - CAULKS & SEALANTS: COLOR MATCHES TO ADJACENT MATERIAL.
  - HOLLOW METAL WALL DOORS & FRAMES: PAINTED TO MATCH ADJACENT MATERIAL.
  - OUTSIDE PANEL DOORS: METAL, PREFERRED TO MATCH ADJACENT MATERIAL.
  - LOWERS: METAL, PREFERRED TO MATCH GRAY.
  - LIGHT FIXTURES: METAL, PREFERRED TO MATCH ADJACENT MATERIAL.
  - MECHANICAL EQUIPMENT: SCREENED BY PARAPET.
  - EXTERIOR UTILITY EQUIPMENT: IF ALLOWED, PAINTED TO MATCH ADJACENT MATERIAL.
  - TRIM SIZES, TYP. U.L.O.:
    - WINDOWS & DOORS 6-1/2"
    - VERTICAL 1/2"
    - HORIZONTAL 3/4"

MATERIAL CALCULATIONS (CITY OF DES MOINES)	
<b>SIDE ELEVATION (GREEN BELT)</b>	2,888.8 SF TOTAL W/O PENETRATIONS: 1,884.8 SF
GLASS =	375.32 SF 12.9%
BRICK =	421.52 SF 14.6%
FIBER CEMENT =	202.34 SF 7.0%
METAL PANEL =	372.21 SF 12.9%
CMU =	241.89 SF 8.4%
<b>SW 13TH STREET (STREET FACING)</b>	6,214.2 SF TOTAL W/O PENETRATIONS: 4,317.4 SF
GLASS =	1,474.00 SF 23.7%
BRICK =	1,881.54 SF 30.4%
FIBER CEMENT =	1,118.00 SF 18.0%
METAL PANEL =	1,801.52 SF 29.1%
CMU =	94.22 SF 1.5%
<b>MURPHY STREET (STREET FACING)</b>	8,984.1 SF TOTAL W/O PENETRATIONS: 7,181.2 SF
GLASS =	2,282.71 SF 25.4%
BRICK =	2,983.52 SF 33.2%
FIBER CEMENT =	1,810.50 SF 20.1%
METAL PANEL =	3,221.37 SF 35.9%
CMU =	126.02 SF 1.4%
<b>INTERNAL ELEVATION (EAST)</b>	3,281.18 SF TOTAL W/O PENETRATIONS: 2,232.73 SF
GLASS =	1,188.78 SF 36.2%
BRICK =	81.25 SF 2.5%
FIBER CEMENT =	681.25 SF 20.8%
METAL PANEL =	1,359.50 SF 41.5%
CMU =	420.38 SF 12.8%
<b>INTERNAL ELEVATION (SOUTH)</b>	8,217.4 SF TOTAL W/O PENETRATIONS: 6,535.8 SF
GLASS =	2,381.78 SF 29.1%
BRICK =	822.25 SF 10.0%
FIBER CEMENT =	1,858.25 SF 22.6%
METAL PANEL =	1,888.25 SF 23.0%
CMU =	867.15 SF 10.5%

WINDOW RECESS BY MATERIAL:	
BRICK:	5'-0"
CMU VENEER:	5'-0"
FIBER CEMENT PANEL:	5'-0"
METAL PANEL:	5'-0"
METAL PANEL:	5'-0"



**TELUS CONDOS**  
DES MOINES, IA



**BUILDING #1 - FIRST FLOOR PLAN**  
SCALE: 1/8" = 1'-0"

- GENERAL FLOOR PLAN NOTES**
- REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINE DOCUMENTS.
  - ALL SITE INFO, FURNITURE, FIXTURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE TO BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO CIVIL, FIRE, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS AND CONSULT STATES FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
  - DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
  - ALL CONDITIONS, NOTES, WARNINGS, ETC. SHOULD BE HELD AS TIGHT AS POSSIBLE TO THE ROOF OR FLOOR DECK OR SHOULD BE KEPT WITHIN INTERIOR WALL CAVITIES. NOTHING SHALL BE BELOW THE BOTTOM EDGE OF MAIN BUILDING FRAMES. NOTHING SHALL BE EXPOSED IN FINISHED SPACES. NOTHING SHALL PREVENT A SMOOTH CONTINUOUS FINISH WALL OR CEILING AS NOTED ON THE DRAWINGS AND NO FINISHING RUN IN EXTERIOR WALLS UNLESS PRIOR APPROVAL IS GRANTED BY THE ARCHITECT OR OWNER. PLACEMENT COORDINATION BETWEEN TRADES IS REQUIRED.
  - RETAIN VERTICAL EXPANSION JOINTS MINIMAL AT COLUMN/BEAM LINES AND AT THE HEAD OF DOOR JAMBSPROFILES OR AS OTHERWISE NOTED ON WHERE REQUIRED PER BUILDING MATERIAL PRACTICE TO REDUCE STRESS CONCENTRATIONS. NO CONTROL JOINT/EXPANSION JOINT IN LOAD BEARING AREAS, STAY AWAY 2'-0" MIN.
  - CAULK & SEAL ALL CONTROL/EXPANSION, SAWCUT JOINTS AT ALL EXTERIOR & EXTERIOR MASONRY & CONCRETE.
  - PROVIDE LOWER LEVEL AND/OR UNDERFLOOR DRAINAGE SYSTEM TO REDUCE MOISTURE LEVELS AND PROTECT AGAINST MOISTURE DAMAGE. PROVIDE FLOOR/FOUNDATION DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURES AND PROTECT AGAINST MOISTURE DAMAGE. BOTH SYSTEMS SHALL DRAIN TO AN APPROPRIATE RECEIPT.
  - PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT PONDING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

**BUILDING #1 UNIT MIX:**

**FIRST FLOOR**

UNIT 1A	2
UNIT 1B	2
UNIT 2A	1
UNIT 2B	1
UNIT 2C	1
TOTAL	7 (21 REQ GARAGES, 1 ADA GARAGES)

**SECOND FLOOR**

UNIT 1A	4
UNIT 1B	4
UNIT 2A	2
UNIT 2B	2
UNIT 2C	2
TOTAL	18

**THIRD FLOOR**

UNIT 1A	4
UNIT 1B	4
UNIT 2A	2
UNIT 2B	2
UNIT 2C	2
TOTAL	18

**TOTALS**

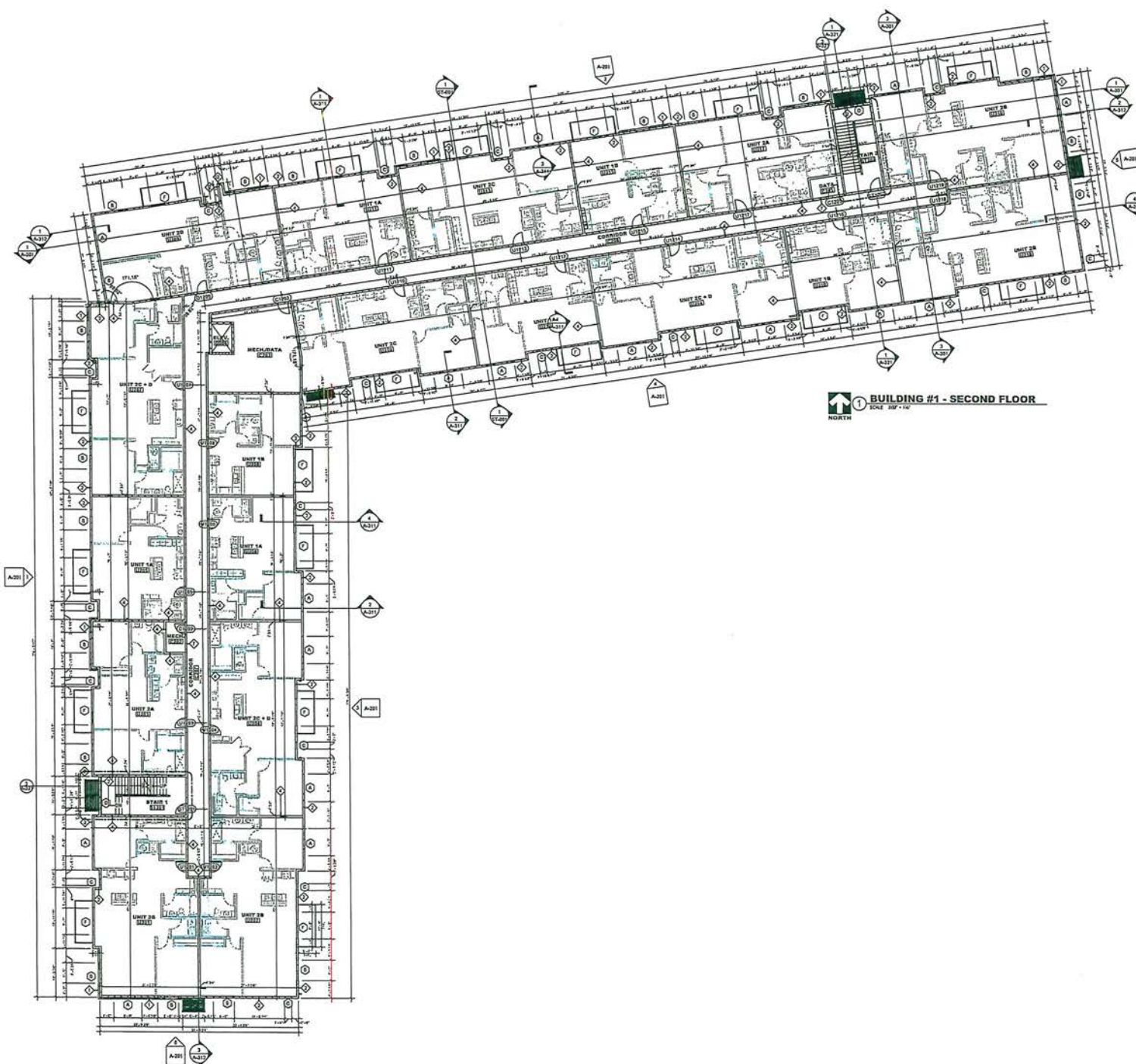
UNIT 1A	10	17 1-BEDROOMS = 75%
UNIT 1B	10	
UNIT 2A	4	
UNIT 2B	4	
UNIT 2C	4	
TOTAL	36	(21 GARAGES)



**TELUS CONDOS, IA**  
DES MOINES, IA

DATE	10/24/2023
BY	10/24/2023
PROJECT	TELUS CONDOS
SCALE	1/8" = 1'-0"
DATE	10/24/2023
BY	10/24/2023
PROJECT	TELUS CONDOS
SCALE	1/8" = 1'-0"

DATE: 10/24/23  
SCALE: 1/8" = 1'-0"  
PROJECT: TELUS CONDOS  
SCALE: 1/8" = 1'-0"  
DATE: 10/24/23  
SCALE: 1/8" = 1'-0"  
PROJECT: TELUS CONDOS  
SCALE: 1/8" = 1'-0"



**BUILDING #1 - SECOND FLOOR**  
 SCALE: 1/8" = 1'-0"  
 NORTH

**GENERAL FLOOR PLAN NOTES**

1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DRAWINGS AND OTHER DOCUMENTS.
2. ALL SITE INFO, FURNITURE, FIXTURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO CIVIL, FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS FOR CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
4. ALL CONCRETE, PIPES, WIRING, ROUSING, ETC. SHOULD BE HELD AS TIGHT AS POSSIBLE TO THE ROOF OR FLOOR DECK OR SHOULD BE KEPT WITHIN INTERIOR WALL CASINGS. NOTHING SHALL BE BELOW THE BOTTOM EDGE OF WALL BUILDING PANELS. NOTHING SHALL BE EXPOSED IN FINISHED SPACES. NOTHING SHALL PENETRATE A SMOOTH CONTIGUOUS FINISHED WALL OR CEILING UNLESS NOTED ON THE DRAWINGS AND NO PLUMBING RUN IN EXTERIOR WALLS UNLESS PRIOR APPROVAL IS GRANTED BY THE ARCHITECT OR OWNER. PLACEMENT COORDINATION WITH OTHER TRADES IS REQUIRED.
5. HORIZONTAL VERTICAL EXPANSION JOINTS SHALL BE AT COLUMN/BEAM LINES AND AT THE HEAD OF DOORS. JAWSPANNERS OR AS OTHERWISE NOTED OR UNLESS REQUIRED PER STANDARD FACTORY PRACTICE TO REDUCE STRESS BRACING AND CONTROL JOINT/EXPANSION JOINT IN LOAD BEARING AREAS, STAY AWAY 2'-0" MIN.
6. CALLK & SEAL ALL CONTROL-EXPANSION, SAWCUT JOINTS AT ALL INTERIOR & EXTERIOR MASONRY & CONCRETE.
7. PROVIDE LOWER LEVEL MOISTURE UNDERFLOOR DRAINAGE SYSTEM TO REDUCE PROTECTING UNIT AND PROTECT AGAINST MOISTURE SEEPAGE. PROVIDE PROTECTIVE WALL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURES AND PROTECT AGAINST MOISTURE SEEPAGE. BOTH SYSTEMS SHALL OUTLET TO APPROPRIATE SLOPE.
8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT POOLING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

**BUILDING #1 UNIT MIX:**

FIRST FLOOR	
UNIT 1A	1
UNIT 1B	1
UNIT 2A	1
UNIT 2B	1
UNIT 2C-D	1
UNIT 2E	1
TOTAL	6 (3 REG GARAGES, 3 ADA GARAGES)

**SECOND FLOOR**

UNIT 1A	4
UNIT 1B	4
UNIT 2A	4
UNIT 2B	4
UNIT 2C-D	4
UNIT 2E	4
TOTAL	24

**THIRD FLOOR**

UNIT 1A	4
UNIT 1B	4
UNIT 2A	4
UNIT 2B	4
UNIT 2C-D	4
UNIT 2E	4
TOTAL	24

**TOTAL**

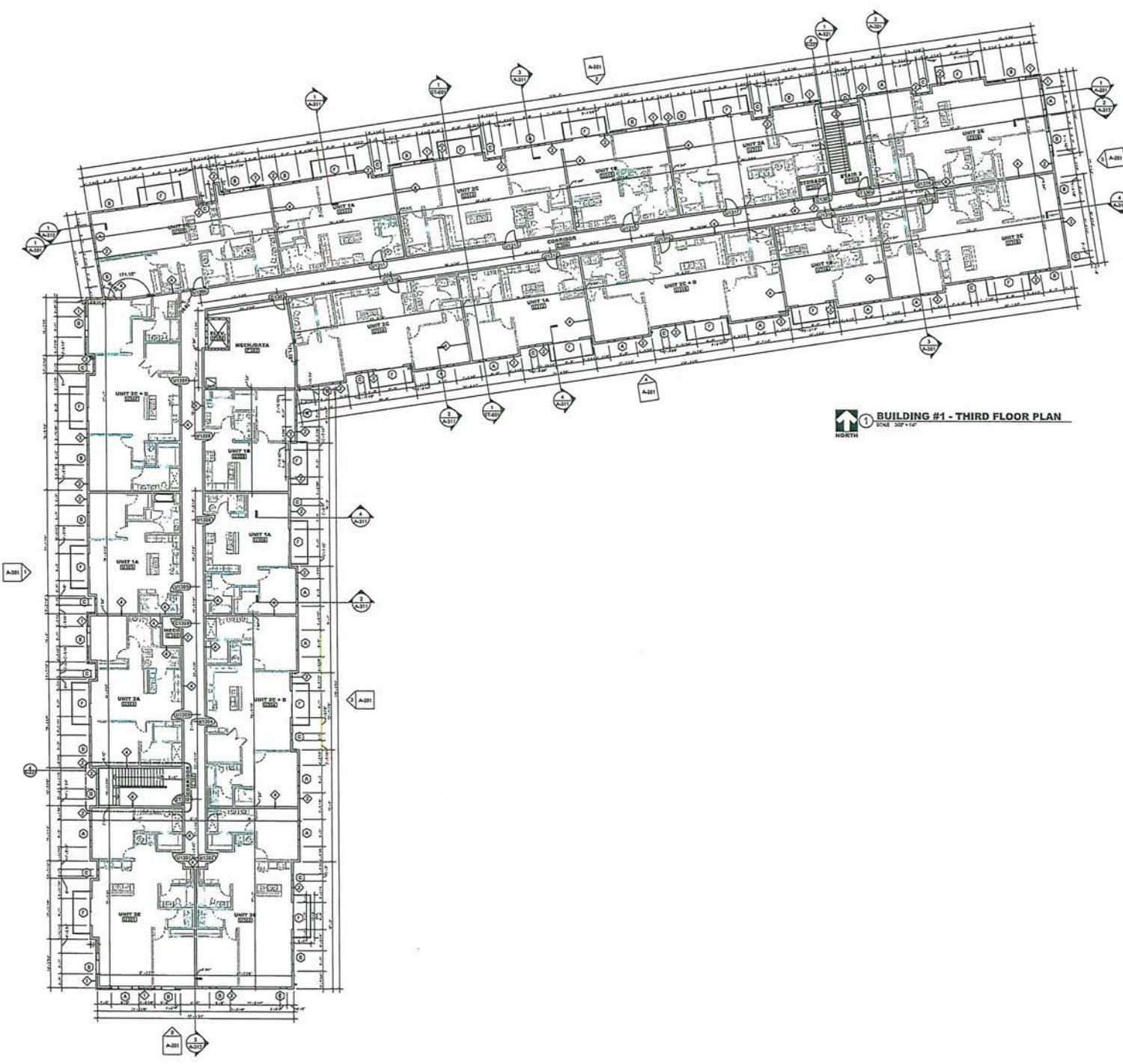
UNIT 1A	10	17 3-BEDROOMS + 13%
UNIT 1B	10	
UNIT 2A	10	
UNIT 2B	10	
UNIT 2C-D	10	
UNIT 2E	10	
TOTAL	60	48 (32 GARAGES)



**TELUS CONDOS**  
 DES MOINES, IA

NO.	DATE	BY	REVISION





**BUILDING #1 - THIRD FLOOR PLAN**  
 NORTH  
 SCALE 1/8" = 1'-0"

- GENERAL FLOOR PLAN NOTES**
1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER SURVEY DOCUMENTS.
  2. ALL SITE INFO, FURNITURE, FIXTURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO CIVIL, FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS AND CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
  3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
  4. ALL CORNERS, PIPES, WIRING, RODS, ETC. SHOULD BE HELD AS TIGHT AS POSSIBLE TO THE ROOF OR FLOOR DECK OR SHOULD BE KEPT WITHIN INTERIOR WALL CAVITIES. NOTHING SHALL BE EXPOSED TO THE BOTTOM EDGE OF MARK FLOORING FRAMES. NOTHING SHALL BE EXPOSED IN FINISHED SPACES. NOTHING SHALL PREVENT A SMOOTH CONTINUOUS FINISH WALL OR CEILING AS NOTED ON THE DRAWINGS AND NO PLUMBING RUN IN EXTERIOR WALLS UNLESS PRIOR APPROVAL IS GRANTED BY THE ARCHITECT OR OWNER. PLACE/HEIGHT COORDINATION BETWEEN TRADES IS REQUIRED.
  5. NETIAL VERTICAL EXPANSION JOINT MATERIAL AT COLUMN/BEAM LINES AND AT THE HEAD OF SCOR ANCHORING/RODS OR AS OTHERWISE NOTED OR WHEN REQUIRED PER STANDARD MATERIAL PRACTICE TO REDUCE STRESS CRACKING. USE CONTROL JOINT/EXPANSION JOINT IN LOAD BEARING AREAS, STAY AWAY 2'-0" MIN.
  6. CABLE & SEAL ALL CONTROL/EXPANSION JOINTS AT ALL INTERIOR & EXTERIOR MASONRY & CONCRETE.
  7. PROVIDE LOWER LEVEL AND/OR UNDERFLOOR DRAINAGE SYSTEM TO REDUCE FLOODING AND/OR WATER DAMAGE TO INTERIOR FINISHES. PROVIDE FOUNDATION WALL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURES AND PROTECT AGAINST MOISTURE SEEPAGE. SODS SYSTEMS SHALL OUTLET TO APPROPRIATE SITES.
  8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT POOLING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

**BUILDING #1 UNIT MIX:**

FIRST FLOOR	
UNIT 1A	1
UNIT 1B	1
UNIT 1C	1
UNIT 1D	1
UNIT 1E	1
UNIT 1F	1
UNIT 1G	1
UNIT 1H	1
UNIT 1I	1
UNIT 1J	1
UNIT 1K	1
UNIT 1L	1
UNIT 1M	1
UNIT 1N	1
UNIT 1O	1
UNIT 1P	1
UNIT 1Q	1
UNIT 1R	1
UNIT 1S	1
UNIT 1T	1
UNIT 1U	1
UNIT 1V	1
UNIT 1W	1
UNIT 1X	1
UNIT 1Y	1
UNIT 1Z	1
TOTAL	18 (01 REG GARAGES, 1 ADA GARAGE)

SECOND FLOOR	
UNIT 2A	1
UNIT 2B	1
UNIT 2C	1
UNIT 2D	1
UNIT 2E	1
UNIT 2F	1
UNIT 2G	1
UNIT 2H	1
UNIT 2I	1
UNIT 2J	1
UNIT 2K	1
UNIT 2L	1
UNIT 2M	1
UNIT 2N	1
UNIT 2O	1
UNIT 2P	1
UNIT 2Q	1
UNIT 2R	1
UNIT 2S	1
UNIT 2T	1
UNIT 2U	1
UNIT 2V	1
UNIT 2W	1
UNIT 2X	1
UNIT 2Y	1
UNIT 2Z	1
TOTAL	18

THIRD FLOOR	
UNIT 3A	1
UNIT 3B	1
UNIT 3C	1
UNIT 3D	1
UNIT 3E	1
UNIT 3F	1
UNIT 3G	1
UNIT 3H	1
UNIT 3I	1
UNIT 3J	1
UNIT 3K	1
UNIT 3L	1
UNIT 3M	1
UNIT 3N	1
UNIT 3O	1
UNIT 3P	1
UNIT 3Q	1
UNIT 3R	1
UNIT 3S	1
UNIT 3T	1
UNIT 3U	1
UNIT 3V	1
UNIT 3W	1
UNIT 3X	1
UNIT 3Y	1
UNIT 3Z	1
TOTAL	18

TOTALS	
UNIT 1A	18
UNIT 1B	18
UNIT 1C	18
UNIT 1D	18
UNIT 1E	18
UNIT 1F	18
UNIT 1G	18
UNIT 1H	18
UNIT 1I	18
UNIT 1J	18
UNIT 1K	18
UNIT 1L	18
UNIT 1M	18
UNIT 1N	18
UNIT 1O	18
UNIT 1P	18
UNIT 1Q	18
UNIT 1R	18
UNIT 1S	18
UNIT 1T	18
UNIT 1U	18
UNIT 1V	18
UNIT 1W	18
UNIT 1X	18
UNIT 1Y	18
UNIT 1Z	18
TOTAL	18 (01 GARAGES)

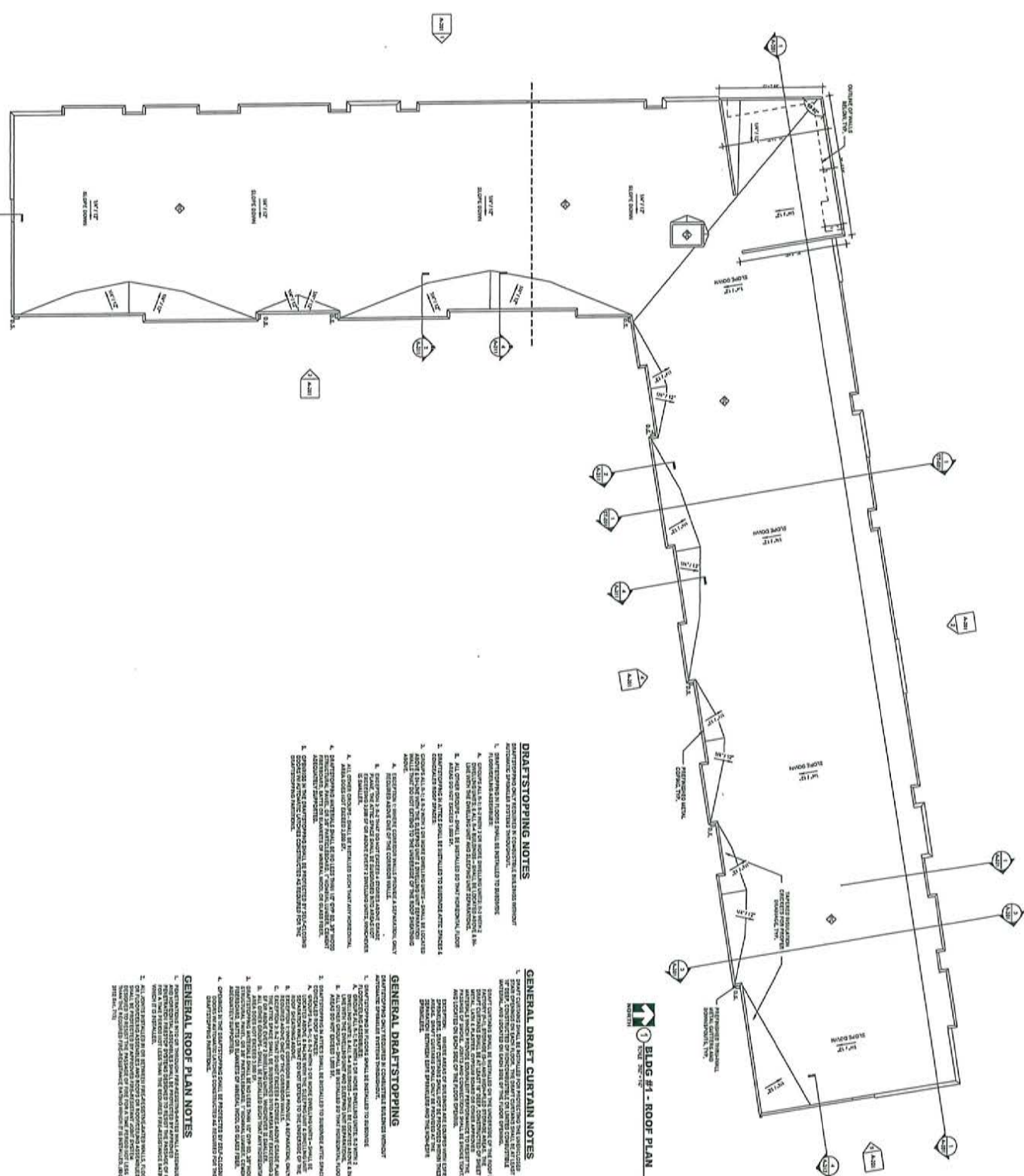
**SIMONSON**  
 SIMONSON ARCHITECTURAL CONTRACTORS LLC  
 1112 24th Street, Des Moines, IA 50319  
 515.281.2222

**Hubbell**  
 HUBBELL REALTY COMPANY

**TELUS CONDOS**  
 DES MOINES, IA

DATE	BY	REVISION
11/06/2019	MM	ISSUE FOR PERMIT
08/29/2019	MM	REVISED PER 2018 IBC
11/06/2019	MM	REVISED PER 2018 IBC

Job No. 21162  
 Sheet Title: BUILDING #1 - THIRD FLOOR PLAN  
 Sheet No. A-103



**DRAFTSTOPPING NOTES**

1. DRAFTSTOPPING WALLS SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
2. DRAFTSTOPPING WALLS SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
3. DRAFTSTOPPING WALLS SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
4. DRAFTSTOPPING WALLS SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
5. DRAFTSTOPPING WALLS SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.

**GENERAL DRAFT CURTAIN NOTES**

1. DRAFT CURTAINS SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
2. DRAFT CURTAINS SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
3. DRAFT CURTAINS SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
4. DRAFT CURTAINS SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
5. DRAFT CURTAINS SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.

**GENERAL DRAFTSTOPPING**

1. DRAFTSTOPPING WALLS SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
2. DRAFTSTOPPING WALLS SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
3. DRAFTSTOPPING WALLS SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
4. DRAFTSTOPPING WALLS SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
5. DRAFTSTOPPING WALLS SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.

**GENERAL ROOF PLAN NOTES**

1. ROOF PLAN SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
2. ROOF PLAN SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
3. ROOF PLAN SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
4. ROOF PLAN SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
5. ROOF PLAN SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.

**GENERAL ROOF PLAN NOTES**

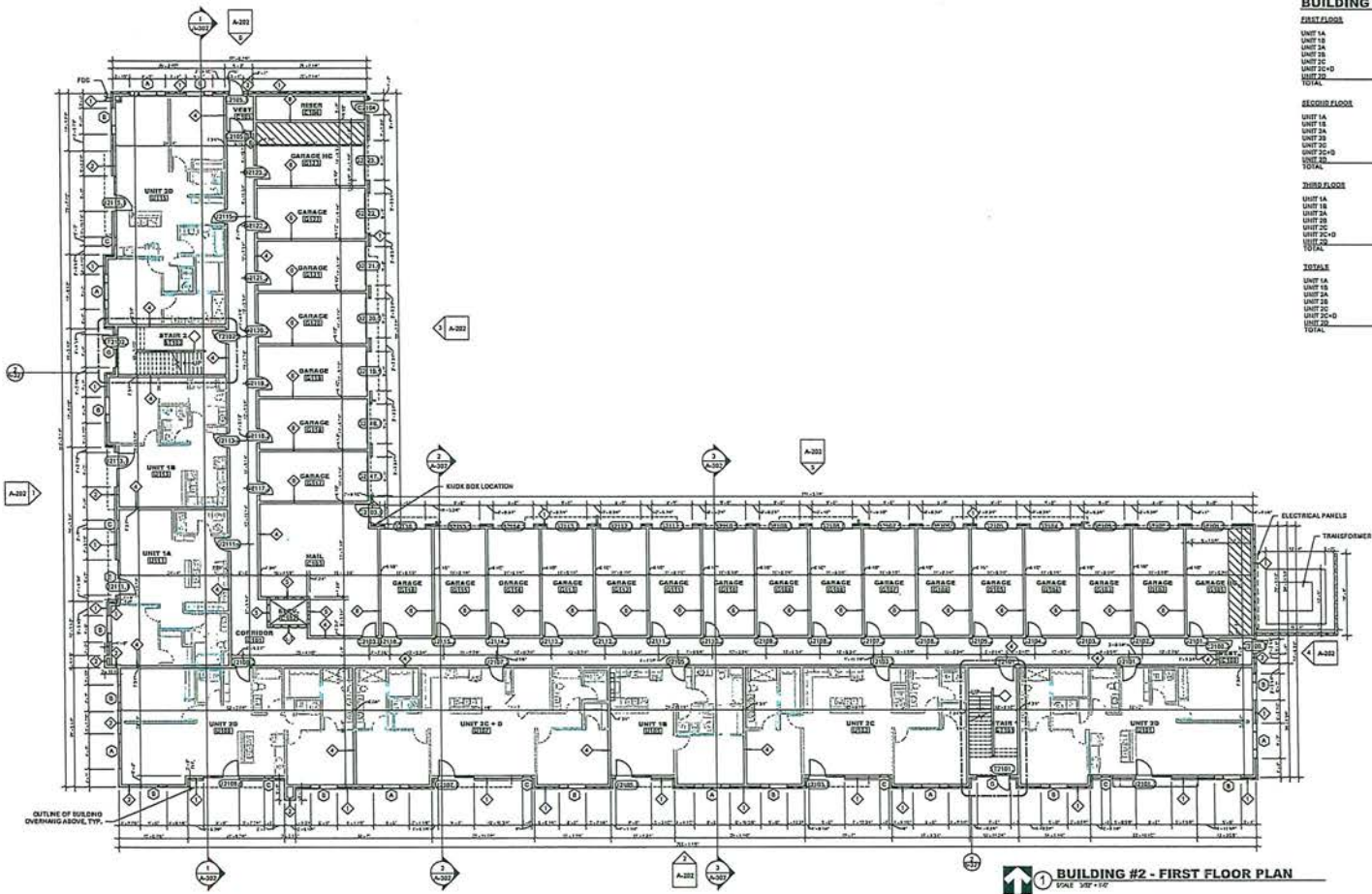
1. ALL ROOF PLAN SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
2. ALL ROOF PLAN SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
3. ALL ROOF PLAN SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
4. ALL ROOF PLAN SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.
5. ALL ROOF PLAN SHALL BE CONSTRUCTED WITH A MINIMUM OF 4" THICK CONCRETE WITH REINFORCING BARS AS SHOWN.

**TELUS CONDOS**  
DES MOINES, IA



**SIMONSON**  
PLANNING + ARCHITECTURE + INTERIORS  
Simonson + Associates Architects LLC  
1111 Grand Ave., Suite 100, Des Moines, IA 50319  
515.281.1111  
www.simonson.com

Project Name	TELUS CONDOS
Client	TELUS CONDOS
Architect	SIMONSON PLANNING + ARCHITECTURE + INTERIORS
Scale	AS SHOWN
Date	10/23/2024
Drawn By	JK
Checked By	JK
Project No.	22153
Sheet No.	27
Scale	AS SHOWN
Project Name	TELUS CONDOS
Client	TELUS CONDOS
Architect	SIMONSON PLANNING + ARCHITECTURE + INTERIORS
Scale	AS SHOWN
Date	10/23/2024
Drawn By	JK
Checked By	JK
Project No.	22153
Sheet No.	27
Scale	AS SHOWN



- GENERAL FLOOR PLAN NOTES**
1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER OCCUPANCY DOCUMENTS.
  2. ALL SITE INFO, FURNITURE, FIXTURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND ANY HAVE TO BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO CIVIL, FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS AND CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
  3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
  4. ALL CONDUITS, PIPES, WIRING, ROUGH-IN, ETC. SHOULD BE HELD AS TIGHT AS POSSIBLE TO THE ROOF OR FLOOR DECK OR SHOULD BE LEFT WITHIN INTERIOR WALL CAVITIES. NOTHING SHALL BE BELOW THE BOTTOM EDGE OF MAIN BUILDING FRAMES. NOTHING SHALL BE EXPOSED IN FINISHED SPACES. NOTHING SHALL PREVENT A SMOOTH CONTINUOUS FINISHED WALL OR CEILING AS NOTED ON THE DRAWINGS AND NO PLUMBING SHALL BE EXPOSED UNLESS PRIOR APPROVAL IS GRANTED BY THE ARCHITECT OR OWNER. PLACEMENT COORDINATION BETWEEN TRADES IS REQUIRED.
  5. INSTALL VERTICAL EXPANSION JOINT MINIMAL AT COLUMN/BEAM LINES AND AT THE HEAD OF BEAM/JAMB/OPENINGS OR AS OTHERWISE NOTED OR WHERE REQUIRED PER STANDARD MATERIAL PRACTICE TO REDUCE STRESS CRACKING. NO CONTROL JOINT/EXPANSION JOINT IN LOAD BEARING AREAS. STAY AWAY 2'-0" MIN.
  6. CAULK & SEAL ALL CONTROL-EXPANSION, SAVICUT JOINTS AT ALL INTERIOR & EXTERIOR MASONRY & CONCRETE.
  7. PROVIDE LOWER LEVEL AND/OR UNDERFLOOR DRAINAGE SYSTEM TO REDUCE MOISTURE TO UNIT AND PROTECT AGAINST MOISTURE SEEPAGE. PROVIDE FOUNDATION WALL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURE AND PROTECT AGAINST MOISTURE SEEPAGE. BOTH SYSTEMS SHALL OUTLET TO APPROPRIATE RELIEF.
  8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT FLOWING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

**BUILDING #2 UNIT MIX:**

**FIRST FLOOR**

UNIT 1A	1
UNIT 2A	1
UNIT 2B	1
UNIT 2C	1
UNIT 2D	1
UNIT 2E	1
TOTAL	6 (11 REG. GARAGES, 2 ADA GARAGES)

**SECOND FLOOR**

UNIT 1A	1
UNIT 1B	1
UNIT 2B	1
UNIT 2C	1
UNIT 2D	1
UNIT 2E	1
TOTAL	6

**THIRD FLOOR**

UNIT 1A	1
UNIT 1B	1
UNIT 2B	1
UNIT 2C	1
UNIT 2D	1
UNIT 2E	1
TOTAL	6

**TOTAL**

UNIT 1A	3	11 BEDROOMS + 20%
UNIT 2A	1	
UNIT 2B	1	
UNIT 2C	1	
UNIT 2D	1	
UNIT 2E	1	
TOTAL	12	12 GARAGES

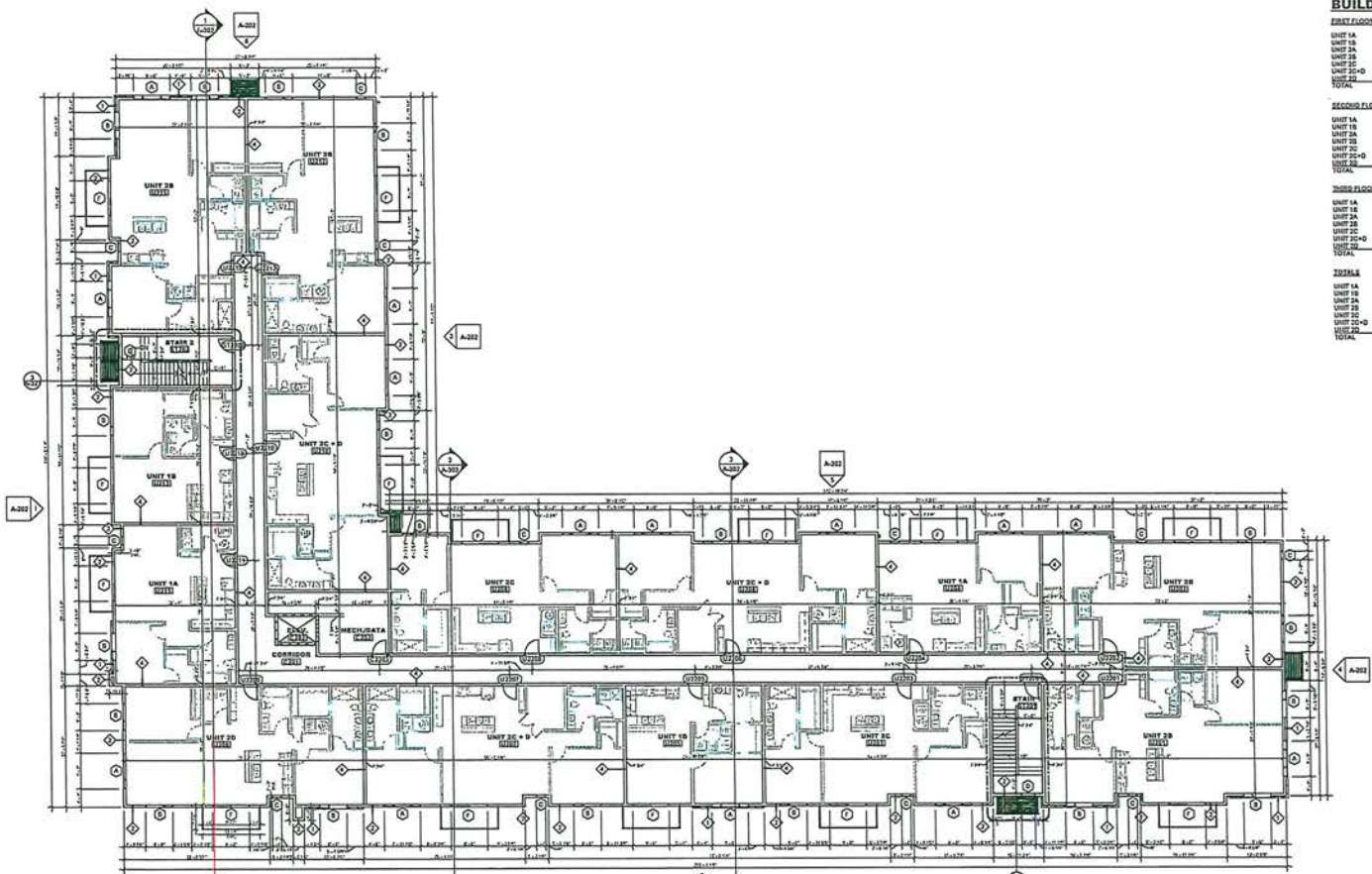
**1 BUILDING #2 - FIRST FLOOR PLAN**  
SCALE 1/8" = 1'-0"



**TELUS CONDOS**  
DES MOINES, IA

DATE	10/20/2022
BY	MATT FRENCH
NO.	21162
PROJECT	BUILDING #2 - FIRST FLOOR PLAN
SCALE	1/8" = 1'-0"

NO. 21162  
SP  
DATE: 10/20/2022  
PROJECT: BUILDING #2 - FIRST FLOOR PLAN  
SCALE: 1/8" = 1'-0"  
**A-111**



**1 BUILDING #2 - SECOND FLOOR PLAN**  
 SCALE: 3/8" = 1'-0"

**GENERAL FLOOR PLAN NOTES**

1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINE DOCUMENTS.
2. ALL SITE INFO, FURNITURE, FIXTURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE TO BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO CIVIL, FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS OR CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
4. ALL CONDUITS, PIPES, WIRING, ROUGH-IN, ETC. SHOULD BE HELD AS TIGHT AS POSSIBLE TO THE ROOF OR FLOOR DECK OR SHOULD BE KEPT WITHIN INTERIOR WALL CAVITIES. NOTHING SHALL BE BELOW THE BOTTOM EDGE OF MAIN BUILDING FRAME. NOTING SHALL BE EXPRESSED BY FINISHING SPACES. NOTHING SHALL PREVENT A SMOOTH CONTINUOUS FINISHED WALL OR CEILING AS NOTED ON THE DRAWINGS AND NO PLUMBING SHALL IN EXTERIOR WALLS UNLESS PRIOR APPROVAL IS GRANTED BY THE ARCHITECT OR OWNER. PLACEMENT COORDINATION BETWEEN TRADES IS REQUIRED.
5. INSTALL VERTICAL EXPANSION JOINTS PERMANENT AT COLUMN GRID LINES AND AT THE HEAD OF BEAMS. JAMES SUPERFICES OR AS OTHERWISE NOTED OR WHERE REQUIRED PER STANDARD MATERIAL PRACTICE TO REDUCE STRESS CRACKING. NO CONTROL JOINT/EXPANSION JOINT IN LOAD BEARING AREAS. STAY AWAY 2'-0" MIN.
6. CAULK & SEAL ALL CONTROL-EXPANSION, SAWCUT JOINTS AT ALL EXTERIOR & EXTERIOR MASONRY & CONCRETE.
7. PROVIDE LOWER LEVEL AND/OR UNDERFLOOR DRAINAGE SYSTEM TO REDUCE HYDROSTATIC UPLIFT AND PROTECT AGAINST MOISTURE SEEPAGE. PROVIDE FOUNDATION WALL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURES AND PROTECT AGAINST MOISTURE SEEPAGE. BOTH SYSTEMS SHALL OUTLET TO APPROPRIATE RELIEF.
8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATERS AWAY FROM BUILDING WITHOUT PONDING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

**BUILDING #2 UNIT MIX:**

**UNIT FLOOR**

UNIT 1A	1
UNIT 1B	1
UNIT 1C	1
UNIT 1D	1
UNIT 1E	1
UNIT 1F	1
UNIT 1G	1
UNIT 1H	1
UNIT 1I	1
UNIT 1J	1
UNIT 1K	1
UNIT 1L	1
UNIT 1M	1
UNIT 1N	1
UNIT 1O	1
UNIT 1P	1
UNIT 1Q	1
UNIT 1R	1
UNIT 1S	1
UNIT 1T	1
UNIT 1U	1
UNIT 1V	1
UNIT 1W	1
UNIT 1X	1
UNIT 1Y	1
UNIT 1Z	1
TOTAL	20 (20 RECL GARAGE, 2 ADA GARAGES)

**SECOND FLOOR**

UNIT 1A	2
UNIT 1B	2
UNIT 1C	2
UNIT 1D	2
UNIT 1E	2
UNIT 1F	2
UNIT 1G	2
UNIT 1H	2
UNIT 1I	2
UNIT 1J	2
UNIT 1K	2
UNIT 1L	2
UNIT 1M	2
UNIT 1N	2
UNIT 1O	2
UNIT 1P	2
UNIT 1Q	2
UNIT 1R	2
UNIT 1S	2
UNIT 1T	2
UNIT 1U	2
UNIT 1V	2
UNIT 1W	2
UNIT 1X	2
UNIT 1Y	2
UNIT 1Z	2
TOTAL	34

**THIRD FLOOR**

UNIT 1A	3
UNIT 1B	3
UNIT 1C	3
UNIT 1D	3
UNIT 1E	3
UNIT 1F	3
UNIT 1G	3
UNIT 1H	3
UNIT 1I	3
UNIT 1J	3
UNIT 1K	3
UNIT 1L	3
UNIT 1M	3
UNIT 1N	3
UNIT 1O	3
UNIT 1P	3
UNIT 1Q	3
UNIT 1R	3
UNIT 1S	3
UNIT 1T	3
UNIT 1U	3
UNIT 1V	3
UNIT 1W	3
UNIT 1X	3
UNIT 1Y	3
UNIT 1Z	3
TOTAL	54

**TOTALS**

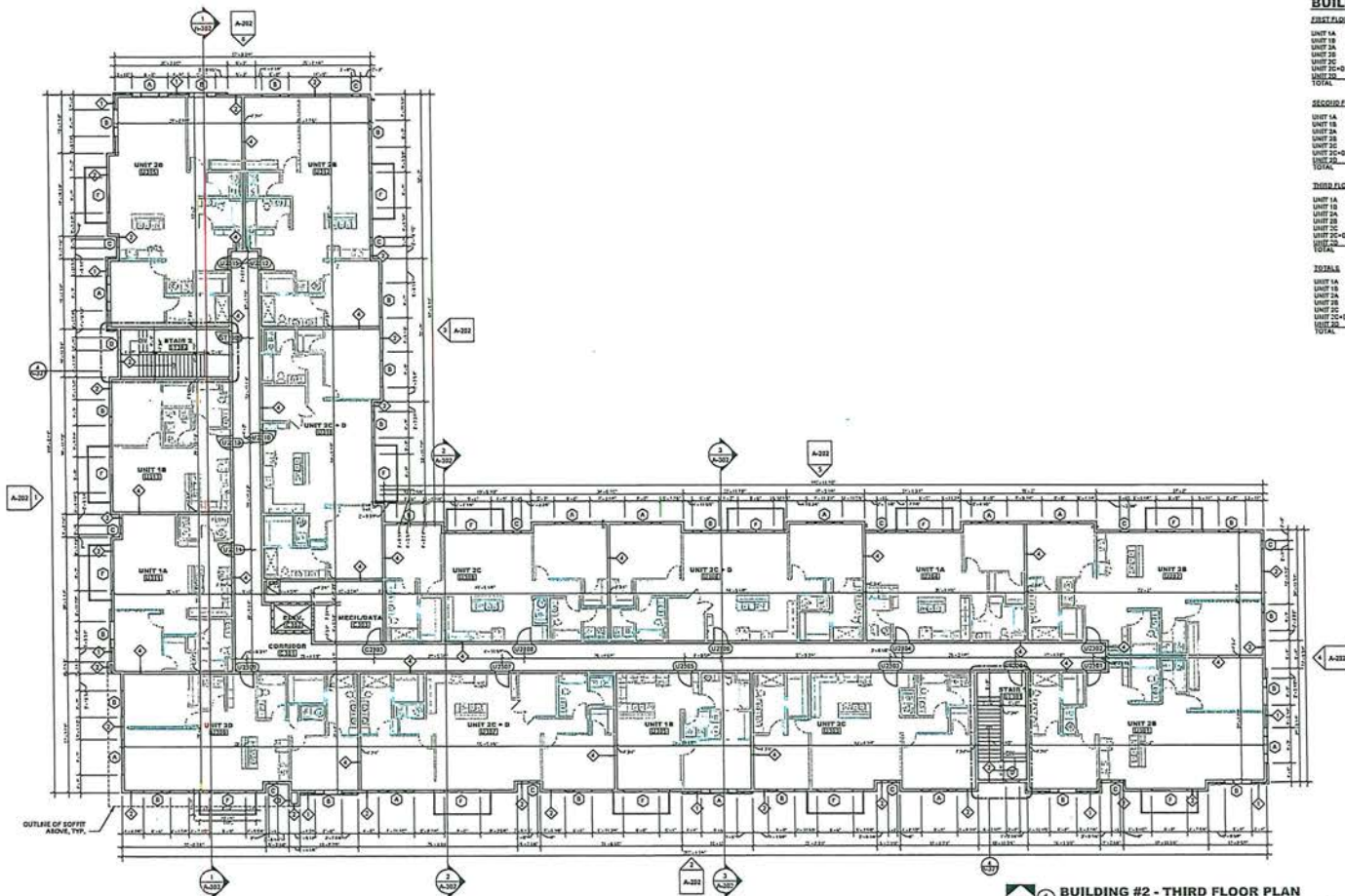
UNIT 1A	5	11 1-BEDROOMS + 20%
UNIT 1B	5	
UNIT 1C	5	
UNIT 1D	5	
UNIT 1E	5	
UNIT 1F	5	
UNIT 1G	5	
UNIT 1H	5	
UNIT 1I	5	
UNIT 1J	5	
UNIT 1K	5	
UNIT 1L	5	
UNIT 1M	5	
UNIT 1N	5	
UNIT 1O	5	
UNIT 1P	5	
UNIT 1Q	5	
UNIT 1R	5	
UNIT 1S	5	
UNIT 1T	5	
UNIT 1U	5	
UNIT 1V	5	
UNIT 1W	5	
UNIT 1X	5	
UNIT 1Y	5	
UNIT 1Z	5	
TOTAL	54	20 (20 GARAGES)



**TELUS CONDOS**  
 DES MOINES, IA

FILE NO.	15-03-0000
DATE	10/20/2015
PROJECT	TELUS CONDOS
UNIT/FLOOR	2ND FLOOR
DESCRIPTION	SECOND FLOOR PLAN

DATE: 10/20/2015  
 TIME: 5P  
 PROJECT: BUILDING #2 - SECOND FLOOR PLAN  
 DRAWING: A-112



**GENERAL FLOOR PLAN NOTES**

1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINE DOCUMENTS.
2. ALL SITE INFO, FURNITURE, FIXTURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE TO BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO CHL, FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS AND CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
4. ALL CONDUITS, PIPES, WIRING, ROUGH-IN, ETC. SHOULD BE HELD AS TIGHT AS POSSIBLE TO THE ROOF OR FLOOR DECK OR SHOULD BE KEPT WITHIN INTERIOR WALL COURSES. SOFFITS SHALL BE BELOW THE BOTTOM EDGE OF MAIN BUILDING FRAMES. SOFFITS SHALL BE COVERED BY FINISHED SPACES. NOTHING SHALL PREVENT A SMOOTH CONTINUOUS FINISHED WALL OR CEILING AS NOTED ON THE DRAWING AND NO PLUMBING SHALL EXTERIOR WALLS UNLESS PRIOR APPROVAL IS OBTAINED BY THE ARCHITECT OR OWNER. PLACEMENT COORDINATION BETWEEN TRADES IS REQUIRED.
5. INSTALL VERTICAL EXPANSION JOINT MINIMAL AT COLUMN/ROOF LINES AND AT THE HEAD OF SCORING AND PARTITION OR AS OTHERWISE NOTED ON OTHERS. REQUIRE PER STANDARD MATERIAL PRACTICE TO REDUCE STRESS CRACKING. NO CONTROL JOINT/EXPANSION JOINT IN LOAD BEARING AREAS, EXIST AWAY 2'-4" MIN.
6. CAULK & SEAL ALL CONTROL-EXPANSION, SAWCUT JOINTS AT ALL INTERIOR & EXTERIOR BRACKERY & CONCRETE.
7. PROVIDE LOWER LEVEL AND/OR UNDERFLOOR DRAINAGE SYSTEM TO REDUCE HYDROSTATIC UPLIFT AND PROTECT AGAINST MOISTURE SEEPAGE. PROVIDE FLOOR/WALL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURES AND PROTECT AGAINST MOISTURE SEEPAGE. BOTH SYSTEMS SHALL OUTLET TO APPROPRIATE RELIEF.
8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT PONDING OF WATER ADJACENT TO BUILDING OR ON PARAPETS.

**BUILDING #2 UNIT MIX:**

FIRST FLOOR	
UNIT 1A	1
UNIT 1B	2
UNIT 2A	1
UNIT 2B	1
UNIT 2C	1
UNIT 2D	1
UNIT 2E	1
UNIT 2F	1
TOTAL	8 (21 REG. GARAGES, 2 ADA GARAGES)
SECOND FLOOR	
UNIT 1A	2
UNIT 1B	2
UNIT 2A	2
UNIT 2B	4
UNIT 2C	3
UNIT 2D	3
UNIT 2E	3
UNIT 2F	3
TOTAL	18
THIRD FLOOR	
UNIT 1A	2
UNIT 1B	2
UNIT 2A	2
UNIT 2B	2
UNIT 2C	2
UNIT 2D	2
UNIT 2E	2
UNIT 2F	2
TOTAL	14
TOTALS	
UNIT 1A	5
UNIT 1B	5
UNIT 2A	5
UNIT 2B	11
UNIT 2C	7
UNIT 2D	7
UNIT 2E	7
UNIT 2F	7
TOTAL	58 (11 1-BEDROOMS + 20K)
TOTAL	88 (23 GARAGES)

**1 BUILDING #2 - THIRD FLOOR PLAN**  
SCALE 3/8" = 1'-0"



**TELUS CONDOS, IA**  
DES MOINES, IA

DATE	10/20/2015
BY	10/20/2015
REVISION	
DATE	
BY	
REVISION	
DATE	
BY	
REVISION	

Job No. 21162  
Sheet No. 8P  
Project: BUILDING #2 - THIRD FLOOR PLAN

Scale: A-113

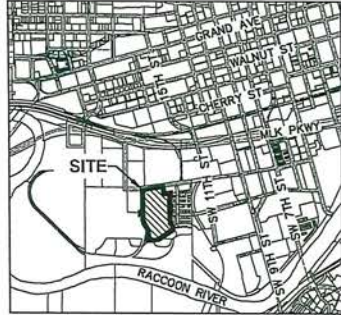




# PRELIMINARY PLAT / DEVELOPMENT PLAN FOR: GRAY'S STATION PLAT 5

## DES MOINES, IOWA

### VICINITY MAP NOT TO SCALE



DES MOINES, IOWA

### OWNER

HRC NPS I LLC  
6900 WESTOWN PKWY  
WEST DES MOINES, IA 50268

### APPLICANT

HUBBELL REALTY COMPANY  
6900 WESTOWN PKWY  
WEST DES MOINES, IA 50268  
CONTACT: CALEB SMITH  
PH. (515) 243-3228

### ENGINEER

CIVIL DESIGN ADVANTAGE  
4121 NW URBANDALE DRIVE  
URBANDALE, IA 50322  
CONTACT: RYAN HARDISTY  
EMAIL: RYAN@CDA-ENG.COM  
PH. (515) 399-4400

### SURVEYOR

CIVIL DESIGN ADVANTAGE, LLC  
4121 NW URBANDALE DRIVE  
URBANDALE, IA 50322  
CONTACT: CHARLIE MCLOTHLEN  
EMAIL: CHARLIE@CDA-ENG.COM  
PH. (515) 399-4400

### DATE OF SURVEY

08/05/2022

### BENCHMARKS

- CITY BENCHMARK #725: BRASS CAP IN THE NORTHEAST TRAFFIC SIGNAL BASE AT THE SOUTHWEST CORNER OF MARTIN LUTHER KING DRIVE AND SOUTHWEST 11TH STREET. ELEVATION=28.61
- CITY BENCHMARK #5981: BRASS TABLET IN THE CONCRETE WALL 43.6 FEET WEST OF THE EAST FACE NEAR THE SANITARY SEWER PUMP STATION. ELEVATION=25.46

### SUBMITTAL DATES

FIRST SUBMITTAL: 10/19/2022  
SECOND SUBMITTAL: 01/04/2023

### LEGAL DESCRIPTION

A PART OF LOTS 6, 7, AND 9 CENTRAL DES MOINES INDUSTRIAL PARK AND A PART OF PARCELS 2018-1467 OF SAID LOTS 6, 7, AND 9, AS SHOWN ON THE PLAT OF SURVEY RECORDED IN BOOK 17873, PAGE 735, BOTH OFFICIAL PLATS IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF LOT 12, GRAY'S STATION PLAT 2, AN OFFICIAL PLAT; THENCE SOUTH 002°24' EAST ALONG THE WEST LINE OF SAID LOT 12; A DISTANCE OF 145.50 FEET TO THE SOUTHWEST CORNER OF SAID LOT 12; THENCE NORTH 81°29'18" EAST ALONG THE SOUTHERLY LINE OF SAID LOT 12; A DISTANCE OF 128.11 FEET TO THE NORTHWEST CORNER OF OUTLOT "Y", SAID GRAY'S STATION PLAT 2; THENCE SOUTH 00°01'40" EAST ALONG THE WESTERLY LINE OF SAID OUTLOT "Y" AND THE WESTERLY LINE OF GRAY'S STATION PLAT 3, AN OFFICIAL PLAT, 85.73 FEET; THENCE SOUTH 89°58'20" WEST CONTINUING ALONG SAID WESTERLY LINE OF SAID GRAY'S STATION PLAT 3, A DISTANCE OF 27.77 FEET; THENCE SOUTH 00°01'40" EAST CONTINUING ALONG SAID WESTERLY LINE, 107.81 FEET; THENCE SOUTH 89°58'20" WEST CONTINUING ALONG SAID WESTERLY LINE, 6.23 FEET; THENCE SOUTH 00°01'40" EAST CONTINUING ALONG SAID WESTERLY LINE AND THE WESTERLY LINE OF GRAY'S STATION PLAT 4, AN OFFICIAL PLAT, 122.09 FEET; THENCE NORTH 89°58'20" EAST CONTINUING ALONG SAID WESTERLY LINE OF SAID GRAY'S STATION PLAT 4, A DISTANCE OF 17.81 FEET; THENCE SOUTH 00°01'40" EAST CONTINUING ALONG SAID WESTERLY LINE, 83.00 FEET; THENCE SOUTH 89°58'20" WEST CONTINUING ALONG SAID WESTERLY LINE, 8.11 FEET; THENCE SOUTH 00°01'40" EAST CONTINUING ALONG SAID WESTERLY LINE, 146.23 FEET; THENCE SOUTHERLY CONTINUING ALONG SAID WESTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 80.00 FEET, WHOSE ARC LENGTH IS 56.39 FEET AND WHOSE CHORD BEARS SOUTH 20°09'18" WEST, 55.20 FEET; THENCE SOUTH 49°30'41" EAST CONTINUING ALONG SAID WESTERLY LINE, 82.02 FEET; THENCE SOUTH 20°22'59" EAST CONTINUING ALONG SAID WESTERLY LINE, 105.01 FEET TO THE SOUTHWEST CORNER OF STREET LOT "A", SAID GRAY'S STATION PLAT 4; THENCE SOUTHERLY ALONG A CURVE CONCAVE SOUTHEASTERLY WHOSE RADIUS IS 298.00 FEET, WHOSE ARC LENGTH IS 32.25 FEET AND WHOSE CHORD BEARS SOUTH 57°30'24" WEST, 32.34 FEET; THENCE SOUTH 54°23'46" WEST, 249.63 FEET; THENCE WESTERLY ALONG A CURVE CONCAVE NORTHERLY WHOSE RADIUS IS 368.00 FEET, WHOSE ARC LENGTH IS 302.72 FEET AND WHOSE CHORD BEARS SOUTH 77°37'44" WEST, 294.28 FEET; THENCE NORTH 79°28'18" WEST, 26.20 FEET; THENCE NORTH 11°31'42" EAST, 70.00 FEET; THENCE SOUTH 78°28'18" EAST, 22.83 FEET; THENCE NORTH 00°08'11" WEST, 61.22 FEET; THENCE NORTHERLY ALONG A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 224.00 FEET, WHOSE ARC LENGTH IS 131.34 FEET AND WHOSE CHORD BEARS NORTH 16°36'00" WEST, 128.48 FEET; THENCE NORTH 33°43'49" WEST, 43.96 FEET; THENCE NORTHERLY ALONG A CURVE CONCAVE EASTERLY WHOSE RADIUS IS 278.00 FEET, WHOSE ARC LENGTH IS 181.83 FEET AND WHOSE CHORD BEARS NORTH 16°56'00" WEST, 159.52 FEET; THENCE NORTH 00°08'11" WEST, 58.59 FEET; THENCE NORTH 89°18'18" WEST, 43.82 FEET; THENCE NORTH 00°40'42" EAST, 52.00 FEET TO THE SOUTH LINE OF LOT 8, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°18'18" EAST ALONG SAID SOUTH LINE, 43.18 FEET TO THE SOUTHEAST CORNER OF SAID LOT 8; THENCE NORTH 00°08'11" WEST ALONG THE EAST LINE OF SAID LOT 8, A DISTANCE OF 333.17 FEET; THENCE NORTHERLY CONTINUING ALONG SAID EAST LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 804.49 FEET, WHOSE ARC LENGTH IS 84.04 FEET AND WHOSE CHORD BEARS NORTH 03°28'30" WEST, 83.89 FEET; THENCE NORTH 06°16'08" WEST CONTINUING ALONG SAID EAST LINE, 17.29 FEET TO THE NORTHEAST CORNER OF SAID LOT 8, ALSO BEING A POINT ON THE SOUTHERLY LINE OF TUTTLE STREET RIGHT OF WAY; THENCE NORTH 81°10'49" EAST ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 411.78 FEET TO THE POINT OF BEGINNING AND CONTAINING 11.47 ACRES (499,715 SQUARE FEET).

THE PROPERTY IS SUBJECT TO ANY AND ALL EASEMENTS OF RECORD.

### ZONING

GRAY'S STATION PUD - ZON2017-00087

### EXISTING/ PROPOSED USE

EXISTING USE: UNDEVELOPED  
PROPOSED USE: RESIDENTIAL

### TAX ABATEMENT - SUSTAINABILITY

- BIKE RACKS FOR PUBLIC USE THAT PROVIDE A MINIMUM OF 10% OF THE AUTHORIZED PARKING PROVIDED.
- 50% INCREASE IN REQUIRED LANDSCAPING.
- PRIMARY ENTRY WITHIN A MILE OF A DART TRANSIT STOP.
- REDEVELOPMENT OF A PREVIOUSLY DEVELOPED SITE.

### INDEX OF SHEETS

NO.	DESCRIPTION
CD.1	COVER SHEET
C1.1	TOPOGRAPHIC SURVEY / DEMOLITION PLAN
C2.0-2.4	DIMENSION PLAN
C3.0-3.5	GRADING PLAN
C4.0-4.4	UTILITY PLAN
C5.1-5.2	DETAILS
L02.01-L02.05	RDG LANDSCAPE PLAN (SITE LAYOUT PLAN)
L04.01-L04.05	RDG LANDSCAPE PLAN (SITE PLANTING PLAN)
L05.01	RDG LANDSCAPE PLAN (DETAILS)

### DEVELOPMENT SUMMARY

TOTAL SITE AREA:	11.47 ACRES (499,715 SF)
EXISTING IMPERVIOUS AREA:	0.00 ACRES (0 SF)

### DEEN SPACE CALCULATION:

TOTAL SITE:	-	499,715 SF (11.47 AC)
BUILDINGS:	-	84,115 SF
STREETS:	-	85,894 SF
DRIVEWAYS:	-	12,745 SF
SIDEWALKS:	-	24,388 SF
OPEN SPACE PROVIDED:	-	282,622 SF (58.6%)

### UNITS:

85 TOWNHOME UNITS  
84 CONDOMINIUM UNITS (LOT 67)  
149 TOTAL UNITS (1.289 UNITS PER ACRE)

### NOTES

- ALL PUBLIC IMPROVEMENTS, INCLUDING SPOT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR PUBLIC IMPROVEMENT PLANS. IF THERE ARE ANY DISCREPANCIES BETWEEN THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLANS THE DETAILED PUBLIC IMPROVEMENT PLANS SHALL GOVERN.
- THIS PROJECT IS ELIGIBLE FOR TAX ABATEMENT IN ACCORDANCE WITH THE URBAN REVENUE DEVELOPMENT AGREEMENT - SECTION 5.02 (8/16/23 P.0251-289).XXXX

### UTILITY WARNING

ANY UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY AND RECORDS OBTAINED BY THIS SURVEYOR. THE SURVEYOR MAKES NO GUARANTEE THAT THE UTILITIES SHOWN COMPRISE ALL THE UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UTILITIES SHOWN ARE IN THE EXACT LOCATION SHOWN.

### GENERAL LEGEND

PROPOSED	EXISTING
PROPERTY BOUNDARY	SANITARY MANHOLE
LOT LINE	WATER VALVE BOX
CENTER LINE	FIRE HYDRANT
RIGHT OF WAY	WATER CURB STOP
BUILDING SETBACK	WELL
PERMANENT EASEMENT	STORM SEWER MANHOLE
TEMPORARY EASEMENT	STORM SEWER SINGLE INTAKE
TYPE SW-501 STORM INTAKE	STORM SEWER DOUBLE INTAKE
TYPE SW-502 STORM INTAKE	FLARED END SECTION
TYPE SW-503 STORM INTAKE	DECIDUOUS TREE
TYPE SW-505 STORM INTAKE	CONIFEROUS TREE
TYPE SW-506 STORM INTAKE	DECIDUOUS SHRUB
TYPE SW-507 STORM INTAKE	CONIFEROUS SHRUB
TYPE SW-512 STORM INTAKE	ELECTRIC POWER POLE
TYPE SW-513 STORM INTAKE	GUY ANCHOR
TYPE SW-401 STORM MANHOLE	STREET LIGHT
TYPE SW-402 STORM MANHOLE	POWER POLE W/ TRANSFORMER
FLARED END SECTION	UTILITY POLE W/ LIGHT
TYPE SW-301 SANITARY MANHOLE	ELECTRIC BOX
STORM/SANITARY CLEANOUT	ELECTRIC TRANSFORMER
WATER VALVE	ELECTRIC MANHOLE OR VAULT
FIRE HYDRANT ASSEMBLY	TRAFFIC SIGN
SIGN	TELEPHONE JUNCTION BOX
DETECTABLE WARNING PANEL	TELEPHONE MANHOLE/VAULT
WATER CURB STOP	TELEPHONE POLE
SANITARY SEWER	GAS VALVE BOX
SANITARY SERVICE	CABLE TV JUNCTION BOX
STORM SEWER	CABLE TV MANHOLE/VAULT
STORM SERVICE	MAIL BOX
WATERMAIN WITH SIZE	BENCHMARK
WATER SERVICE	SOIL BORING
SAW/CUT (FULL DEPTH)	UNDERGROUND TV CABLE
SILT FENCE	GAS MAIN
USE AS CONSTRUCTED	FIBER OPTIC
MINIMUM PROTECTION ELEVATION	UNDERGROUND TELEPHONE
	OVERHEAD ELECTRIC
	UNDERGROUND ELECTRIC
	FIELD TILE
	SANITARY SEWER W/ SIZE
	STORM SEWER W/ SIZE
	WATER MAIN W/ SIZE



1-800-292-8989  
www.iowaonecall.com



4121 NW URBANDALE DRIVE, URBANDALE, IA 50322  
PH: (515) 399-4400  
PROJECT NO. 2202.113

THE PROJECT REQUIRES AN IOWA NPDES PERMIT #2 AND CITY OF DES MOINES GRADING PERMIT. CIVIL DESIGN ADVANTAGE WILL PROVIDE THE PERMITS AND THE INITIAL STORM WATER POLLUTION PREVENTION PLAN (SWPPP) FOR THE CONTRACTORS USE DURING CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR UPDATING THE SWPPP THROUGHOUT CONSTRUCTION AND MEETING LOCAL, STATE AND FEDERAL REQUIREMENTS.

ALL CONSTRUCTION MATERIALS, DUMPSTERS, DETACHED TRAILERS OR SIMILAR ITEMS ARE PROHIBITED ON PUBLIC STREETS OR WITHIN THE PUBLIC R.O.W.

THE 2022 EDITION OF THE SDAS STANDARD SPECIFICATIONS, THE PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) AND ALL CITY SUPPLEMENTS, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.

### PRELIMINARY PLAT APPROVAL:

APPROVED  APPROVED WITH CONDITIONS - SEE EXHIBIT "A"

IN ACCORDANCE WITH CHAPTER 106, DES MOINES MUNICIPAL CODE, AS AMENDED.

DEVELOPMENT SERVICES DIRECTOR: \_\_\_\_\_ DATE: \_\_\_\_\_

I HEREBY CERTIFY THAT THIS ENGINEERING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF IOWA.

**PRELIMINARY**  
DESIGN FOR CONSTRUCTION

DATE: \_\_\_\_\_  
BY: \_\_\_\_\_  
REGISTERED PROFESSIONAL ENGINEER  
IOWA  
2021

MY LICENSE RENEWAL DATE IS DECEMBER 31, 2023  
PAGES OR SHEETS COVERED BY THIS SEAL:  
C SERIES SHEETS

### SITE PLAN APPROVAL:

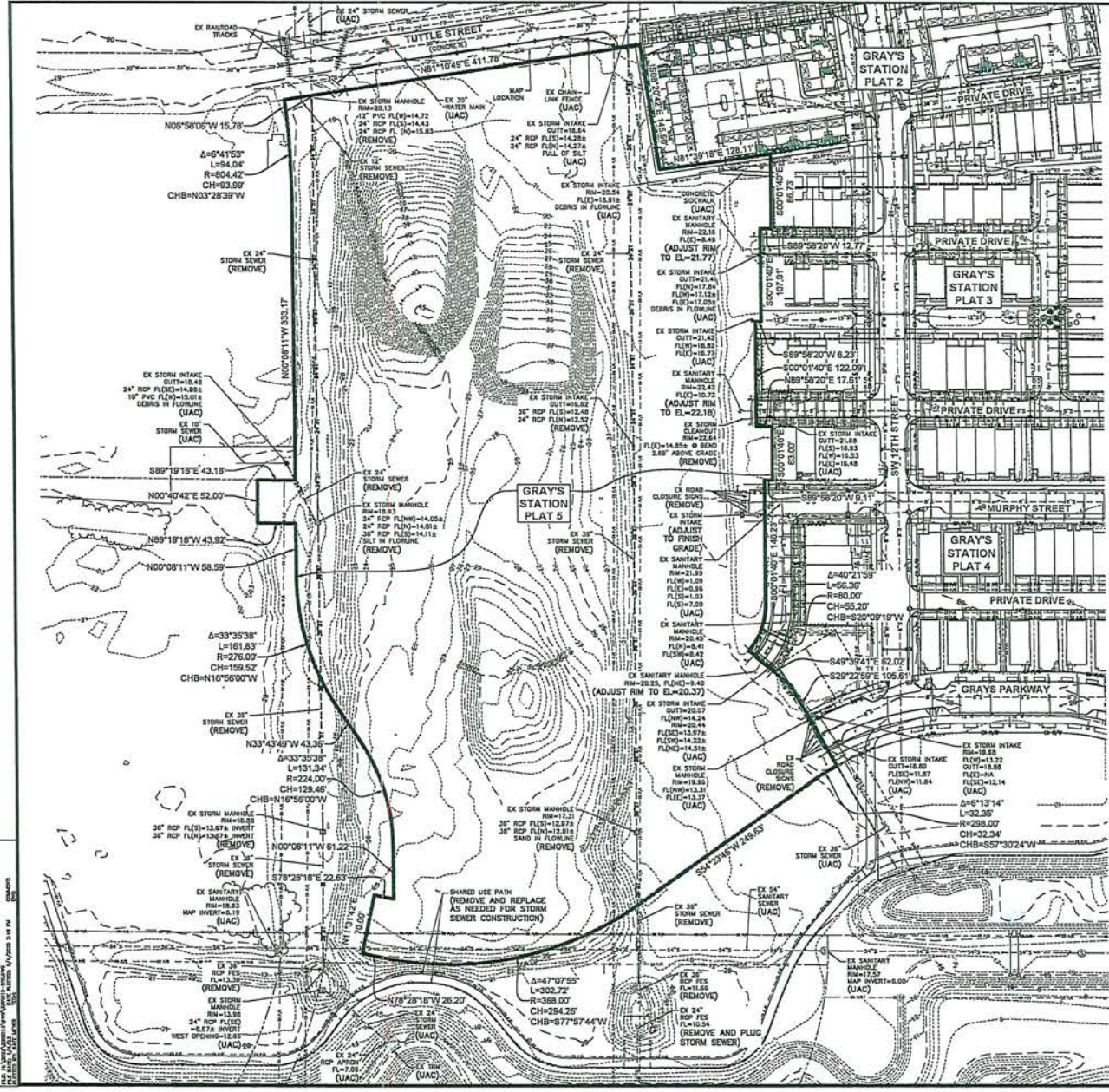
APPROVED  APPROVED WITH CONDITIONS - SEE EXHIBIT "A"

IN ACCORDANCE WITH SECTION 135-9, 2019 DES MOINES MUNICIPAL CODE, AS AMENDED.

NO CHANGES TO THIS PLAN UNLESS APPROVED IN WRITING FROM THE DEVELOPMENT SERVICES DIRECTOR.

DEVELOPMENT SERVICES DIRECTOR: \_\_\_\_\_ DATE: \_\_\_\_\_





**DEMOLITION NOTES**

1. PRIOR TO ANY WORK AT THE SITE, CONTRACTOR SHALL EXAMINE ANY APPLICABLE DRAWINGS AVAILABLE FROM THE OWNER OR ENGINEER AND CONSULT WITH OWNER'S PERSONNEL AND UTILITY COMPANY REPRESENTATIVES. NO COMPENSATION WILL BE ALLOWED FOR DAMAGE FROM FAILURE TO COMPLY WITH THIS REQUIREMENT.
2. PRIOR TO DEMOLITION, CONTRACTOR SHALL NOTIFY IN WRITING (48 HRS NOTICE) THE FOLLOWING:
  - a. CITY
  - b. APPROPRIATE UTILITY COMPANIES
  - c. OWNER
  - d. CIVIL DESIGN ADVANTAGE ARCHITECT
3. DO NOT DESTROY EXISTING UTILITIES UNLESS OTHERWISE NOTED. COORDINATE REMOVAL OR ABANDONMENT OF ALL UTILITIES WITH THE APPROPRIATE UTILITY SUPPLIER AND REGULATORY AGENCIES.
4. PROTECT EXISTING UTILITIES THAT ARE TO REMAIN. THE LOCATION OF ALL UTILITIES NOTED ON THE PLANS ARE TAKEN FROM EXISTING RECORDS. THE EXACT LOCATION AND ELEVATION OF ALL UTILITIES MUST BE DETERMINED BY THE CONTRACTOR. IT SHALL BE THE DUTY OF THE CONTRACTOR TO ASCERTAIN WHETHER ANY ADDITIONAL FACILITIES OTHER THAN THOSE SHOWN ON THE PLAN MAY BE PRESENT.
5. BACKFILL ALL EXCAVATIONS WITH COMPACTED MATERIAL CONFORMING TO THE SPECIFICATIONS OF THE STANDARD SPECIFICATIONS AND MOISTURE RANGE OF OPTIMUM MOISTURE TO 4% ABOVE OPTIMUM MOISTURE. TESTING OF BACKFILL TO BE BY A GEOTECHNICAL ENGINEER EMPLOYED BY THE OWNER. IN THE EVENT OF A TEST FAILURE, ANY RETESTING SHALL BE PAID FOR BY THE CONTRACTOR.
6. FIELD VERIFY EXISTING GRADES AND LOCATION OF EXISTING UTILITIES, CONDUIT LINES, POLES, TREES, FENCES, BUILDING AND OTHER SITE FEATURES PRIOR TO DEMOLITION AND IMMEDIATELY NOTIFY THE ENGINEER AND/OR OWNER OF ANY DISCREPANCIES.
7. DEMOLITION NOTES AS SHOWN ON THE PLAN ARE NOT ALL INCLUSIVE. CONTRACTOR TO ABANDON IN PLACE OR REMOVE AND DISPOSE OF ALL EXISTING SITE IMPROVEMENTS ABOVE AND BELOW GROUND TO COMPLY WITH THE GENERAL INTENT OF THIS DOCUMENT.
8. ALL DEMOLITION/REMOVAL DEBRIS SHALL BE DISPOSED OF OFFSITE IN FULL COMPLIANCE WITH CURRENT ENVIRONMENTAL REGULATIONS.
9. CONSTRUCTION LIMITS SHALL BE CONFIRMED TO THE SITE BOUNDARY AS NOTED. ANY DAMAGE TO PROPERTIES OUTSIDE THE SITE BOUNDARY SHALL BE AT THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
10. ALL WORK SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES, NOTICES INDICATED ON THE DRAWINGS SHALL REQUIRE THE CONTRACTOR FROM COMPLYING WITH ANY APPROPRIATE SAFETY REGULATIONS.

**TRAFFIC CONTROL NOTES**

1. ALL APPLICABLE CITY PERMITS, INCLUDING BUT NOT LIMITED TO CLOSURE PERMITS, SHALL BE OBTAINED PRIOR TO ANY CONSTRUCTION WITHIN CITY LIMITS OR LAKE CLOSURES.
2. ALL TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
3. PROMINENT SIGNING THAT CONVEYS A MESSAGE CONTRARY TO THE MESSAGE OF TEMPORARY SIGNING AND NOT APPLICABLE TO THE SPECIFIC CONDITIONS SHALL BE COVERED BY THE CONTRACTOR WHEN DIRECTED BY THE CITY.
4. THE CONTRACTOR SHALL COORDINATE HIS TRAFFIC CONTROL WITH OTHER CONSTRUCTION PROJECTS IN THE AREA.
5. SHOWALL CLOSURE SIGNS REQUIRED FOR ALL SIDEWALK CLOSURES, SIGNAGE AND TEMPORARY PEDESTRIAN ACCESS ROUTE THROUGH THE CONSTRUCTION SHALL MEET THE REQUIREMENTS OF PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG), SECTION 502 AND 504 DOT DESIGN MANUAL, CHAPTER 12A.
6. THE CONTRACTOR IS CAUTIONED NOT TO DISTURB OR REMOVE ANY EXISTING PAVEMENT, NOR TO DISTURB THE EXISTING TRAFFIC PATTERNS MORE THAN IS NECESSARY FOR THE PROPER DEMOLITION OF THE WORK.
7. ALL SIGNING AND LAKE STOPPING WILL NEED TO COMPLY WITH LOCAL MAINTENANCE AND REPLACEMENT OF THE SIGNING AND STOPPING WILL BE THE RESPONSIBILITY OF THE APPLICANT.

DATE	
REVISIONS	
NO. 1	ISSUED FOR PERMIT
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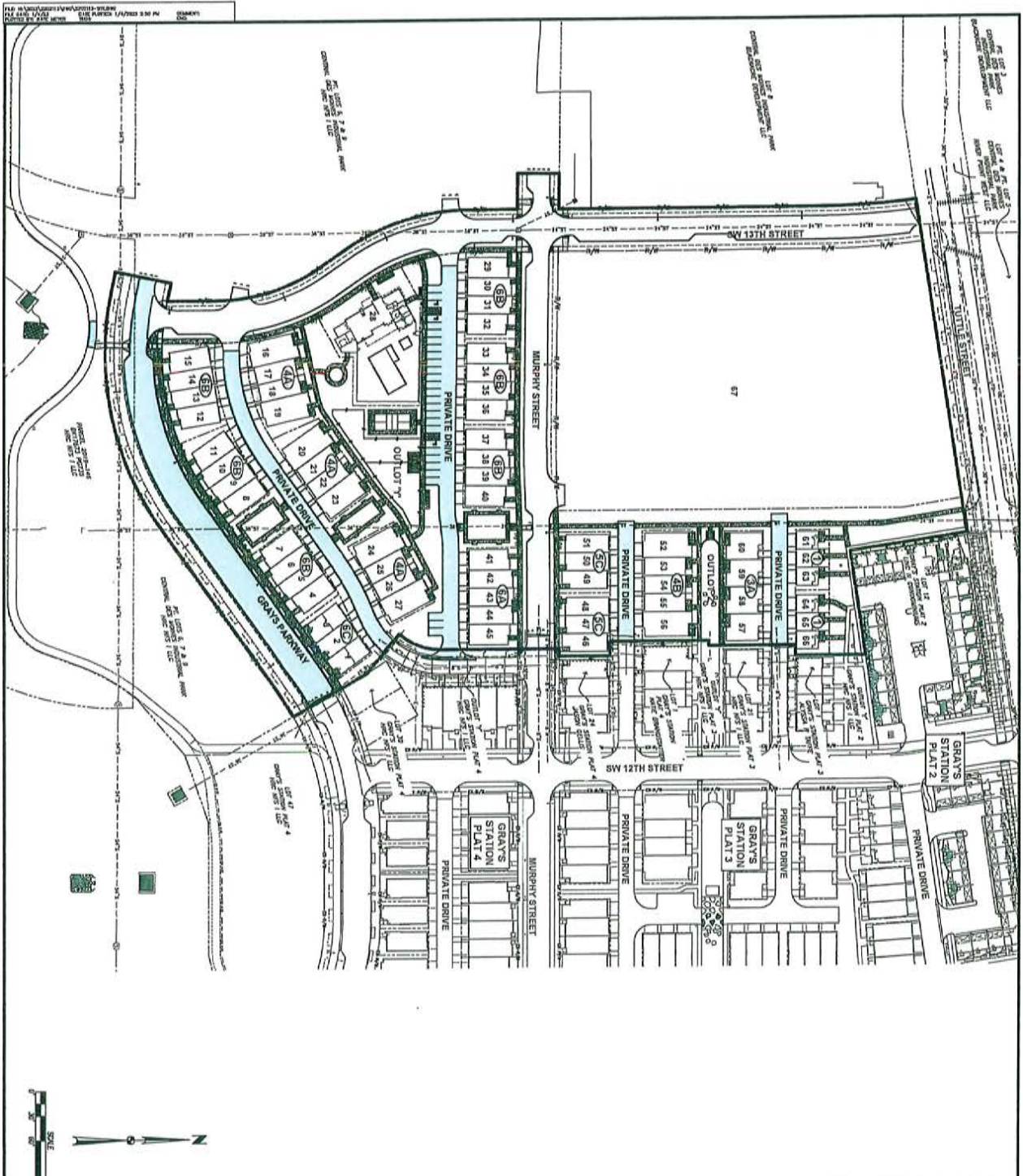
4121 NW URBANDALE DRIVE  
URBANDALE, IA 50322  
PHONE: (515) 269-4400

TECH: \_\_\_\_\_  
ENGINEER: \_\_\_\_\_

**GRAY'S STATION PLAT 5**  
TOPOGRAPHIC SURVEY / DEMOLITION PLAN

CIVIL DESIGN ADVANTAGE  
DES. MONIES, IOWA

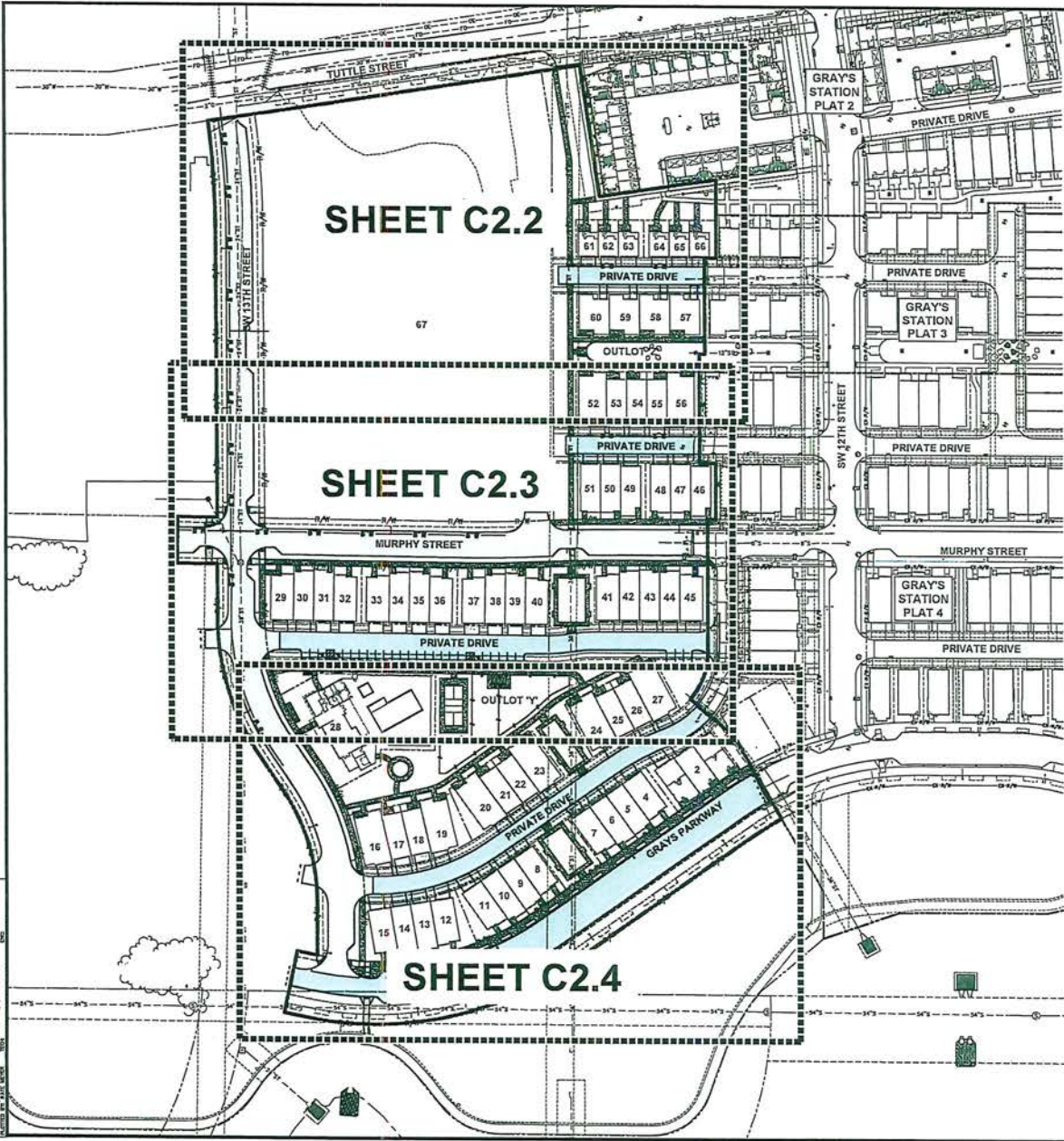
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2202.113



NOTE: ALL PUBLIC IMPROVEMENTS, INCLUDING SPOT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE IMPROVEMENT PLANS. THERE ARE NO ASSUMPTIONS BETWEEN THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLANS. THE DETAIL "FIELD IMPROVEMENT PLANS SHALL GOVERN."

PLAT	PRODUCT NUMBER	PRODUCT DESCRIPTION
5	1 (LOTS 61-60)	PRODUCT 1 - 3 UNITS
5	3A (LOTS 57-60)	PRODUCT 3A - 4 UNITS
5	3B (LOTS 16-27)	PRODUCT 3B - 4 UNITS
5	4B (LOTS 52-56)	PRODUCT 4B - 5 UNITS
5	5C (LOTS 42-51)	PRODUCT 5C - 3 UNITS
5	5A (LOTS 41-43)	PRODUCT 5A - 5 UNITS
5	5B (LOTS 44-51, 28-40)	PRODUCT 5B - 4 UNITS
5	5C (LOTS 1-3)	PRODUCT 5C - 3 UNITS

PLAT	PRODUCT NUMBER	LOT NUMBERS	COLOR PACKAGE NUMBER
5	1	61-60	1
5	3A	57-60	1
5	3B	16-27	1
5	4A	52-56	2
5	4B	28-33	1
5	4C	24-27	2
5	4D	52-56	2
5	5C	42-51	1
5	5A	41-43	1
5	5B	44-51	1
5	5C	61-60	1
5	5B	44-51	1
5	5C	28-33	2
5	5B	24-27	2
5	5C	52-56	2
5	5B	41-43	2
5	5C	1-3	2



- ### GENERAL NOTES
1. THE 2002 EDITION OF THE DES MOINES STANDARD SPECIFICATIONS AND ALL CITY SUPPLEMENTALS IF APPLICABLE SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.
  2. ALL WORK SHALL COMPLY WITH ADA ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES.
  3. ALL WORK SHALL BE IN ACCORDANCE WITH OSHA CODES AND STANDARDS. NOTHING INDICATED ON THE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLIANCE WITH ANY APPLICABLE SAFETY REGULATIONS.
  4. PRIOR TO ANY WORK AT THE SITE, THE CONTRACTOR SHALL EXAMINE ANY APPLICABLE PERMITS AVAILABLE FROM THE CIVIL ENGINEER, AND ARCHITECT, AND CONSULT WITH OWNER'S PERSONNEL, AND UTILITY COMPANY REPRESENTATIVES. NO CONSTRUCTION SHALL BE ALLOWED FOR DAMAGE FROM FAILURE TO COMPLY WITH THIS REQUIREMENT.
  5. PRIOR TO ANY CONNECTION WITH CITY BLOCK OR ANY CONNECTION TO PUBLIC SENIOR CONTRACTOR SHALL NOTIFY THE CITY'S CONSTRUCTION DIVISION. CONTRACTOR SHALL NOTIFY CITY OF JOB NUMBER ENDORSING DEPT. AT LEAST 48 HOURS PRIOR TO COMMENCEMENT OF CONSTRUCTION TO COORDINATE INSPECTIONS. CALL 515-281-4177 OR 515-208-4177.
  6. ALL CONSTRUCTION WITHIN PUBLIC RIGHT-OF-WAY, EASEMENTS, AND/OR ANY CONNECTION TO PUBLIC SENIOR AND STREETS, SHALL COMPLY WITH THE CITY'S STANDARDS. NO STOPS ARE ALLOWED IN SANITARY MAINLINES.
  7. ALL EMBANKMENTS ARE TO BE BACK OF CURB, BUILDING FACE OR PROPERTY LINE. CONTRACTOR TO VERIFY BUILDING EMBANKMENT WITH ARCHITECTURAL PLANS UNLESS OTHERWISE NOTED.
  8. CONTRACTOR TO VERIFY BUILDING EMBANKMENT WITH ARCHITECTURAL PLANS AND BUILDING.
  9. PLACE 3/4" INCH EXPANSION JOINT BETWEEN ALL P.C.C. PAVEMENT/SIDEWALKS AND BUILDINGS. PLACE 1/2" INCH EXPANSION JOINT BETWEEN SIDEWALKS AND P.C.C. PAVEMENT.
  10. REMOVE ALL CURBS SPILLED INTO BLOCK AT THE END OF EACH WORK DAY.
  11. ALL PROPERTY PINS SHALL BE PROTECTED FROM CHANGING OR OTHER OPERATIONS. ANY PINS DISTURBED SHALL BE RESET AT THE CONTRACTOR'S EXPENSE.
  12. DO NOT STORE CONSTRUCTION MATERIALS AND EQUIPMENT IN THE RIGHT-OF-WAY.
  13. THE CONTRACTOR SHALL NOT DISTURB DESIRABLE GRASS AREAS AND DESIRABLE TREES OUTSIDE THE CONSTRUCTION LIMITS. THE CONTRACTOR WILL NOT BE PERMITTED TO PILE OR SERVICE WOODS AND EQUIPMENT OR USE THESE AREAS FOR STORAGE OF MATERIALS, STORAGE, PARKING AND SERVICE AREAS.
  14. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ANY AREAS OF PAVEMENT OR EQUIPMENT ON THE PAVEMENT OR SIDEWALKS.
  15. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ANY AREAS OF PAVEMENT OR EQUIPMENT ON THE PAVEMENT OR SIDEWALKS.
  16. TEMPORARY WARNING DEVICES AND SAFETY FENCE AT CERTAIN LOCATIONS WHERE REPLACEMENT FEATURES ARE REQUIRED SHALL BE INSTALLED AS DIRECTED BY THE CONTRACTOR.
  17. THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR THE CONSTRUCTION OF INCLUDING STAGING OF CONSTRUCTION DETAILS.
  18. CONTRACTOR SHALL VERIFY REMOVAL LIMITS WITH CITY'S CONSTRUCTION INSPECTOR PRIOR TO ANY CONCRETE REMOVAL.
  19. ALL PERMITS AND ADDITIONAL FEES REQUIRED TO COMPLETE THE WORK SHALL BE OBTAINED BY THE CONTRACTOR.
  20. CONTRACTOR SHALL APPLY FOR SPECIAL PARKING PERMIT IN ADVANCE OF WORK TO SIDEWALK PARKING METER REMOVAL.
  21. ALL WORK WITHIN THE CITY RIGHT-OF-WAY REQUIRES A RIGHT-OF-WAY PERMIT. CONTRACTOR SHALL APPLY FOR A SIDEWALK/DRIVEWAY APPROACH PERMIT IN ADVANCE OF WORK.

- ### CITY OF DES MOINES STANDARD NOTES
1. ANY AMENDMENTS OR CHANGES TO THE PROJECT SITE THAT DO NOT MEET WHAT IS SHOWN ON THE SITE PLAN NEED TO BE APPROVED WITH THE PERMIT AND DEVELOPMENT CENTER PRIOR TO INSTALLATION/CONSTRUCTION.
  2. LIGHTING SHALL CONSIST OF LOW-SLARE CUT OFF TYPE FIXTURES TO REDUCE THE GLARE OF LIGHT POLLUTION ON SURROUNDING PROPERTIES.
  3. THE DESIRED LANDSCAPING, BOTH EXISTING AND PROPOSED, SHALL BE MAINTAINED FOR THE LIFE OF THE RESTORATION OF EQUIPMENT OR CERTIFICATE OF REPAIR COMPLIANCE.
  4. ALL DISTURBED AREAS SHALL BE RESTORED BY SEEDING OR SOODING.
  5. THIS SITE SHALL BE MAINTAINED IN COMPLIANCE WITH ALL CITY CODES.
  6. ANY GROUND-MOUNTED OR WALL-MOUNTED UTILITIES AND METERS SHALL BE SCREENED AND NOT VISIBLE FROM PUBLIC STREET.
  7. ANY WALL-MOUNTED UTILITIES SHALL BE SCREENED ON ALL SIDES AND FINISHED TO MATCH BUILDING.
  8. ALL UTILITIES SHALL BE UNDERGROUND.
  9. ALL ROOFER MECHANICAL EQUIPMENT MUST BE SCREENED ON ALL SIDES WITH ARCHITECTURAL SCREENING EQUAL TO THE HEIGHT OF THE EQUIPMENT.
  10. MECHANICAL EQUIPMENT SHALL NOT BE LOCATED IN THE FRONT YARD OR STREET SIDE YARD AND SHALL BE SCREENED FROM VIEW FROM ANY PUBLIC RIGHT-OF-WAY WITH LANDSCAPING, FENCING, OR WALLS CONSISTENT WITH THE BUILDING DESIGN, COLORS, AND MATERIALS.
  11. NO SIGN SHALL BE UTILIZED AS A GROUNDCOVER MATERIAL, PER CITY CODE SECTION 130-7.2.
  12. FOR TOWNHOMES, INDIVIDUAL RESIDENTIAL TOTES FOR TRASH AND RECYCLING WASTE WILL BE STORED INDOORS, AND PLACED AT THE CURB UPON DAY OF DISPOSAL.
  13. ANY BUILDING OR SITE STORAGE SHALL CONFORM WITH THE PROVISIONS DESCRIBED IN THE GRAY'S STATION P.D.

- ### PAVEMENT THICKNESS
1. SIDEWALKS 4" P.C.C.
  2. CLASS 'A' SIDEWALK 6" P.C.C.
  3. CYCLE TRACK 6" P.C.C.
  4. PRIVATE DRIVES 6" P.C.C.
  5. SIDEWALK RAMP 6" P.C.C.
  6. PUBLIC STREETS 7" & 8" P.C.C.

### GRAY'S STATION ADDRESSING

PLAT	LOT NUMBER	ADDRESS NUMBER	STREET NAME
5	1	XXX	XXX
5	2		
5	3		
5	4		
5	5		
5	6		
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5	8		
5	9		
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5	66		
5	67		

DATE: \_\_\_\_\_

REVISIONS: \_\_\_\_\_

SECOND SUBMITTAL: \_\_\_\_\_

FIRST SUBMITTAL: \_\_\_\_\_

TECH: \_\_\_\_\_

ENGINEER: \_\_\_\_\_

DES MOINES, IOWA

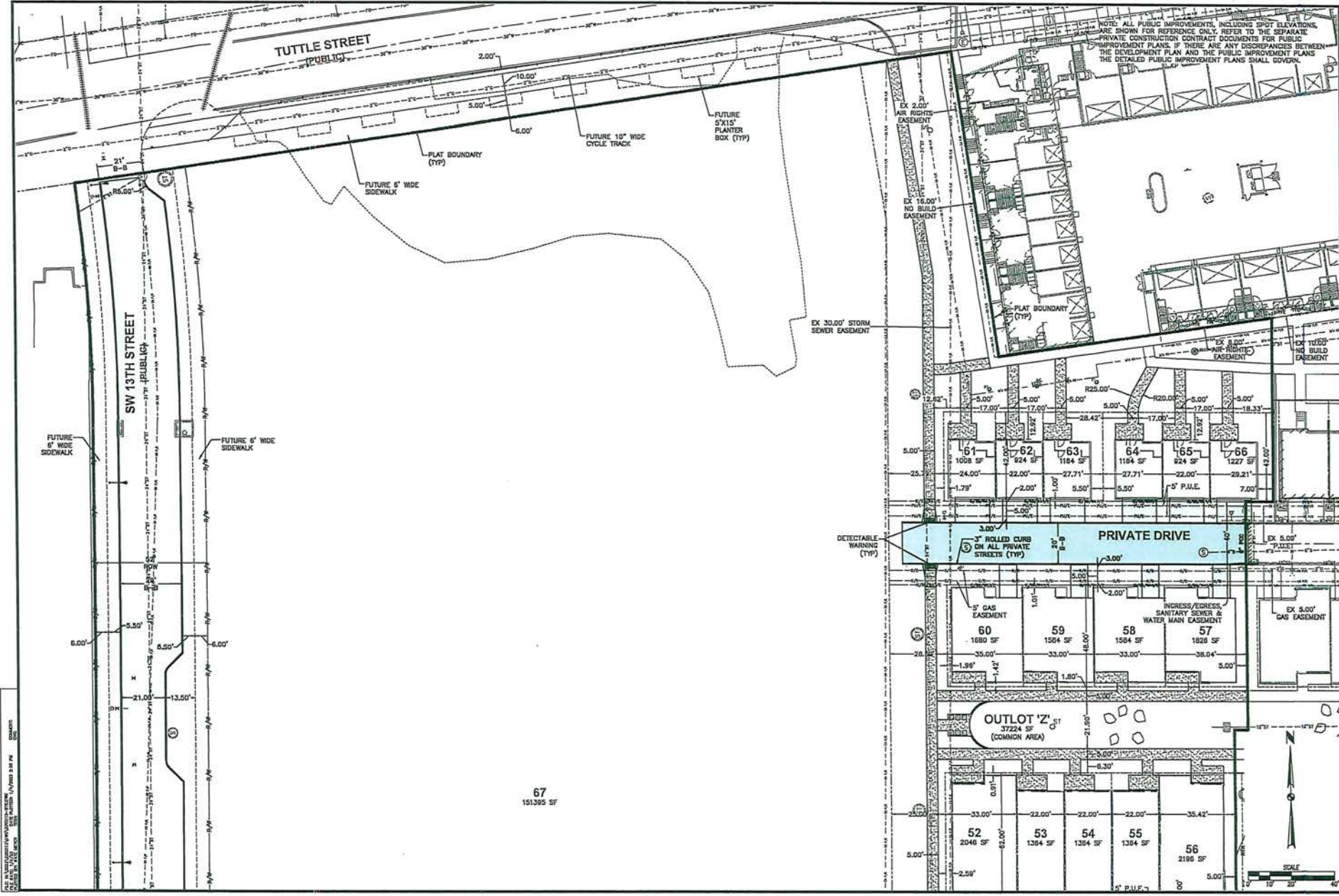
**GRAY'S STATION PLAT 5**

DIMENSION PLAN

**C2.1**

2202.113

NOTE: ALL PUBLIC IMPROVEMENTS, INCLUDING SPOT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR PUBLIC IMPROVEMENT PLANS. IF THERE ARE ANY DISCREPANCIES BETWEEN THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLANS THE DETAILED PUBLIC IMPROVEMENT PLANS SHALL GOVERN.



SCALE: 1" = 20'

DATE: 11/11/2022

PROJECT: GRAY'S STATION PLAT 5

DESIGNER: CIVIL DESIGN ADVANTAGE

**EA**

**GRAY'S STATION PLAT 5**

**DIMENSION PLAN**

**C2.2**

2202.113

DES MOINES, IOWA

4121 NW URBANDALE DRIVE  
URBANDALE, IA 50022  
PHONE: (515) 285-4660

TECH: [ ]  
ENGINEER: [ ]

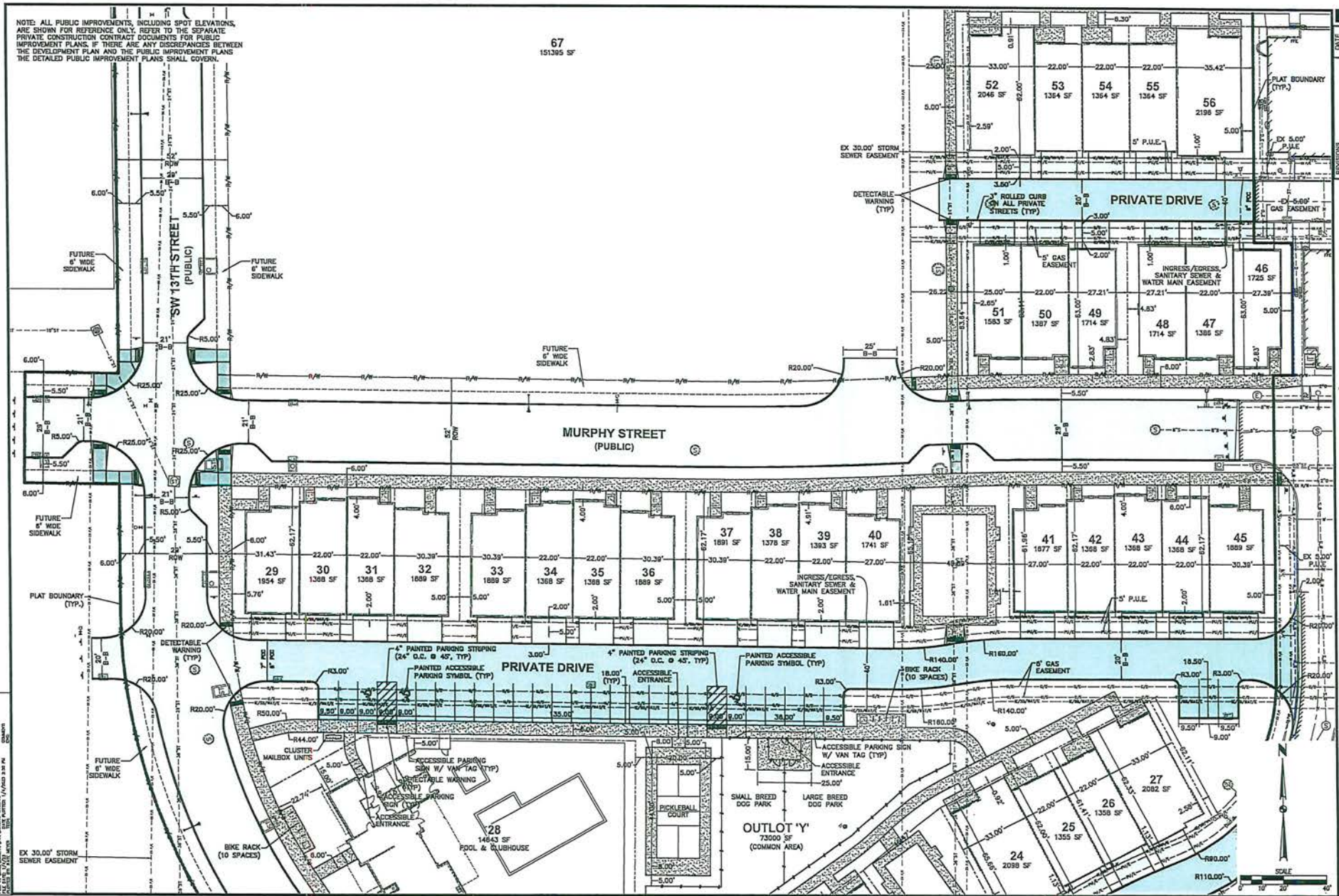
REVISIONS:

NO.	DATE	DESCRIPTION

SECOND SUBMITTAL  
FIRST SUBMITTAL

NOTE: ALL PUBLIC IMPROVEMENTS, INCLUDING SPOT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR PUBLIC IMPROVEMENT PLANS. IF THERE ARE ANY DISCREPANCIES BETWEEN THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLANS THE DETAILED PUBLIC IMPROVEMENT PLANS SHALL GOVERN.

67  
151395 SF



DATE	
REVISIONS	
SECOND SUBMITTAL	03/09/2022
FIRST SUBMITTAL	03/09/2022

4121 NW URBANDALE DRIVE  
URBANDALE, IA 50322  
PHONE: (515) 885-4660

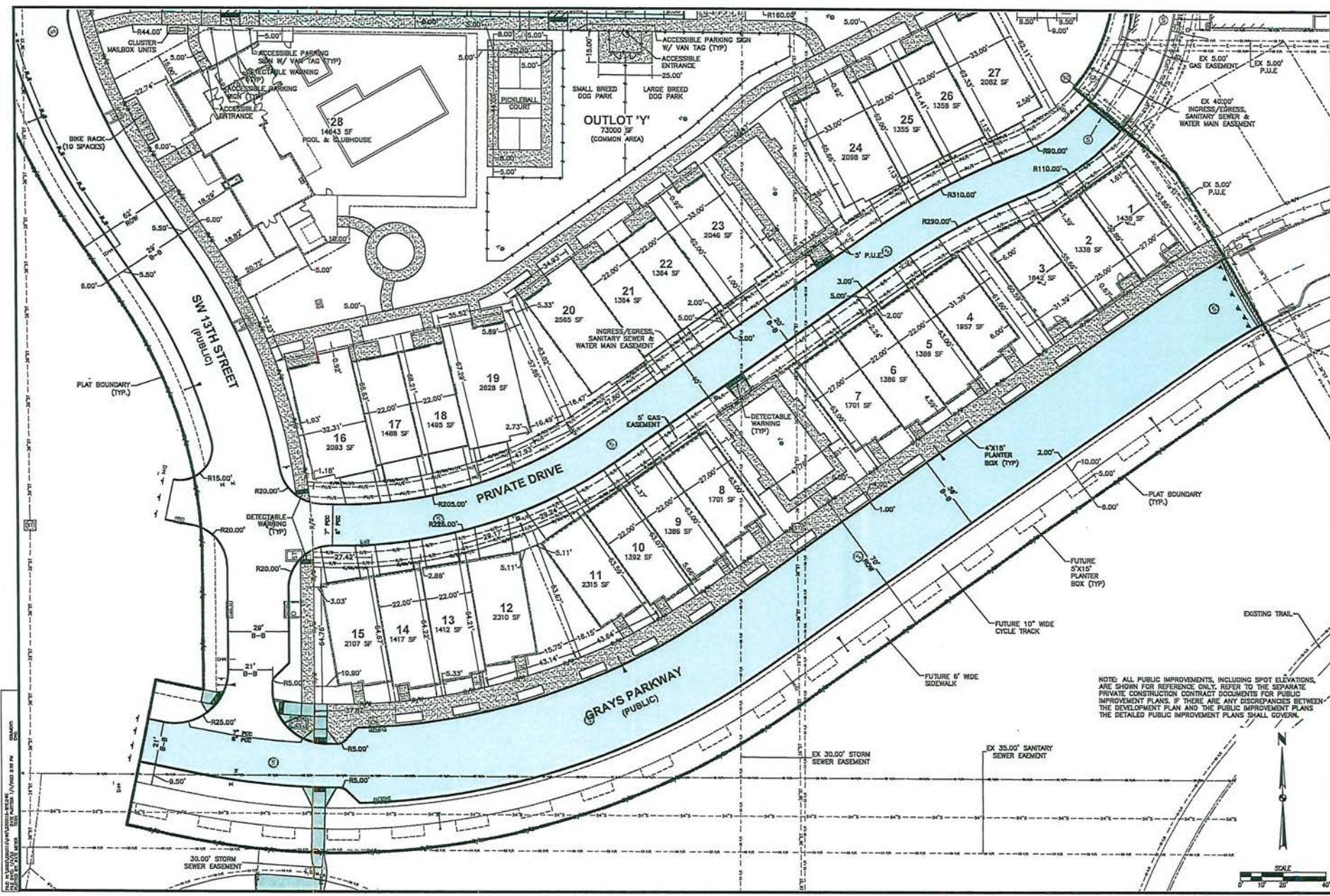


**GRAY'S STATION PLAT 5**  
DIMENSION PLAN

**C2.3**  
2202.113

DES MOINES, IOWA  
CIVIL DESIGN ADVANTAGE ENGINEER, INC.

TECH:



NOTE: ALL PUBLIC IMPROVEMENTS, INCLUDING SPOT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR PUBLIC IMPROVEMENT PLANS. IF THERE ARE ANY DISCREPANCIES BETWEEN THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLANS THE DETAILED PUBLIC IMPROVEMENT PLANS SHALL GOVERN.

DATE	
REVISIONS	
SECOND SUBMITTAL	01/04/2023
FIRST SUBMITTAL	10/19/2022

4121 NW URBANDALE DRIVE  
 URBANDALE, IA 50322  
 PHONE: (515) 389-4400

TECH: [Blank]  
 ENGINEER: [Blank]

**EA**  
 CIVIL DESIGN ADVANTAGE

**GRAY'S STATION PLAT 5**  
 DIMENSION PLAN

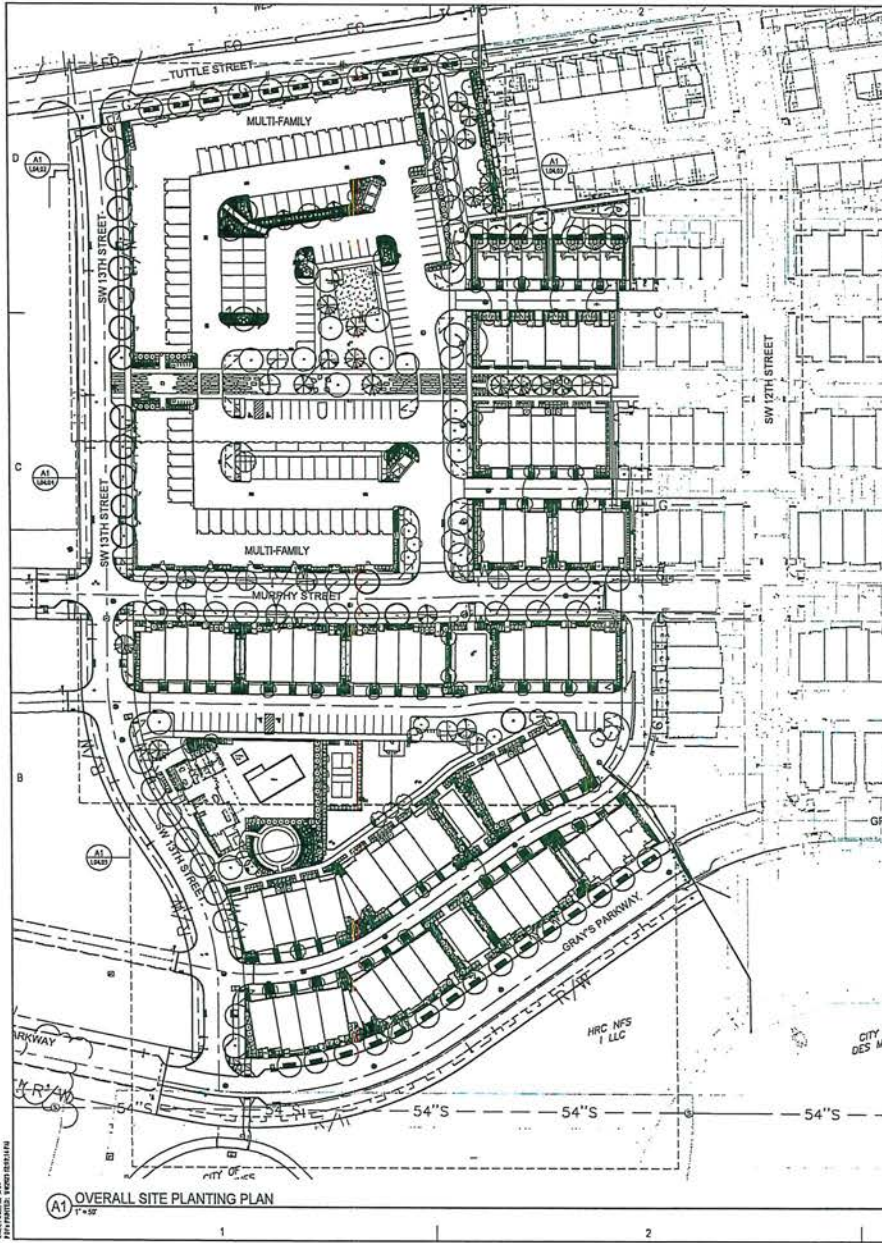
**C2.4**  
 2202.113











PLANT SCHEDULE						
PERCENTAGE	QTY.	ABBR.	COMMON NAME	BOTANICAL NAME	SIZE	NOTES
<b>SHADE TREES</b>						
7.93%	11	GO	Hickory	<i>Celtis occidentalis</i>	2"	8.8.8
8.22%	12	GB	Madroño Tree	<i>Grigio White Golden Columnar</i>	2"	8.8.8
8.22%	12	GD	Kentucky Coffeetree	<i>Gymnocladia dioica 'saxosa'</i>	2"	8.8.8
7.23%	11	AT	Japanese Flowering Almond	<i>Prunus japonica 'Sonnchoten'</i>	2"	8.8.8
8.22%	12	PA	London Planetree	<i>Platanus x acerifolia 'Lancelotti'</i>	2"	8.8.8
8.22%	12	TD	Red Cypress	<i>Taxodium distichum</i>	2"	8.8.8
8.96%	13	OK	Red Oak	<i>Quercus rubra</i>	2"	8.8.8
8.22%	12	OM	Bur Oak	<i>Quercus macrocarpa</i>	2"	8.8.8
8.85%	10	AN	Red Maple	<i>Acer rubrum</i>	2"	8.8.8
<b>UNDERSTORY TREES</b>						
10.04%	10	CF	Flowering Dogwood	<i>Cornus florida</i>	1.5"	8.8.8
17.02%	18	CA	Prunella Dogwood	<i>Cornus alamosa</i>	1.5"	8.8.8
8.31%	8	CC	Eastern Redbud	<i>Cercis canadensis</i>	1.5"	8.8.8
8.31%	8	MI	Prune Crabapple	<i>Malus ioensis</i>	1.5"	8.8.8
15.96%	18	AC	Shubert's Spindle Tree	<i>Amelanchier canadensis</i>	1.5"	2 or 3 Shrub, 8.8.8
17.02%	18	AG	Autumn Balmanceeberry	<i>Amelanchier x grandiflora 'Autumn Brilliance'</i>	1.5"	2 or 3 Shrub, 8.8.8
7.45%	7	RP	Goldenrain Tree	<i>Koeleria paniculata</i>	1.5"	8.8.8
7.45%	7	SK	Japanese Tree Lilac	<i>Syringa reticulata 'Honey Gold'</i>	1.5"	8.8.8
<b>EVERGREEN TREES</b>						
8.96%	13	PB	Norway Spruce	<i>Picea abies</i>	6"	8.8.8
8.96%	13	PS	White Pine	<i>Pinus strobus</i>	6"	8.8.8
7.33%	11	PF	Lambert Pine	<i>Pinus Seedling Vancouveria Pyramid</i>	6"	8.8.8
<b>TYPE ONE MIX</b>						
450	IF	Little Honey Suckle/pipe Shrub	<i>Lonicera x edulis 'Little Honey'</i>	5 gal	CONT.	
340	JS	Galaxy Carpet Juniper	<i>Juniperus sibirica 'Nana'</i>	5 gal	CONT.	
400	SD	Indiancandy Conifers	<i>Syringa reticulata 'Indiancandy'</i>	5 gal	CONT.	
400	SD	Blue Spruce	<i>Juniperus horizontalis 'Blue Spire'</i>	5 gal	CONT.	
400	CA	Red Juniper Feathered Grass	<i>Juniperus horizontalis 'Red Feather'</i>	1 gal	CONT.	
450	SH	Prunella Drooping	<i>Spiraea helioides</i>	1 gal	CONT.	
430	AM	Amelanchier yunnan	<i>Amelanchier yunnanensis</i>	1 gal	CONT.	
350	HO	Prunella Florida	<i>Prunella floridana</i>	1 gal	CONT.	
350	VM	Petunias	<i>Prunella Minor Bowles'</i>	1 gal	CONT.	
<b>TYPE TWO MIX</b>						
350	SSB	Little Blarney 'The Blues'	<i>Schizanthus lanceolatus 'Blues'</i>	1 gal	CONT.	
440	HS	Hollyhock Hollyhock	<i>Hollyhock 'Hollyhock'</i>	1 gal	CONT.	
375	NR	Winklers Low Cabinet	<i>Hesperis matronalis 'Winklers Low'</i>	1 gal	CONT.	
410	SB	Blue Hill Salvia	<i>Salvia x sylvestris 'Blue Hill'</i>	1 gal	CONT.	
<b>TYPE THREE MIX</b>						
350	BC	Side Ouse Grama	<i>Bouteloua curtipendula</i>	1 gal	CONT.	
250	CL	Hardpan Blue Ouse Grama	<i>Chloris verticillata</i>	1 gal	CONT.	
400	MS	Maiden Grass	<i>Miscanthus sinensis 'Graceline'</i>	1 gal	CONT.	
430	PV	Shenandoah Sweetgrass	<i>Panicum scoparium 'Shenandoah'</i>	1 gal	CONT.	
<b>TYPE FOUR MIX</b>						
50	CS	Red Twig Dogwood	<i>Cornus sericea 'Riverside'</i>	5 gal	CONT.	
30	RA	Red Yew Juniper	<i>Juniperus horizontalis 'Red Yew'</i>	5 gal	CONT.	
20	VC	Koningskops Viburnum	<i>Viburnum cederstroemi</i>	5 gal	CONT.	
<b>SOO/SITEO</b>						
			Leaf Type Ficus	<i>Ficus sp.</i>		

Parcel 1 Area, including ROW	49773.50		
Parcel 2 Area, including ROW	88239.50		
Outdoor Area	20147.50		
Outdoor 2 Area	15233.50		
Outdoor 2	City	Factor	Total
Overhead Street Tree	30	1000	30000
Overhead In-Lane Tree	60	1000	60000
Underpass Tree	37	100	3700
Overpass Tree	22	300	6600
Outdoor 2 Total Canopy Coverage	percentage		60.94%
Outdoor 1	City	Factor	Total
Overhead Street Tree	30	1000	30000
Overhead In-Lane Tree	3	1000	3000
Underpass Tree	40	700	28000
Overpass Tree	9	300	2700
Outdoor 1 Total Canopy Coverage	percentage		53.37%
Total Plant Coverage, Less A.G.M.	percentage		219.00%
Total Plant Coverage, Including A.G.M.	percentage		514.6%
Total Plant Coverage, Including A.G.M.	percentage		43.97%

LEGEND	
	100000000
	GRAVEL
	PRUNE TREE, 1/2
	UNDERSTORY TREE, 1/2

- ### GENERAL PLANTING PLAN NOTES
- QUANTITIES SHOWN IN THE PLANT SCHEDULE ARE FOR INFORMATION ONLY. THE DRAWING SHALL PREVAIL IF A CONFLICT OCCURS.
  - FOR UNDERGROUND UTILITY LOCATED CALL DOWN ONE CALL AT 811, 48 HOUR NOTICE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL UNDERGROUND LOCATED NOT COVERED BY THE DIGGING HOTLINE SYSTEM. NO COMPENSATION FOR DAMAGES TO EXISTING UTILITIES WILL BE ALLOWED RESULTING FROM FAILURE TO COMPLY WITH THIS REQUIREMENT.
  - THE LANDSCAPE CONTRACTOR SHALL PROTECT ALL UTILITIES, STRUCTURES AND SITE IMPROVEMENTS. ANY DAMAGE SHALL BE REPAIRED BY THE LANDSCAPE CONTRACTOR AT NO COST TO THE OWNER.
  - FINISH GRADES IN ALL LANDSCAPE AREAS ARE TO BE REVIEWED BY THE LANDSCAPE ARCHITECT PRIOR TO ANY SEEDING, SOODING, OR PLANTING. THE CONTRACTOR SHALL SUBMIT A WRITTEN REQUEST FOR REVIEW FIVE (5) DAYS PRIOR TO THE DESIRED DATE OF REVIEW. NO COMPENSATION FOR RE-SEEDING, RE-SOODING, OR RE-PLANTING OF AN AREA REQUIRING REMEDIAL GRADING SHALL BE ALLOWED RESULTING FROM FAILURE TO COMPLY WITH THIS REQUIREMENT.
  - INSTALL SOD IN THE AREAS INDICATED ON THE PLAN.
  - PRIOR TO SEEDING OR SOODING, VERIFY THAT SOIL ADJACENT TO PAVEMENTS AND CURBS ARE AT THE CORRECT ELEVATION TO ASSURE A LEVEL TRANSITION FROM THE PAVEMENT TO THE LAWN AREAS. NO COMPENSATION FOR REMOVAL AND REPLACING OF INSTALLED LAWN, DUE TO UNACCEPTABLE TRANSITION FROM ONE SURFACE TO THE OTHER (E.G. SOD TOO HIGH OR TOO LOW), WILL BE ALLOWED RESULTING FROM FAILURE TO COMPLY WITH THIS REQUIREMENT.
  - SEED ALL OTHER DISTURBED AREAS DUE TO CONSTRUCTION OPERATIONS.
  - PRIOR TO ANY EXCAVATION AT THE SITE, THE CONTRACTOR SHALL EXAMINE ANY APPLICABLE GRADING AVAILABLE FROM THE OWNER, ARCHITECT, LANDSCAPE ARCHITECT, AND/OR CIVIL ENGINEER AND CONSULT WITH ALL UTILITY COMPANY REPRESENTATIVES TO DETERMINE POSSIBLE UTILITY LOCATIONS.
  - NOTIFY THE LANDSCAPE ARCHITECT IN WRITING FIVE (5) DAYS PRIOR TO THE ANTICIPATED DATE FOR REVIEW OF THE PLANT LOCATION STAKING. REVIEW TO BE REQUESTED PRIOR TO EXCAVATION OF THE PLANTING PITS.
  - ALL DECIDUOUS TREES SHALL BE PLANTED 5 FEET OR MORE FROM ANY R.O.W. AND ALL CONIFEROUS TREES AT LEAST 10 FEET AWAY UNLESS INDICATED OTHERWISE ON THE DRAWING.
  - ALL SOIL AND OTHER DEBRIS SPILLED OR TRACKED ONTO THE PUBLIC R.O.W. OR ADJACENT PROPERTIES SHALL BE CLEANED UP BY THE CONTRACTOR AT THE END OF EACH WORK DAY.
  - PLANT LABELS AND TAGS SHALL BE LEFT ON THE PLANT MATERIAL UNTIL AFTER THE INITIAL INSPECTION BY THE LANDSCAPE ARCHITECT.
  - PRUNE ALL EXISTING TREES THAT REMAIN AS FOLLOWS:
    - REMOVE BROKEN LIMBS
    - REMOVE CROSSING LIMBS
    - REMOVE ROOT SPROUTS & SUCKERS
    - ANY MULTISTEMMED TREES - REMOVE STEMS LESS THAN 2" DIAMETER
    - ALL DECIDUOUS TREES - ELEVATE TREE LIMBS TO ALLOW 10' HT. CLEAR.
    - SHRUB TREE TO ITS CHARACTERISTIC FORM
  - ALL SOD AREAS TO BE IRRIGATED. SEE SPECIFICATIONS.
  - CONTACT THE MUNICIPAL FORESTRY DEPARTMENT PRIOR TO PLANTING AT 516.283.480.
  - ALL WIRE, TRAIL, AND BURIAL SHALL BE REMOVED FROM THE ROOTBALL OF TREES PRIOR TO PLANTING...

(A1) OVERALL SITE PLANTING PLAN  
1"=50'

GRAY'S STATION PLAT 5 PRELIMINARY - NOT FOR CONSTRUCTION DESIGN DEVELOPMENT

ARCHITECT  
RDG Planning & Design  
200 West 10th Street  
Des Moines, IA 50319

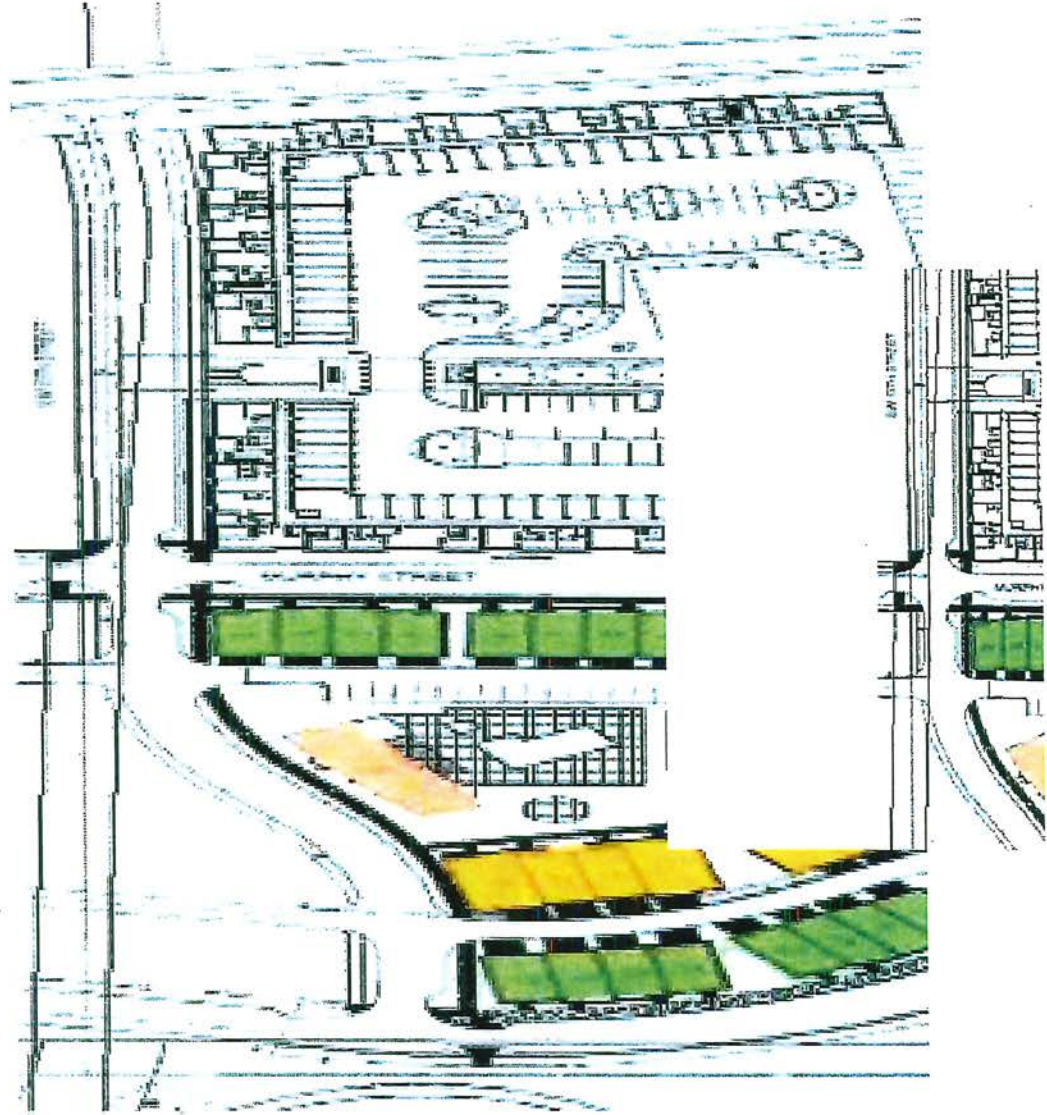
LANDSCAPE ARCHITECT  
HUBBELL REALTY  
200 West 10th Street  
Des Moines, IA 50319

ED PLAN

DATE	DESCRIPTION

SITE PLANTING PLAN

L04.01





- ROOF MEMBRANE - CARLISLE SURE WELD TPO | GRAY
- FIBER CEMENT LAP SIDING & TRIM - HARDIE | AGED PEWTER
- FIBER CEMENT PANEL & TRIM - HARDIE | PEARL GRAY
- RAILING | BLACK
- METAL PANEL - FIRESTONE | DARK BRONZE
- PAINT - SW 7674- PEPPERCORN

NEW FRONT ELEVATION



OLD FRONT ELEVATION

## Elevation Updates in Phase II

In Phase 2 we are proposing the same Product types/units as were approved for Phase 1. The floorplans for each unit type will remain the same. We made some revisions to the Elevations and Exterior Color Packages as we felt that they needed to be updated for Phase II. Some of the elevations didn't have enough intentional variations and the color packages were too light in color for an urban neighborhood.

The following pages show the new and the old front elevations of each product type and a description of the changes.

### Product 1

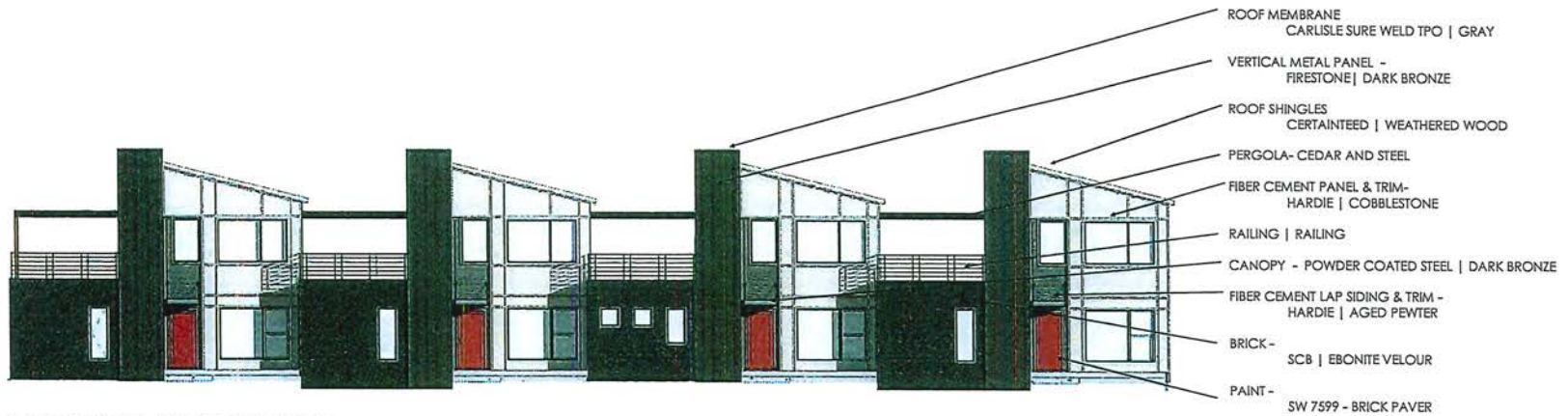
The desire was to create a darker color package for this Product and add some variation between the units.

- Increased visual interest through varying heights of the entry element clad in metal panels.
- Revised the color scheme and made it darker to achieve a more urban look.

## PRODUCT 1 | COMPARISON

COLOR PACKAGE: Product 1—#1

GRAY'S STATION | 07.07.2022 | 3



NEW FRONT ELEVATION

**Product 3**

This elevation looked too busy and had too many design themes and variations.

- Revised vertical elements to all be the same color and revised material to dark metal panels with a vertical pattern.
- The cube-shaped building elements were revised from Hardie lap siding/stone to all stone, to unify the overall building design.



OLD FRONT ELEVATION

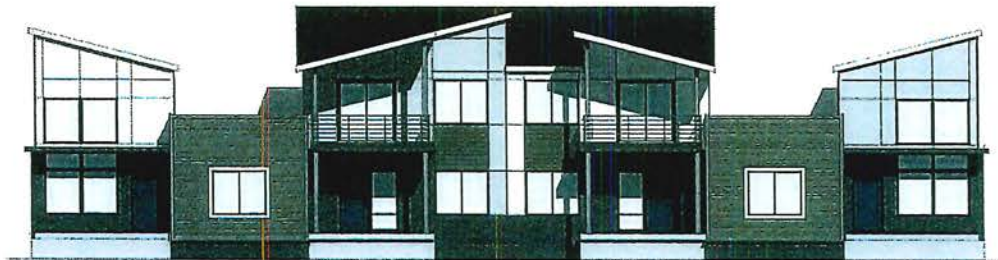
**PRODUCT 3 | COMPARISON**

COLOR PACKAGE: Product 3—#1

GRAY'S STATION | 07.07.2022 | 4



NEW FRONT ELEVATION



OLD FRONT ELEVATION

#### Product 4

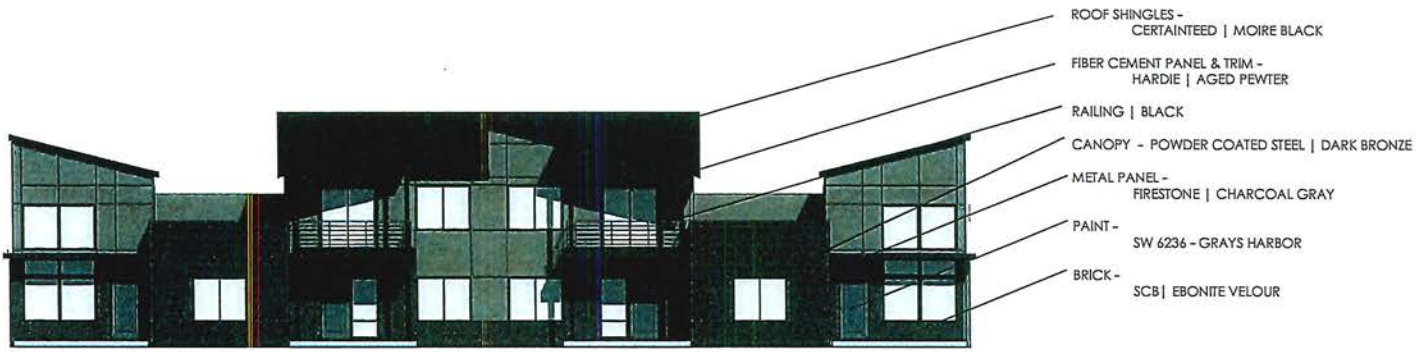
This elevation was too busy and the color scheme too light for this urban environment.

- Changed to a darker color scheme to achieve a more urban look.
- Changed Hardie lap siding on front to metal panels.
- Eliminated the white-colored Hardie panels and matched the panel color on both sides to achieve a unified appearance.
- Steel metal posts will be painted dark gray.

## PRODUCT 4 | COMPARISON

COLOR PACKAGE: Product 4—#1

GRAY'S STATION | 07.07.2022 | 5

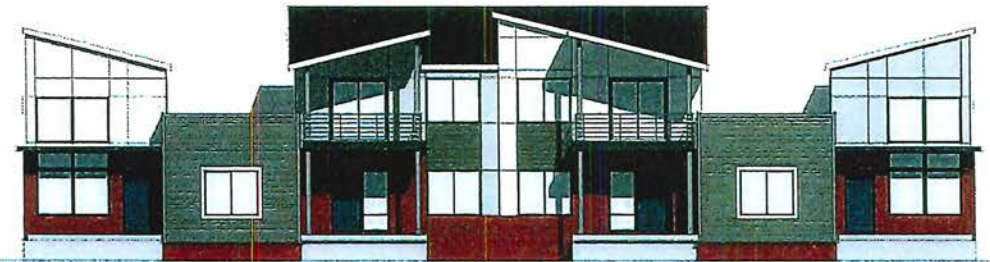


NEW FRONT ELEVATION

**Product 4**

This elevation was too busy and the color scheme too light for this urban environment.

- Changed to a darker color scheme to achieve a more urban look.
- Changed Hardie lap siding on front to metal panels.
- Eliminated the white-colored Hardie panels and matched the panel color on both sides to achieve a unified appearance.
- Steel metal posts will be painted dark gray.

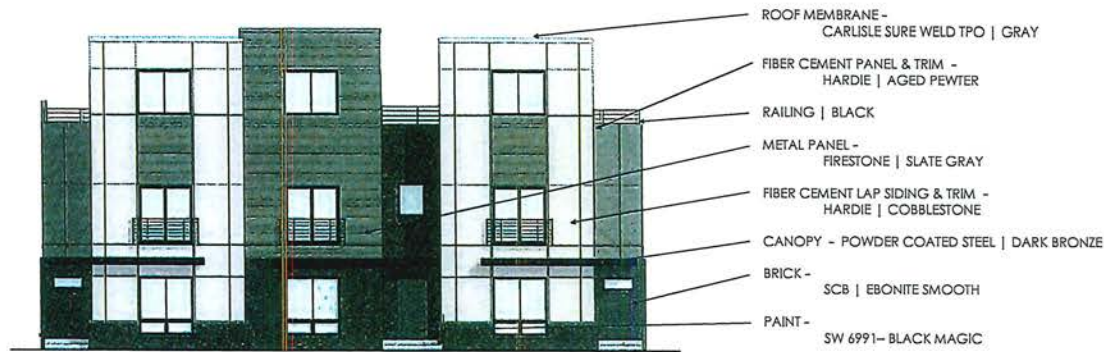


OLD FRONT ELEVATION

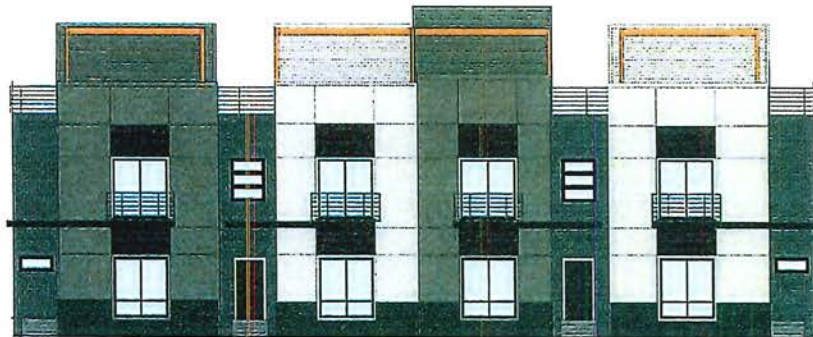
**PRODUCT 4 | COMPARISON**

COLOR PACKAGE: Product 4—#2

GRAY'S STATION | 07.07.2022 | 6



NEW FRONT ELEVATION



OLD FRONT ELEVATION

### Product 5

Elevations appear repetitive and uninspired. We only built one Product 5 building in Phase 1 so far. It was built without the 3<sup>rd</sup> level optional bonus room. We intend to build the bonus room on Product 5 in Phase 2.

- Varied the height of the brick to create diverse building elements and to reduce the effect of the stark vertical building elements.
- Replaced the Hardie panels of one of the main building elements with metal panels.
- Black windows

## PRODUCT 5 | COMPARISON

COLOR PACKAGE: Product 5—#1

GRAY'S STATION | 07.07.2022 | 7





NEW FRONT ELEVATION



OLD FRONT ELEVATION

#### Product 6

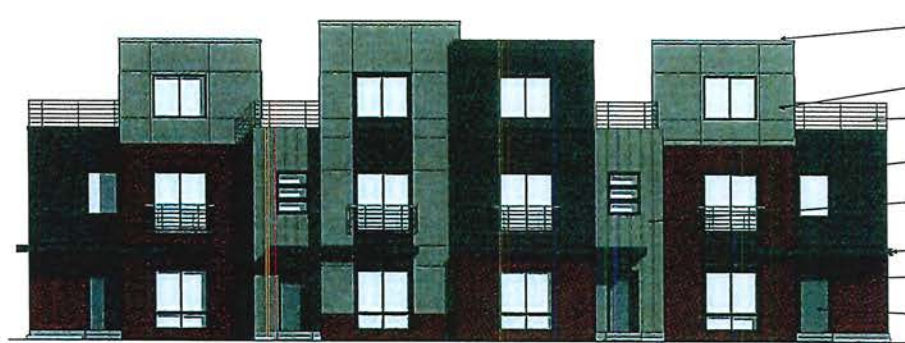
Elevation appeared boring and repetitive. These needed additional visual interest and a more urban look.

- Varied the height of the brick/stone to create diverse building elements and to reduce the repetitive effect of the vertical building elements.
- Introduced building elements sided in metal panels with a vertical pattern.
- Replaced the Hardie panels of one or more of the main building elements with metal panels (horizontal pattern).
- Black windows
- Darkened the color packages

## PRODUCT 6 | COMPARISON

COLOR PACKAGE: Product 6—#1

GRAY'S STATION | 07.07.2022 | 8



NEW FRONT ELEVATION

- ROOF MEMBRANE - CARLISLE SURE WELD TPO | GRAY
- FIBER CEMENT PANEL & TRIM - HARDIE | AGED PEWTER
- RAILING | BLACK
- METAL PANEL - FIRESTONE | DARK BRONZE
- VERTICAL METAL PANEL - FIRESTONE | SLATE GRAY
- CANOPY - POWDER COATED STEEL | DARK BRONZE
- BRICK - SCB | MOUNTAIN SHADOW SMOOTH
- PAINT - SW 6991- BLACK MAGIC



OLD FRONT ELEVATION

### Product 6

Elevation appeared boring and repetitive. These needed additional visual interest and a more urban look.

- Varied the height of the brick/stone to create diverse building elements and to reduce the repetitive effect of the vertical building elements.
- Introduced building elements sided in metal panels with a vertical pattern.
- Replaced the Hardie panels of one or more of the main building elements with metal panels (horizontal pattern).
- Black windows
- Darkened the color packages

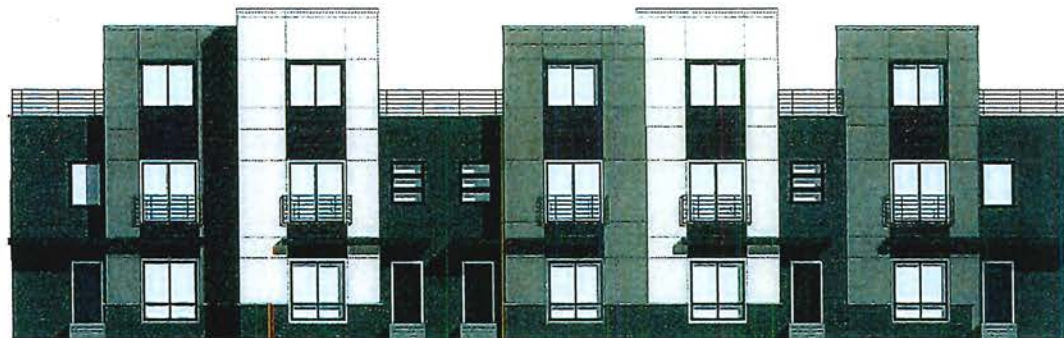
## PRODUCT 6 | COMPARISON

COLOR PACKAGE: Product 6—#2

GRAY'S STATION | 07.07.2022 | 9



NEW FRONT ELEVATION



OLD FRONT ELEVATION

### Product 6

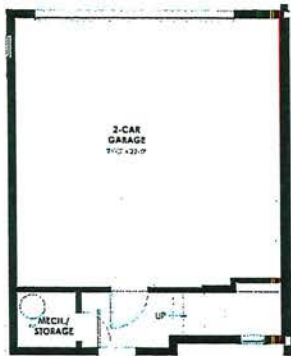
Elevation appeared boring and repetitive. These needed additional visual interest and a more urban look.

- Varied the height of the brick/stone to create diverse building elements and to reduce the repetitive effect of the vertical building elements.
- Introduced building elements sided in metal panels with a vertical pattern.
- Replaced the Hardie panels of one or more of the main building elements with metal panels (horizontal pattern).
- Black windows
- Darkened the color packages

## PRODUCT 6 | COMPARISON

COLOR PACKAGE: Product 6—#3

GRAY'S STATION | 07.07.2022 | 10



FIRST FLOOR



SECOND FLOOR

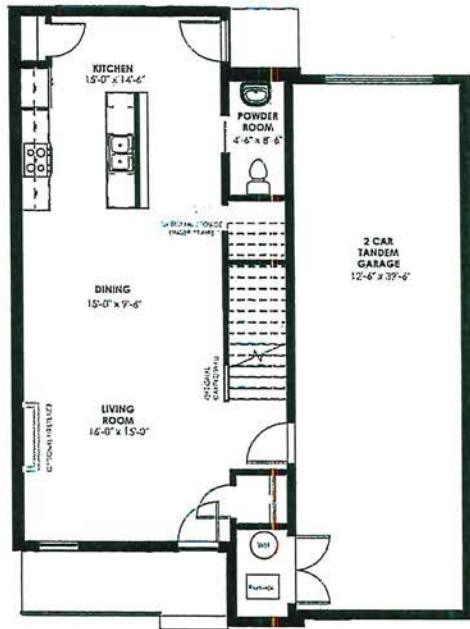


THIRD FLOOR

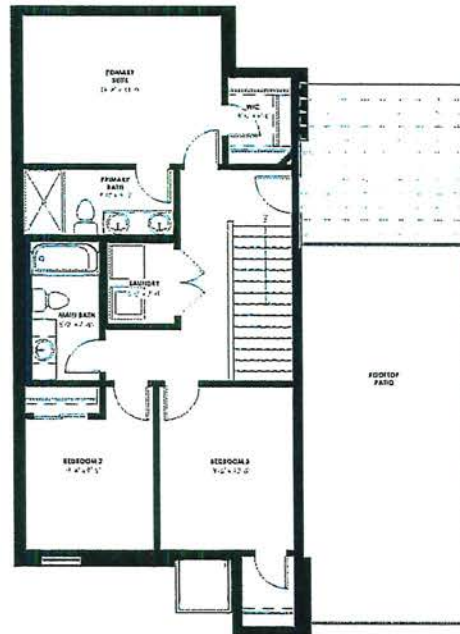
## PRODUCT 1 | PRESTON

Lots: 61-63 and 64-66

GRAY'S STATION | 07.07.2022 | 45

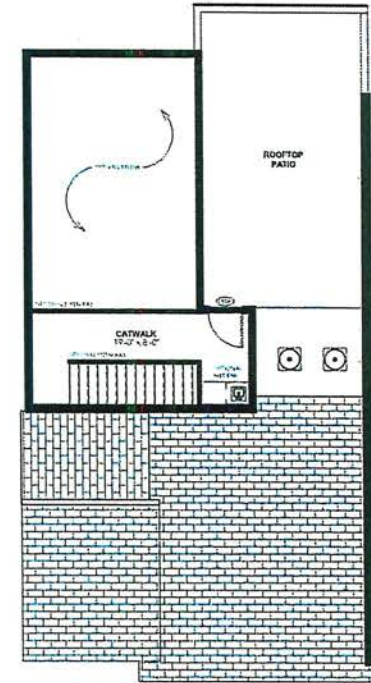
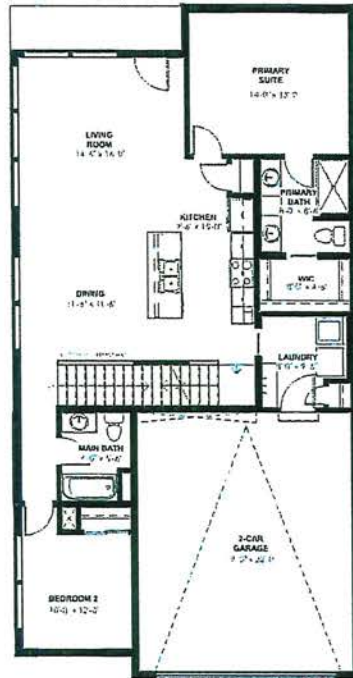
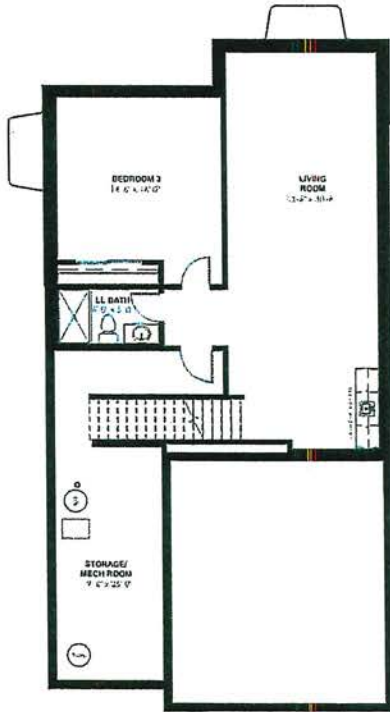


FIRST FLOOR



SECOND FLOOR

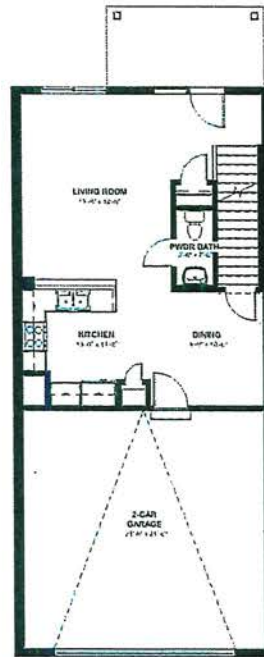
PRODUCT 3 | BEXLEY  
 Lots: 57-60  
 GRAY'S STATION | 07.07.2022 | 46



PRODUCT 4 | MAISON  
 Lots: 16&19, 20&23, 24&27 and 52&56  
 GRAY'S STATION | 07.07.2022 | 47



LOWER LEVEL



FIRST FLOOR



SECOND FLOOR

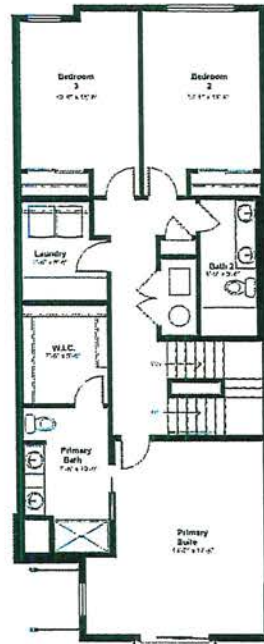
## PRODUCT 4 | MARLOW

Lots: 17&18, 21&22, 25&26, and 53-55

GRAY'S STATION | 07.07.2022 | 48



FIRST FLOOR



SECOND FLOOR



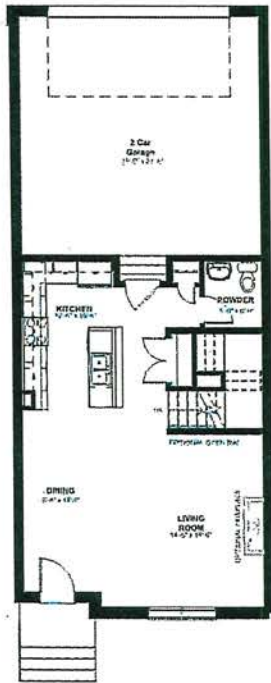
THIRD FLOOR

## PRODUCT 5 | CHAVA

Lots: 46&48 and 49&51

GRAY'S STATION | 07.07.2022 | 49

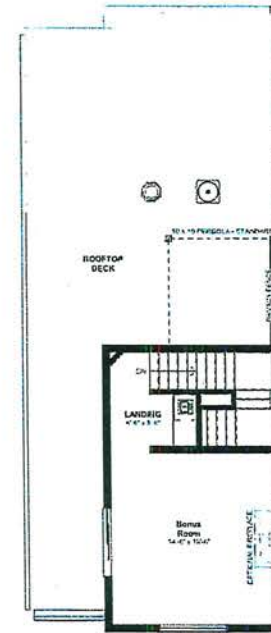




FIRST FLOOR



SECOND FLOOR

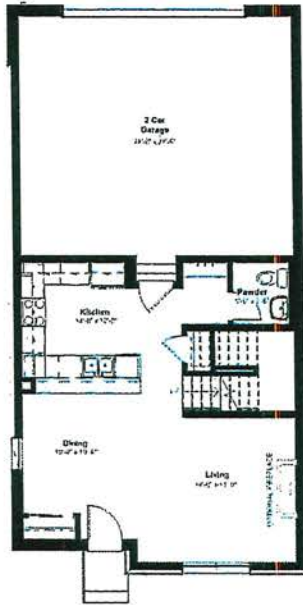


THIRD FLOOR

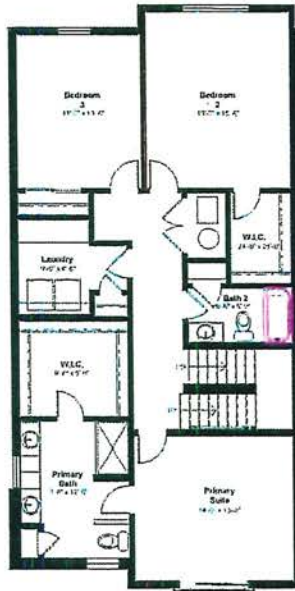
PRODUCT 5 | ASANA

Lots: 47 and 50

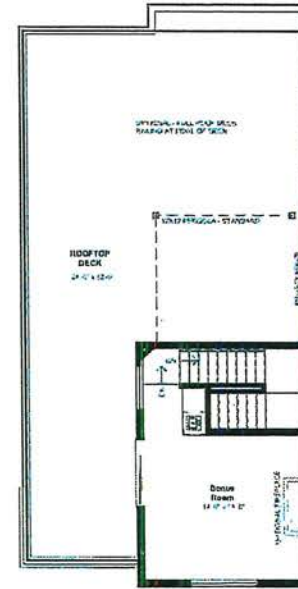
GRAY'S STATION | 07.07.2022 | 50



FIRST FLOOR



SECOND FLOOR



THIRD FLOOR

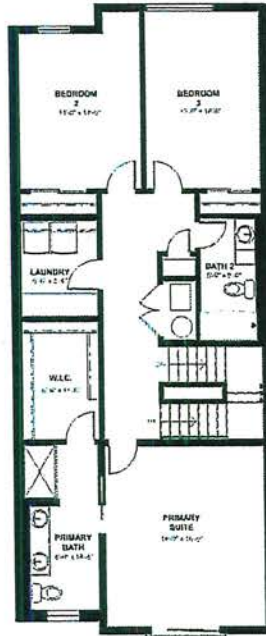
## PRODUCT 6 | GRAYSON

Lots: 1&3, 4&7, 8&11, 12&15, 29&32, 33&36, 37&40, and 41&45

GRAY'S STATION | 07.07.2022 | 51



FIRST FLOOR

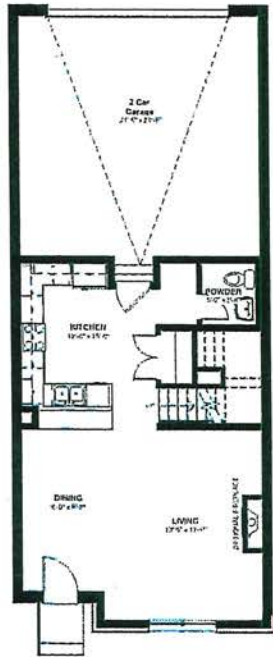


SECOND FLOOR



THIRD FLOOR

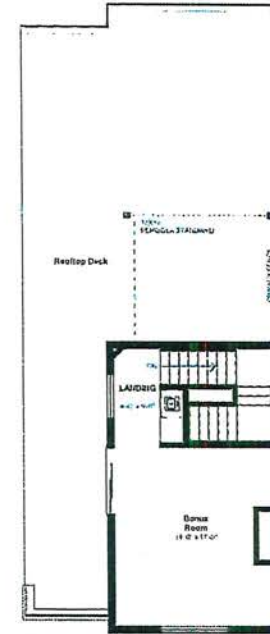
PRODUCT 6 | CORDOVA  
 Lots: 2, 5, 9, 13, 30, 34, 38 and 42&44  
 GRAY'S STATION | 07.07.2022 | 52



FIRST FLOOR



SECOND FLOOR



THIRD FLOOR

## PRODUCT 6 | CORDOVA PLUS

Lots: 6, 10, 14, 31, 35, 39 and 43

GRAY'S STATION | 07.07.2022 | 53



Hubbell Homes  
Des Moines, Iowa

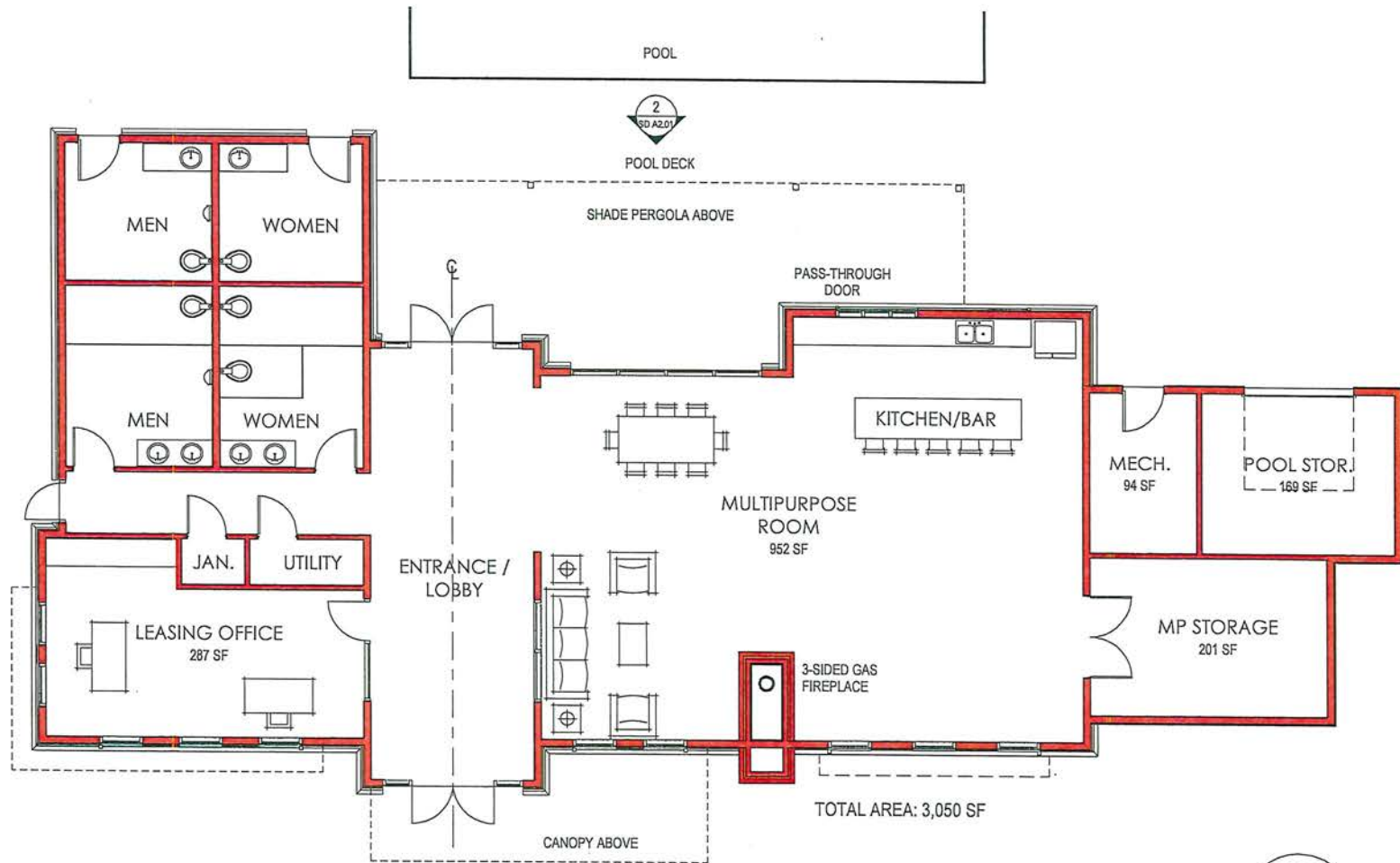
PERSPECTIVE

GRAY'S STATION CLUBHOUSE

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[AM220361]





Hubbell Homes  
Des Moines, Iowa

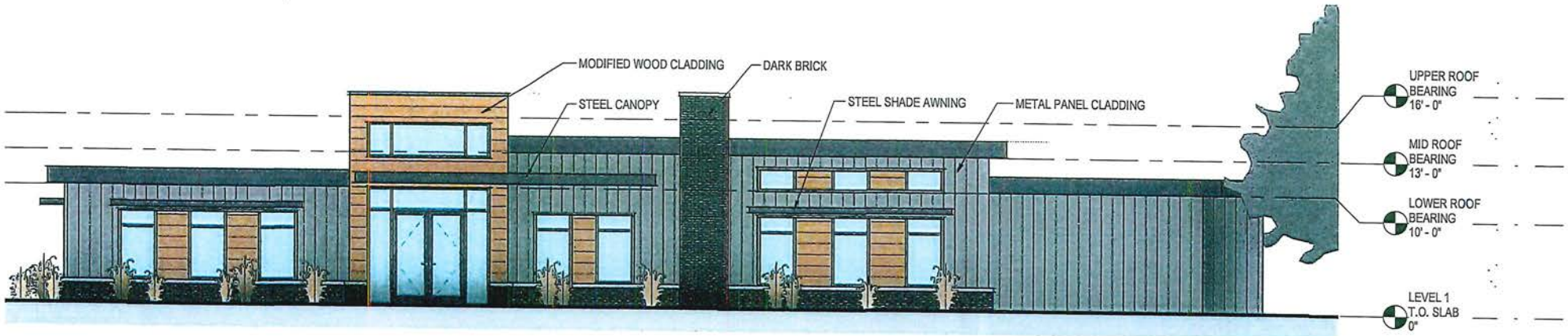
CONCEPTUAL FLOOR PLAN 7/14/2022

GRAY'S STATION CLUBHOUSE



The drawings presented are illustrative character and design intent only, and are subject to change based upon final design considerations (i.e. applicable codes, structural, and MEP design requirements, unit manufacturer plan changes, etc.) © 2016 BSB Design, Inc.

[AM220361]



1 SOUTH ELEVATION  
1/8" = 1'-0"



2 NORTH ELEVATION  
1/8" = 1'-0"

ELEVATIONS

Hubbell Homes  
Des Moines, Iowa

GRAY'S STATION CLUBHOUSE

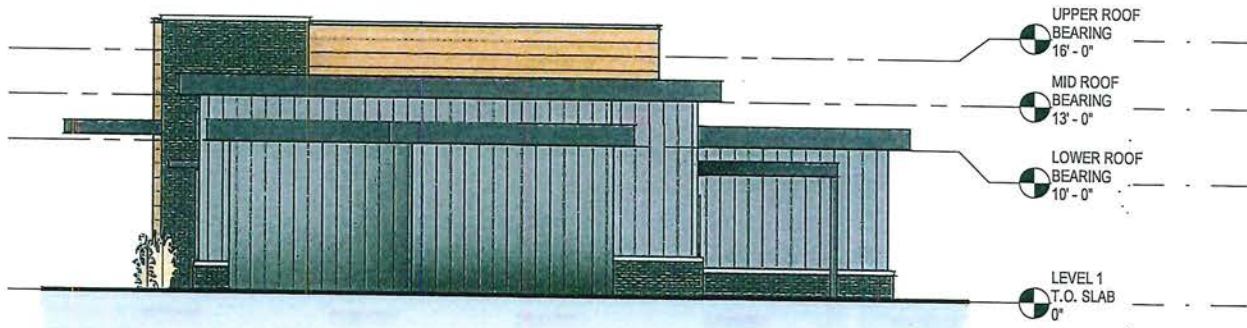
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[AM220361]





1 WEST ELEVATION  
1/8" = 1'-0"



2 EAST ELEVATION  
1/8" = 1'-0"

ELEVATIONS