



Date March 6, 2023

RESOLUTION HOLDING HEARING ON REQUEST FROM HUBBELL REALTY COMPANY (OWNER), REPRESENTED BY CALEB SMITH (OFFICER), FOR REVIEW AND APPROVAL OF PUD FINAL DEVELOPMENT PLAN FOR “GRAY’S STATION PLAT 5 TOWNHOMES AND CLUBHOUSE,” FOR PROPERTY LOCATED IN THE VICINITY OF 1300 TUTTLE STREET

WHEREAS, on February 20, 2023, by Roll Call No. 23-0226, the City Council received a communication from the City Plan and Zoning Commission advising that the Commission voted 8-1-1 to **APPROVE** a request from Hubbell Realty Company (Owner), represented by Caleb Smith (Officer), to approve the PUD Final Development Plan for “Gray’s Station Plat 5 Townhomes and Clubhouse” for property located in the vicinity of 1300 Tuttle Street to allow development of the property with the construction of 65 townhomes and a clubhouse amenity building, subject to compliance with all administrative review requirements and the following:

1. Provision of a sidewalk connection from the clubhouse’s outdoor recreation area/dog park that crosses a private drive to link up with a proposed courtyard space in between lots “40” and “41.”
2. Verification that any proposed rooftop mechanical equipment will be screened from view from all sides.
3. Provision of additional information regarding mechanical equipment and associated screening (both ground- and roof-mounted) for the clubhouse building.
4. Provision of more-detailed materiality information for the clubhouse building that matches the standards outlined within the PUD Conceptual Plan.
5. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25; and

WHEREAS, the Property is legally described as follows:

OUTLOTS Y AND Z AND A PART OF LOTS 6, 7 AND 9, CENTRAL DES MOINES INDUSTRIAL PARK, AN OFFICIAL PLAT AND PART OF LOTS 80, 81, 82, 85 AND 86 IN FACTORY ADDITION, AN OFFICIAL PLAT, ALL BEING IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID LOT 9, CENTRAL DES MOINES INDUSTRIAL PARK HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHING 575161.83 AND EASTING 1602813.57; THENCE SOUTH 89° (DEGREES) 48' (MINUTES) 50" (SECONDS) EAST ALONG THE



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NORTHERLY LINE OF SAID LOT 9, A DISTANCE OF 339.93 FEET TO THE SOUTHWEST CORNER OF LOT 2, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°46'52" EAST CONTINUING ALONG SAID NORTHERLY LINE, 419.55 FEET TO THE SOUTHEAST CORNER OF SAID LOT 2 ALSO BEING A POINT ON THE WEST RIGHT OF WAY LINE OF SW 14TH STREET; THENCE SOUTH 00°01'02" EAST CONTINUING ALONG SAID NORTHERLY LINE, 69.98 FEET TO THE SOUTHWEST CORNER OF STREET LOT B, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°35'21" EAST CONTINUING ALONG SAID NORTHERLY LINE, 70.00 FEET TO THE NORTHWEST CORNER OF LOT 8, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 00°08'06" WEST CONTINUING ALONG SAID NORTHERLY LINE, 423.70 FEET TO THE SOUTHWEST CORNER OF SAID LOT 8; THENCE SOUTH 89°19'18" EAST CONTINUING ALONG SAID NORTHERLY LINE, 307.20 FEET TO THE SOUTHEAST CORNER OF SAID LOT 8, ALSO BEING THE NORTHEAST CORNER OF SAID LOT 9; THENCE NORTH 00°08'11" WEST ALONG THE WESTERLY LINE OF SAID LOT 7, CENTRAL DES MOINES INDUSTRIAL PARK, 333.39 FEET; THENCE NORTHERLY CONTINUING ALONG SAID WESTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 804.20 FEET, WHOSE ARC LENGTH IS 93.82 FEET AND WHOSE CHORD BEARS NORTH 03°29'07" WEST, 93.77 FEET; THENCE NORTH 06°58'06" WEST CONTINUING ALONG SAID WESTERLY LINE, 15.78 FEET TO THE NORTHWEST CORNER OF SAID LOT 7 ALSO BEING THE SOUTH RIGHT OF WAY LINE OF TUTTLE STREET; THENCE NORTH 81°10'49" EAST ALONG THE NORTH LINE OF SAID LOT 7, A DISTANCE OF 411.78 FEET TO THE NORTHEAST CORNER OF SAID LOT 7; THENCE NORTH 00°05'48" WEST ALONG THE WEST LINE OF SAID LOT 6, CENTRAL DES MOINES INDUSTRIAL PARK, 72.11 FEET TO THE NORTHWEST CORNER OF SAID LOT 6; THENCE NORTH 81°52'47" EAST ALONG THE NORTH LINE OF SAID LOT 6, A DISTANCE OF 347.62 FEET TO THE SOUTHEAST CORNER OF LOT 4, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE NORTH 00°31'45" WEST ALONG THE WESTERLY LINE OF SAID OUTLOT Y, CENTRAL DES MOINES INDUSTRIAL PARK, 63.43 FEET; THENCE NORTH 87°29'10" EAST CONTINUING ALONG SAID WESTERLY LINE, 10.37 FEET; THENCE NORTH 00°12'49" WEST CONTINUING ALONG SAID WESTERLY LINE, 554.99 FEET TO THE NORTHEAST CORNER OF LOT 5, SAID CENTRAL DES MOINES INDUSTRIAL PARK AND HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHING 575907.16 AND EASTING 1604699.48; THENCE SOUTH 89°04'27" EAST ALONG THE NORTH LINE OF SAID OUTLOT Y, 15.36 FEET TO THE NORTHEAST CORNER OF SAID OUTLOT Y; THENCE NORTH 00°21'12" WEST ALONG THE WEST LINE OF SAID LOT 85, FACTORY ADDITION, 8.59 FEET TO THE SOUTHERLY RIGHT OF WAY LINE OF MARTIN LUTHER KING JR PARKWAY; THENCE NORTH 86°10'40" EAST ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 100.23 FEET; THENCE NORTH 84°40'04" EAST CONTINUING ALONG SAID SOUTHERLY RIGHT OF WAY



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LINE, 199.44 FEET TO THE WESTERLY RIGHT OF WAY LINE OF SW 11TH STREET; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 173.21 FEET; THENCE SOUTH 74°42'29" WEST, 99.77 FEET; THENCE SOUTH 15°25'34" EAST, 75.29 FEET; THENCE NORTH 74°32'27" EAST, 99.81 FEET TO SAID WESTERLY RIGHT OF WAY LINE; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 344.37 FEET TO A CORNER ON THE EASTERLY LINE OF SAID LOT 81; THENCE SOUTH 39°15'32" WEST ALONG SAID EASTERLY LINE, 97.08 FEET; THENCE SOUTH 79°27'19" WEST, 73.56 FEET TO THE EASTERLY LINE OF THE WEST HALF OF SAID LOT 81; THENCE SOUTHERLY ALONG SAID EASTERLY LINE AND A CURVE CONCAVE EASTERLY WHOSE RADIUS IS 716.25 FEET, WHOSE ARC LENGTH IS 712.02 FEET AND WHOSE CHORD BEARS SOUTH 06°02'51" WEST, 683.06 FEET; THENCE SOUTH 67°48'12" WEST, 51.41 FEET TO A POINT ON THE EAST LINE OF THE WEST 100 FEET OF SAID LOT 80, FACTORY ADDITION; THENCE SOUTH 00°01'40" EAST ALONG SAID EAST LINE, 767.72 FEET; THENCE SOUTH 45°36'19" EAST, 692.69 FEET TO THE WESTERLY LINE OF SAID LOT 81; THENCE SOUTH 76°09'29" EAST, 50.00 FEET TO SAID EASTERLY LINE OF THE WEST HALF OF LOT 81, HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHING 573317.97 AND EASTING 1605461.84; THENCE SOUTHERLY ALONG SAID EASTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 740.65 FEET, WHOSE ARC LENGTH IS 76.04 FEET AND WHOSE CHORD BEARS SOUTH 17°00'55" WEST, 76.01 FEET; THENCE NORTH 70°20'25" WEST, 50.00 FEET TO SAID WESTERLY LINE OF LOT 81; THENCE NORTH 45°36'19" WEST, 663.55 FEET TO SAID EAST LINE OF THE WEST 100 FEET OF LOT 80; THENCE SOUTH 00°01'40" EAST ALONG SAID EAST LINE, 439.03 FEET TO THE NORTH BANK AND MEANDER LINE OF THE RACCOON RIVER; THENCE NORTH 71°44'57" WEST AND ALONG SAID MEANDER LINE AND THE SOUTHERLY LINE OF SAID OUTLOT Z, CENTRAL DES MOINES INDUSTRIAL PARK, 888.50 FEET; THENCE NORTH 83°01'49" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 463.82 FEET; THENCE NORTH 89°10'53" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 291.24 FEET; THENCE SOUTH 82°06'06" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 296.82 FEET; THENCE SOUTH 72°02'02" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 228.32 FEET TO THE SOUTHWEST CORNER OF SAID OUTLOT Z; THENCE NORTH 00°03'17" EAST ALONG THE WEST LINE OF SAID CENTRAL DES MOINES INDUSTRIAL PARK, 1647.05 FEET TO THE POINT OF BEGINNING AND CONTAINING 83.73 ACRES (3,647,186 SQUARE FEET) AND IS SUBJECT TO ANY AND ALL EASEMENTS OF RECORD.

WHEREAS, on February 6, 2023, by Roll Call No. 23-0225, it was duly resolved by the City Council that the request for approval of the PUD Final Development Plan be set down for hearing on March 6, 2023, at 5:00 p.m. at City Hall Council Chambers, 400 Robert D. Ray Dr., Des Moines, Iowa 50309; and



Date March 6, 2023

WHEREAS, due notice of said hearing was published in the Des Moines Register, as provided by law, setting forth the time and place for hearing on said proposed PUD Final Development Plan; and

WHEREAS, in accordance with said notice, those interested in said proposed PUD Final Development Plan, both for and against, have been given opportunity to be heard with respect thereto and have presented their views to the City Council.

NOW, THEREFORE, BE IT RESOLVED, by the City Council of the City of Des Moines, Iowa, as follows:

1. Upon due consideration of the facts, and any and all statements of interested persons and arguments of counsel, any objections to the proposed “Gray’s Station Plat 5 Townhomes and Clubhouse” PUD Final Development Plan are hereby overruled, and the hearing is closed.
2. The proposed Amendment to the “Gray’s Station Plat 5 Townhomes and Clubhouse” PUD Final Plan, as on file in the Development Services Department, is hereby approved subject to compliance with all administrative review comments as well as those requirements stated above.

Moved by _____ to adopt. Second by _____

APPROVED AS TO FORM:

/s/ Chas M. Cahill
Chas M. Cahill
Assistant City Attorney

(SITE-2022-000153)

COUNCIL ACTION	YEAS	NAYS	PASS	ABSENT
COWNIE				
BOESEN				
GATTO				
MANDELBAUM				
SHEUMAKER				
VOSS				
WESTERGAARD				
TOTAL				

MOTION CARRIED APPROVED

Mayor

CERTIFICATE

I, LAURA BAUMGARTNER, City Clerk of said City hereby certify that at a meeting of the City Council of said City of Des Moines, held on the above date, among other proceedings the above was adopted.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my seal the day and year first above written.

City Clerk

February 14, 2023

Communication from the City Plan and Zoning Commission advising that at their February 2, 2023 meeting, the following action was taken regarding a request from Hubbell Realty Company (owner), represented by Caleb Smith (officer), for the following regarding property located in the vicinity of 1300 Tuttle Street:

- A) Review and approval of a 1st amendment to the Gray's Station PUD Conceptual Plan, to update the realignment of Southwest 13th Street and to allow administrative review of future development site plans in leu of public review by the Plan and Zoning Commission and City Council.
- B) Review and approval of a PUD Final Development Plan "Gray's Station Telus Condos," to allow for the construction of two condo buildings containing 84 residential units.
- C) Review and approval of a PUD Final Development Plan "Gray's Station Plat 5 Townhomes and Clubhouse," to allow for the construction of 65 townhomes and a clubhouse amenity building.

COMMISSION RECOMMENDATION:

After public hearing, the members voted 8-1-1 as follows.

Commission Action:	Yes	Nays	Pass	Absent
Francis Boggus				X
Dan Drendel	X			
Leah Rudolphi	X			
Carol Maher		X		
Abby Chungath				X
Kayla Berkson	X			
Chris Draper			X	
Todd Garner				X
Johnny Alcivar	X			
Justyn Lewis	X			
Carolyn Jenison				X
William Page	X			
Andrew Lorentzen				X
Emily Webb	X			
Katie Gillette	X			

APPROVAL of Part A) Approval of the request to amend the Gray's Station PUD Conceptual Plan to update the realignment of Southwest 13th Street.

Furthermore, Staff recommends limited approval of the request to amend the Gray's Station PUD Conceptual Plan to revise the 'Notes' section to allow administrative review of future development site plans in lieu of public review by the Plan and Zoning Commission and City Council, as follows:

- Any final development plan for a new row home type development or a minor revision to an existing development is subject to administrative (by staff) review and approval.
- Any final development plan for a mixed-use building or a multiple-household residential building is subject to review and approval by the Plan and Zoning Commission, with any appeal of that decision considered by the City Council.

Part B) Approval of the proposed PUD Final Development Plan "Gray's Station Telus Condos," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a "band" of architectural concrete masonry unit materials from grade up to 2' for the standalone garages that matches the concrete masonry units proposed for the condo buildings.
2. Provision of a sidewalk connection from the southeastern corner of "Building 2" that crosses the internal vehicular drive to link up with the north-south pedestrian pathway that bisects this development area.
3. Provision of more-detailed exterior façade materiality information that matches the standards outlined within the PUD Conceptual Plan.
4. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

Part C) Approval of the proposed PUD Final Development Plan "Gray's Station Plat 5 Townhomes and Clubhouse," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a sidewalk connection from the clubhouse's outdoor recreation area/dog park that crosses a private drive to link up with a proposed courtyard space in between lots "40" and "41."
2. Verification that any proposed rooftop mechanical equipment will be screened from view from all sides.
3. Provision of additional information regarding mechanical equipment and associated screening (both ground- and roof-mounted) for the clubhouse building.
4. Provision of more-detailed materiality information for the clubhouse building that matches the standards outlined within the PUD Conceptual Plan.

5. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

STAFF RECOMMENDATION TO THE P&Z COMMISSION

Part A) Staff recommends approval of the request to amend the Gray's Station PUD Conceptual Plan to update the realignment of Southwest 13th Street.

Furthermore, Staff recommends limited approval of the request to amend the Gray's Station PUD Conceptual Plan to revise the 'Notes' section to allow administrative review of future development site plans in lieu of public review by the Plan and Zoning Commission and City Council, as follows:

- Any final development plan for a new row home type development or a minor revision to an existing development is subject to administrative (by staff) review and approval.
- Any final development plan for a mixed-use building or a multiple-household residential building is subject to review and approval by the Plan and Zoning Commission, with any appeal of that decision considered by the City Council.

Part B) Staff recommends approval of the proposed PUD Final Development Plan "Gray's Station Telus Condos," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a "band" of architectural concrete masonry unit materials from grade up to 2' for the standalone garages that matches the concrete masonry units proposed for the condo buildings.
2. Provision of a sidewalk connection from the southeastern corner of "Building 2" that crosses the internal vehicular drive to link up with the north-south pedestrian pathway that bisects this development area.
3. Provision of more-detailed exterior façade materiality information that matches the standards outlined within the PUD Conceptual Plan.
4. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

Part C) Staff recommends approval of the proposed PUD Final Development Plan "Gray's Station Plat 5 Townhomes and Clubhouse," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a sidewalk connection from the clubhouse's outdoor recreation area/dog park that crosses a private drive to link up with a proposed courtyard space in between lots "40" and "41."
2. Verification that any proposed rooftop mechanical equipment will be screened from view from all sides.

3. Provision of additional information regarding mechanical equipment and associated screening (both ground- and roof-mounted) for the clubhouse building.
4. Provision of more-detailed materiality information for the clubhouse building that matches the standards outlined within the PUD Conceptual Plan.
5. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

STAFF REPORT TO THE PLANNING COMMISSION

I. GENERAL INFORMATION

1. **Purpose of Request:** The applicant is requesting an amendment to the Gray's Station PUD Conceptual Plan, which would alter the alignment of Southwest 13th Street between Tuttle Street and Grays Parkway. Further, the applicant is proposing to amend the PUD's 'Notes' to remove the requirement that "All Final Development Plans are subject to review and approval by the Plan & Zoning Commission and the City Council," which would instead allow administrative review in lieu.

Additionally, the developer is seeking to subdivide the 'Gray's Station Plat 5' area within the PUD to allow the development and construction of 65 one-household residential lots containing row homes, a clubhouse amenity building, a lot containing two condo buildings with 84 residential units, and two outlots.

2. **Size of Site:** Gray's Station PUD – 84.4 acres; Plat 5 Area – 11.1 acres.
3. **Existing Zoning (site):** Gray's Station PUD, Planned Unit Development District.
4. **Existing Land Use (site):** The area is currently undeveloped land.
5. **Adjacent Land Use and Zoning:**

North – "DX2"; Uses are Tuttle Street, open space, and vacant industrial buildings.

South – "Gray's Station PUD" & "F"; Uses are open space, a city stormwater management facility, the Raccoon River, and Gray's Lake Park.

East – "Gray's Station PUD"; Uses are townhomes and multiple-household dwelling units.

West – "Gray's Station PUD" & "DX2"; Uses are industrial warehousing and undeveloped land.

6. **General Neighborhood/Area Land Uses:** The subject property is located generally to the south of West Martin Luther King, Jr. Parkway and to the west of Southwest 11th Street in the southern fringes of Downtown. The site's vicinity consists of mixed-use and medium-density residential development, parkland, and undeveloped parcels. The Raccoon River flanks the southern boundary of the site. Gray's Lake Park lies further south of the river.

- 7. Applicable Recognized Neighborhood(s):** The subject property is located within the Downtown Des Moines Neighborhood Association. All neighborhood associations were notified of the public hearing by emailing of the Preliminary Agenda on January 13, 2023, and by emailing of the Final Agenda on January 27, 2023. Additionally, separate notifications of the hearing for this specific item were mailed on January 13, 2023 (20 days prior to the public hearing) and January 23, 2023 (10 days prior to the public hearing) to the Downtown Des Moines Neighborhood Association and to the primary titleholder on file with the Polk County Assessor for each property within 250 feet of the site.

All agendas and notices are mailed to the primary contact(s) designated by the recognized neighborhood association to the City of Des Moines Neighborhood Services Department on the date of the mailing. The Sheridan Gardens Neighborhood does not have a mailing address on file. The Brook Run Neighborhood Association mailings were sent to Brandon Brown, 120 Southwest 5th Street, Unit 101, Des Moines, IA 50309.

The applicant can provide a summary of the neighborhood outreach at the public hearing.

- 8. Relevant Zoning History:** The subject property was rezoned to "PUD" on August 14, 2017, at which time a general "PUD" Conceptual Plan was adopted.
- 9. PlanDSM: Creating Our Tomorrow Plan Land Use Plan Designation:** Downtown Mixed Use and Neighborhood Mixed Use.
- 10. Applicable Regulations:** Taking into consideration the criteria set forth in Chapter 18B of the Iowa Code, the Commission reviews all proposals to amend zoning boundaries or regulations within the City of Des Moines. Such amendments must be in conformance with the comprehensive plan for the City and designed to meet the criteria in 414.3 of the Iowa Code. The Commission may make recommendations to the City Council on conditions to be made in addition to the existing regulations so long as the subject property owner agrees to them in writing. The recommendation of the Commission will be forwarded to the City Council.

The application, accompanying evidence and "PUD" Conceptual Plan shall be considered by the Plan and Zoning commission at a public hearing. The Commission shall review the conformity of the proposed development with the standards of the City Code and with recognized principles of civic design, land use planning, and landscape architecture. After the hearing, the Commission may vote to recommend either approval or disapproval of the amended "PUD" Conceptual Plan as submitted, or to recommend that the developer amend the plan or request to preserve the intent and purpose of this chapter to promote public health, safety, morals and general welfare. The recommendations of the Commission shall be referred to the City Council.

Taking into consideration the criteria set forth in Chapter 18B of the Iowa Code, the Commission shall determine if such Preliminary Plat conforms to the standards and requirements outlined in Chapter 354 of the Iowa Code, and the City Subdivision Ordinance and shall approve, conditionally approve or reject such Plat within 45 days after the date of submission to the City Permit and Development Center. Unless the applicant agrees in writing to an extension of time, the Preliminary Plat shall be

deemed approved if the Commission does not act within such 45-day period. The Commission's action for approval or conditional approval shall be null and void unless the Final Plat is submitted to the City Permit and Development Center within 270 days after the date of such action; provided, however, that the Permit and Development Administrator may grant, upon written request of the applicant, up to a 90-day extension for submittal of the Final Plat to the City Permit and Development Center.

Taking into consideration the criteria set forth in Chapter 18B of the Iowa Code, the Commission shall determine if such Preliminary Plat conforms to the standards and requirements outlined in Chapter 354 of the Iowa Code, and the City Subdivision Ordinance and shall approve, conditionally approve or reject such Plat within 45 days after the date of submission to the City Permit and Development Center. Unless the applicant agrees in writing to an extension of time, the Preliminary Plat shall be deemed approved if the Commission does not act within such 45-day period. The Commission's action for approval or conditional approval shall be null and void unless the Final Plat is submitted to the City Permit and Development Center within 270 days after the date of such action; provided, however, that the Permit and Development Administrator may grant, upon written request of the applicant, up to a 90-day extension for submittal of the Final Plat to the City Permit and Development Center.

Pursuant to Section 135-9.1.1.B of the Planning and Design Ordinance, the site plan review requirements of Chapter 135 are designed to ensure the orderly and harmonious development of property in a manner that shall:

- Promote the most beneficial relation between present and proposed future uses of land and the present and proposed future circulation of traffic throughout the city;
- Permit present development of property commensurate with fair and orderly planning for future development of other properties in the various areas of the city with respect to the availability and capacity, present and foreseeable, of public facilities and services. The factors to be considered in arriving at a conclusion concerning proposed present development of property shall include the following:
 - The maximum population density for the proposed development, the proposed density of use, and consideration of the effect the proposal will have on the capacity of existing water and sanitary sewer lines to the end that existing systems will not become overloaded or capacity so substantially decreased that site use will inhibit or preclude planned future development;
 - Zoning restrictions at the time of the proposal;
- The city's comprehensive plan;
- The city's plans for future construction and provision for public facilities and services; and

- The facilities and services already available to the area which will be affected by the proposed site use;
- Encourage adequate provision for surface and subsurface drainage, in order to ensure that future development and other properties in various areas of the city will not be adversely affected;
- Provide suitable screening of parking, truck loading, refuse and recycling disposal, and outdoor storage areas from adjacent residential districts;
- Encourage the preservation of canopied areas and mature trees and require mitigation for the removal of trees; and
- Consider the smart planning principles set forth in Iowa Code Chapter 18B.

II. ADDITIONAL APPLICABLE INFORMATION

1. **Gray's Station PUD Conceptual Plan:** If the proposed amendment to the Gray's Station "PUD" is approved by the City Council, the applicant must submit to the Planning and Urban Design Administrator a revised version of the "PUD" Conceptual Plan that reflects any conditions of approval.
2. **Realignment of Southwest 13th Street:** The proposed amendment to the PUD incorporates a slight realignment of Southwest 13th Street. The initial alignment proposed for Southwest 13th Street was straight north-south between Tuttle Street and Grays Parkway. With the proposed realignment, the southern portion of the street curves slightly east to join Grays Parkway. The slight realignment would not change or impact the design of the PUD in any significant manner.
3. **Review and Approval:** Per the existing PUD Conceptual Plan, all final development plans within the PUD are currently subject to review and approval by the Plan and Zoning Commission and the City Council. The proposed amendment would eliminate the need for a Commission and Council review for future development plans and would allow administrative-only review. While an administrative review is appropriate for minor changes within the existing development plans and for row homes, a more-comprehensive review, including approval by the Plan and Zoning Commission, is necessary for larger mixed-use and multi-family residential developments. The PUD is well-established to allow developments such as row homes. However, there is some ambiguity relating to larger mixed-use and multi-family buildings which might be potentially sited in the southern portion of the PUD. Staff feels a public process is reasonably necessary to allow comprehensive review of those future development plans and other larger, mixed-use, and multiple-household residential buildings.
4. **Preliminary Plat:** The proposed preliminary plat would involve the creation and extension of public and private streets to serve the development. As an infill, Downtown site, the street layouts (both public and private) are proposed to complement the City street grid (in terms of both layout and construction) that already exists in this area. Murphy Street and Grays Parkway are proposed to extend to the west, with additional westward extensions of already-existing east-west private streets. A new segment of Southwest 13th Street is proposed to be constructed from Tuttle

Street to Grays Parkway. Additional east-west private streets are proposed in the southern portion of the development.

65 one-household lots would be created for clustered, rowhome-style development. Additionally, larger lots would be created for a neighborhood "clubhouse" building and open space, as well as a lot that would contain (2) 3-story residential buildings for a total of 84 dwelling units.

All new streets would be constructed with urban cross-sections generally consistent with the construction of streets within the Gray's Station area and overall Downtown. Public utilities (sanitary sewer, water service) are proposed to be extended both within public rights-of-way and within private streets. Public storm sewer is also proposed within the development, and the area's overall stormwater management has been contemplated and accounted for with the existing Gray's Station city stormwater facility that sits to the south of this development area.

On-street and off-street pedestrian connections are proposed that would provide linkages both within this development, as well as with the Gray's Station Trail to the south and the recently-platted area to the east. The Tuttle Street Cycle Track project is proposed to be extended through the northern border of this development area. A pedestrian "Paseo" is proposed to be extended from the east and bisect the northern half of the development.

- 5. Development Plan – "Gray's Station Telus Condos":** The Telus condos development plan consists of (2) 3-story residential buildings that would contain 84 dwelling units, as well three standalone garage buildings for motor vehicle parking.

The northern building ("Building 1") is proposed to frame the corner of Tuttle and Southwest 13th Streets, with the southern building ("Building 2") proposed to frame the corner of Murphy and Southwest 13th Streets. The aforementioned Paseo is proposed between the two buildings before intersecting with Southwest 13th Street. A motor vehicle parking area would sit behind the proposed residential buildings. A total of 137 motor vehicle parking spaces (53 uncovered, 84 in garages) are proposed. 20 bicycle parking spaces are proposed adjacent to the Paseo in between the residential buildings.

Building and site design is required to conform to the design criteria set forth in the Gray's Station PUD, specifically the criteria for "High Density Residential" buildings. The buildings are proposed to be clad in a mix of brick, concrete masonry units, fiber cement board, and architectural metal paneling in a manner consistent with the material palette stipulated within the PUD Conceptual Plan. Buildings are proposed to sit abutting street-facing lot lines, with street-facing entrances. Proposed floor plans demonstrate a stacking action amongst the units, with ground floor units opening to the street, and upper-floor units with street- or internal-facing balconies.

- 6. Development Plan – "Gray's Station Plat 5 Row Homes and Clubhouse":** The development plan for the row homes and clubhouse demonstrates 17 individual clusters of 3-5 row homes, as well as a larger area within the southern portion of the development area for the clubhouse and outdoor recreation areas. Each rowhome would sit on its own lot.

Row homes are proposed to be oriented toward either a public street or a designated pedestrian way. Rear-loading garages for motor vehicle parking are proposed for each rowhome. These garages would have space for up to 2 vehicles and are proposed to be accessed from the development's private drives that would function as alleys. Additional motor vehicle parking facilities are proposed to be either on street in a parallel fashion or off of a private drive directly abutting the clubhouse/outdoor recreation area. 20 bicycle parking spaces are demonstrated in a clumped fashion oriented around the outdoor recreation area.

The outdoor recreation area is proposed to consist of a swimming pool, dog park, pickleball courts, landscaping arrangements, and other unprogrammed open space. The clubhouse is designed as a flexible, general-use entertainment space that would open up to the pool area.

Building and individual site design is required to conform to the design criteria set forth in the Gray's Station PUD, specifically the criteria for "Low-Medium Residential" buildings. With the exception of the clubhouse, all buildings that are a part of this development plan are proposed to be at least 2 stories. Variations of 5 different rowhome "product types" are proposed to be placed in clusters throughout the area of this development plan. These product types differ in their building heights, exterior façade materials mixes, color palettes, entryway configurations, and proposed floor plans. Similar to the condo buildings, a mix of fiber cement board paneling, fiber cement board lap siding, architectural metal paneling, brick, stone, and finished concrete surfaces are proposed for the exterior of the row homes. Material placement and percentages differ by specific product type. Individual townhouse clusters are positioned between approximately 5 to 12 feet from the edges of streets or other pedestrian ways. Row homes would contain stoops/porches that would orient toward their respective streets/pedestrian ways.

SUMMARY OF DISCUSSION

Nick Tarpey presented staff report and recommendation.

Chris Draper asked how large a project would need to be before it would come before the commission if the staff recommendation is adopted.

Nick Tarpey stated multi-family or mixed-use projects. Any one household residential project would be administratively reviewed.

Chris Draper asked if there are any future concerns with connectivity to broadband.

Jason Van Essen stated the question of broadband is outside the scope of this PUD. Amendments before the commission would not have an impact.

Will Page asked if the provision "to allow administrative review of future development site plans in lieu of public review by the Plan and Zoning Commission and City Council" is applicable to this project only.

Jason Van Essen stated there is an understood development pattern for the rowhomes and that staff supports reviewing those administratively. Other new project types should

have a public hearing, but staff believes that could be limited to just the Commission, eliminating the need to go before the City Council. These amendments would take effect for future projects.

Johnny Alcivar asked if there was a threshold for rowhome development being administratively reviewed.

Jason Van Essen stated if the staff recommendation is adopted, rowhome development would only be reviewed administratively.

Chris Draper asked if there were thoughts of how different a rowhome development would need to be before its reviewed publicly.

Jason Van Essen stated within the PUD, the developer has created several different designs. City staff feels comfortable reviewing proposed changes given the existing standards within the PUD and past negotiations on previous iterations.

Carol Maher asked if that would give city staff the ability to approve type 2 deign alternatives.

Jason Van Essen stated no, the development will need to match the standards within the PUD. If city staff and the developer disagreed on those standards, it would come before the commission.

Dan Drendel asked if it would be developed under the same PUD if another developer bought out future phases.

Jason Van Essen stated it is possible for another builder to follow the standards within the current PUD. If they wanted to go a different direction, amendments to the PUD would be needed to accommodate new designs.

Chris Draper asked if the existing powerlines would be undergrounded.

Jason Van Essen stated there are a few major lines that will remain.

Caleb Smith, Hubbell Realty Company, 6900 Westown Parkway, WDM stated after a simple restriping plan for the LINC, they thought it would make sense to suggest removing the clause that requires review by the Plan and Zoning Commission and City Council. City staff has come forward with a reasonable approach given the size, style and scope of the townhomes have been well established. It does make sense for the commission to review multi-family projects due to the variety they will have. The overhead utility line is a massive transmission line that would be a multi-million dollar project and something Mid-American Energy isn't supportive of. These plans do include utility easement corridors that will provide all units with telecom communication.

Will Page asked if they could explain their thoughts of darker colors being more appropriate for urban settings.

Caleb Smith stated they learned a lot during the first phase of this development and noticed those darker colors sold better. They also have a network of builders across the country that allows them to see these types of trends.

Carol Maher asked if these are all rental units.

Calen Smith stated there are no rental properties in this phase of the project.

Carol Maher asked about parking around the multi-family development.

Ryan Hardisty, Civil Design Advantage, 4121 NW Urbandale Drive, Urbandale, IA stated the TELUS project has 84 total units with 137 parking spaces. There are 84 total garages with a few being detached.

Carol Maher asked why the parking ratio is more than 1 to 1.

Ryan Hardisty stated given the success of the LINC project, parking issues started to occur once the building was fully rented. Based on those issues, they revamped parking for TELUS.

Carol Maher asked if a parking space is included with purchase of a unit.

Michael Bialas, Hubbell Realty Company, 6900 Westown Parkway, WDM stated the parking spaces and garages will be sold separately. If they are not sold, those spaces would be turned over to the association.

Carol Maher asked if they would offer indoor bike storage.

Michael Bialas stated that is something they are exploring. If all the garages are not sold, they would likely convert one to bike storage.

Carol Maher stated they are only focused on how to fit more cars, instead of encouraging other means of transportation. They should be more forward thinking and include adequate bike parking and EV charging stations.

Emily Webb stated she understands the need for more parking. She lives in an urban rowhouse setting and just having a garage makes it challenging for others to visit you.

Justyn Lewis stated there are parking issues around this area, but he would like to see this development promote bicycling and walkability.

Ryan Hardisty stated they are providing 20 bike parking spaces at the main entrance.

Justyn Lewis asked if that's inside the building.

Ryan Hardisty stated they're exterior, located off SW 13th Street.

Will Page asked if they agree with staff recommendations.

Caleb Smith stated yes.

CHAIRPERSON OPENED THE PUBLIC HEARING

Troy Hall, 2530 University Avenue stated he is generally supportive of this development and would like to continue seeing densification of downtown. He would encourage the developer to embrace architectural diversity and encourage different means of transportation.

CHAIRPERSON CLOSED THE PUBLIC HEARING

Chris Draper stated he hopes we aren't straying away from the underlying strategies within tax abatement that would reduce parking ratios.

COMMISSION ACTION:

Will Page made a motion for:

Part A) Approval of the request to amend the Gray's Station PUD Conceptual Plan to update the realignment of Southwest 13th Street.

Furthermore, Staff recommends limited approval of the request to amend the Gray's Station PUD Conceptual Plan to revise the 'Notes' section to allow administrative review of future development site plans in lieu of public review by the Plan and Zoning Commission and City Council, as follows:

- Any final development plan for a new row home type development or a minor revision to an existing development is subject to administrative (by staff) review and approval.
- Any final development plan for a mixed-use building or a multiple-household residential building is subject to review and approval by the Plan and Zoning Commission, with any appeal of that decision considered by the City Council.

Part B) Approval of the proposed PUD Final Development Plan "Gray's Station Telus Condos," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a "band" of architectural concrete masonry unit materials from grade up to 2' for the standalone garages that matches the concrete masonry units proposed for the condo buildings.
2. Provision of a sidewalk connection from the southeastern corner of "Building 2" that crosses the internal vehicular drive to link up with the north-south pedestrian pathway that bisects this development area.
3. Provision of more-detailed exterior façade materiality information that matches the standards outlined within the PUD Conceptual Plan.
4. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

Part C) Approval of the proposed PUD Final Development Plan "Gray's Station Plat 5 Townhomes and Clubhouse," subject to compliance with all administrative review comments, plus the following conditions:

1. Provision of a sidewalk connection from the clubhouse's outdoor recreation area/dog park that crosses a private drive to link up with a proposed courtyard space in between lots "40" and "41."
2. Verification that any proposed rooftop mechanical equipment will be screened from view from all sides.
3. Provision of additional information regarding mechanical equipment and associated screening (both ground- and roof-mounted) for the clubhouse building.
4. Provision of more-detailed materiality information for the clubhouse building that matches the standards outlined within the PUD Conceptual Plan.
5. Verification that all exterior façade glazing contains a minimum 50% transmittance factor and a reflectance factor of no more than .25.

Motion passed: 8-1-1

Respectfully submitted,



Jason Van Essen, AICP
Planning & Urban Design Administrator

JMV:tjh

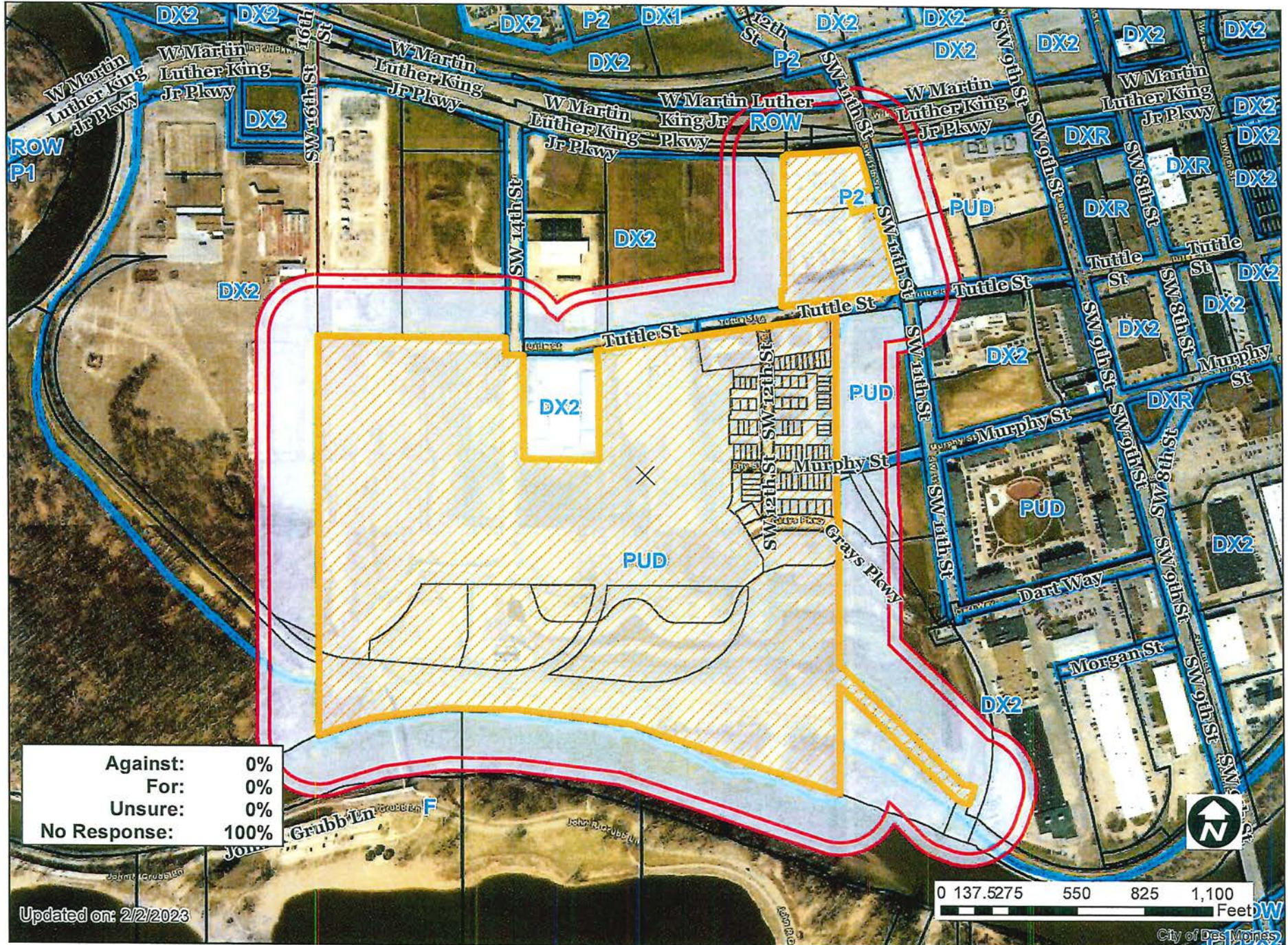


Updated on: 1/12/2023

0 137.5275 550 825 1,100 Feet

City of Des Moines

1 inch = 517 feet



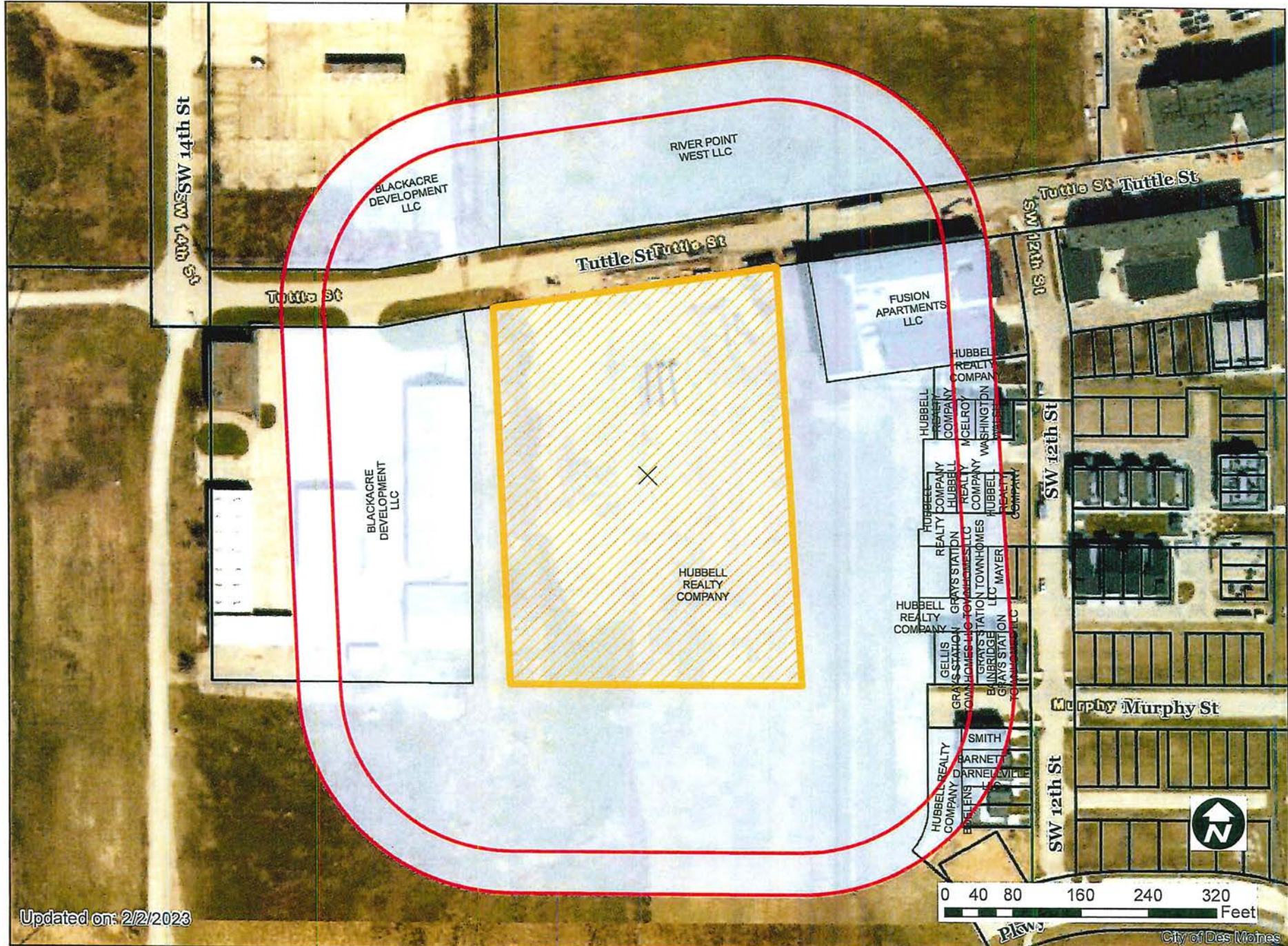
Against:	0%
For:	0%
Unsure:	0%
No Response:	100%

Updated on: 2/2/2023

0 137.5275 550 825 1,100 Feet

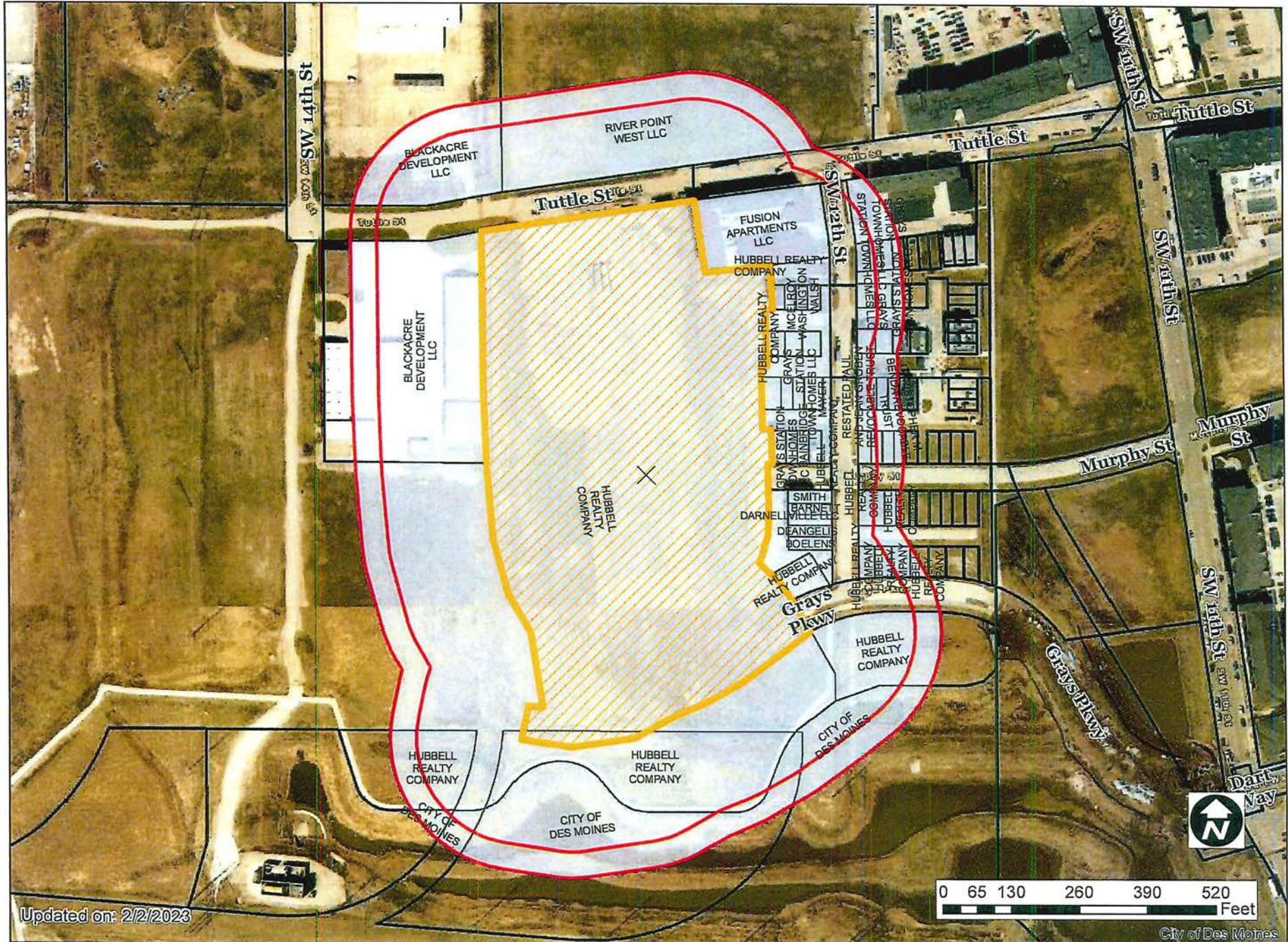
City of Des Moines

1 inch = 517 feet

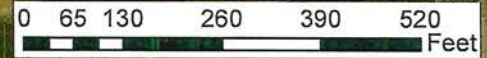


Updated on: 2/2/2023

1 inch = 151 feet

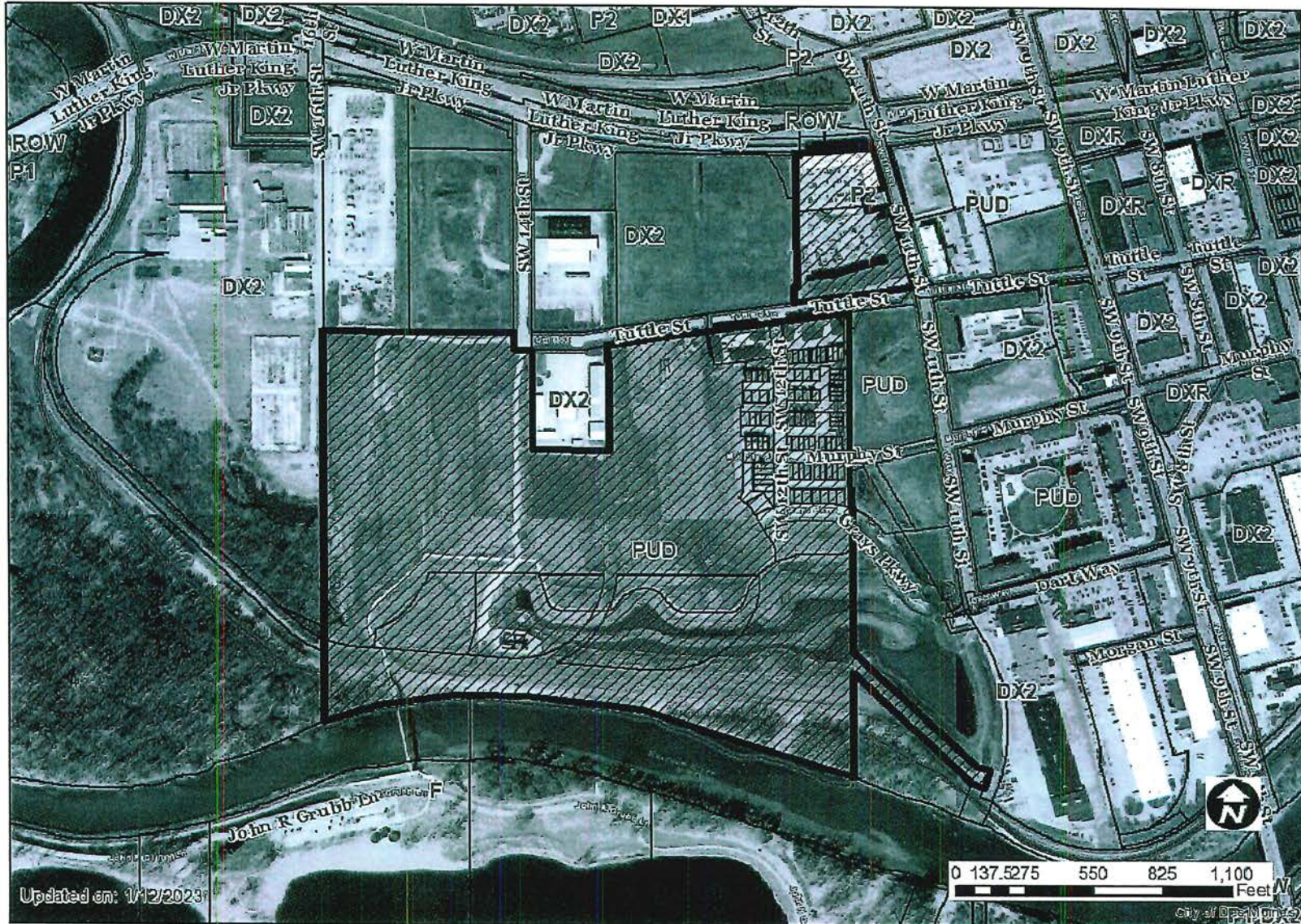


Updated on: 2/2/2023



City of Des Moines

1 inch = 243 feet



Updated on: 1/12/2023

1 inch = 517 feet

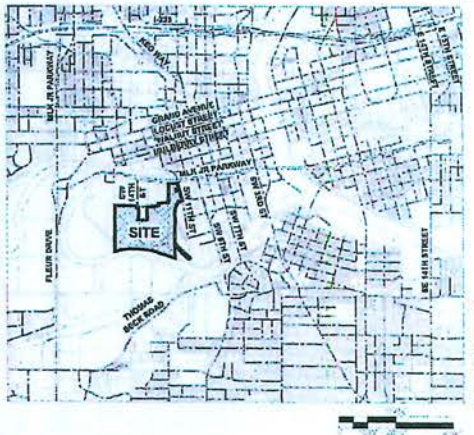
Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa

Sheet 1 of 20

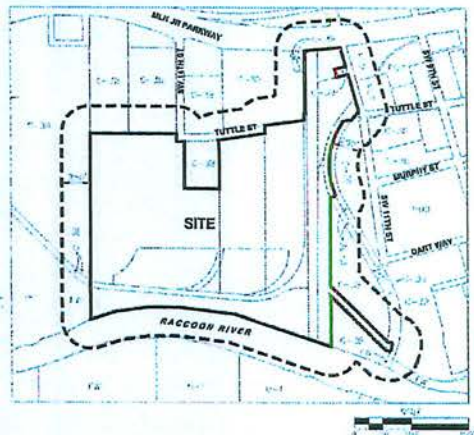


GRAY'S STATION

VICINITY MAP (1"=2000')



ZONING OF ADJACENT PROPERTIES (1"=500')



DEVELOPER

Gray's Station, LLC
 500 West 17th Street, Suite 100
 Des Moines, IA 50319
 Tel: (515) 281-5288
 Fax: (515) 281-5288

OWNER

Gray's Station, LLC
 500 West 17th Street, Suite 100
 Des Moines, IA 50319
 Tel: (515) 281-5288
 Fax: (515) 281-5288

SHEET INDEX

1. Conceptual Plan (this sheet)
 2. Final Plan (see separate sheet)

CITY PRE-APPLICATION MEETING DATE

February 17, 2017

ZONING

City of Des Moines
 Planning & Zoning Commission
 500 West 17th Street, Suite 100
 Des Moines, IA 50319
 Tel: (515) 281-5288
 Fax: (515) 281-5288

PLANDSM LAND USE

See separate sheet

ENGINEER

RDg
 500 West 17th Street, Suite 100
 Des Moines, IA 50319
 Tel: (515) 281-5288
 Fax: (515) 281-5288

NEIGHBORHOOD MEETINGS & MEETING DATE

The Engineer has conducted neighborhood meetings and public hearings regarding this project. The meetings were held on the following dates: [List dates and locations].

NOTES

1. This plan is subject to the final review and approval of the City of Des Moines, Iowa.
2. All adjacent property owners have been notified and their comments have been incorporated into this plan.
3. The engineer has conducted neighborhood meetings and public hearings regarding this project.
4. The engineer has prepared this plan in accordance with the applicable codes and regulations.
5. The engineer has prepared this plan in accordance with the applicable codes and regulations.
6. The engineer has prepared this plan in accordance with the applicable codes and regulations.
7. The engineer has prepared this plan in accordance with the applicable codes and regulations.
8. The engineer has prepared this plan in accordance with the applicable codes and regulations.
9. The engineer has prepared this plan in accordance with the applicable codes and regulations.
10. The engineer has prepared this plan in accordance with the applicable codes and regulations.

P.U.D. PROPERTY DESCRIPTION

Property located in the City of Des Moines, Iowa, bounded on the north by [Street], on the south by [Street], on the east by [Street], and on the west by [Street]. The total area is approximately [Area] square feet.

The property is situated in a residential neighborhood and is proposed to be developed as a planned unit development (PUD). The development will consist of [Number] units, including [Type of units], and will include [Amenities].

The proposed development is consistent with the City of Des Moines Comprehensive Zoning Ordinance, specifically [Section]. The development will be subject to the review and approval of the Planning & Zoning Commission and the City Council.

PUD CONCEPTUAL PLAN APPROVAL

REVIEWED BY THE PLAN & ZONING COMMISSION ON JULY 6, 2017, AND JULY 20, 2017.

REVIEWED BY THE CITY COUNCIL ON JULY 24, 2017 (ROLL CALL NUMBER 17-1289), AND AUGUST 2, 2017 (ROLL CALL NUMBER 17-1307).

APPROVED BY THE CITY COUNCIL ON AUGUST 14, 2017, BY ROLL CALL NUMBER 17-1407. ORDINANCE NO. 15,800.

IN ACCORDANCE WITH SECTION 134-699 OF THE DES MOINES CODE, AS AMENDED.

[Signature]
DATE

[Signature]
PLANNING DIRECTOR

LICENSED PROFESSIONAL

[Signature]
DATE

PLANNING & ZONING COMMISSION

[Signature]
DATE

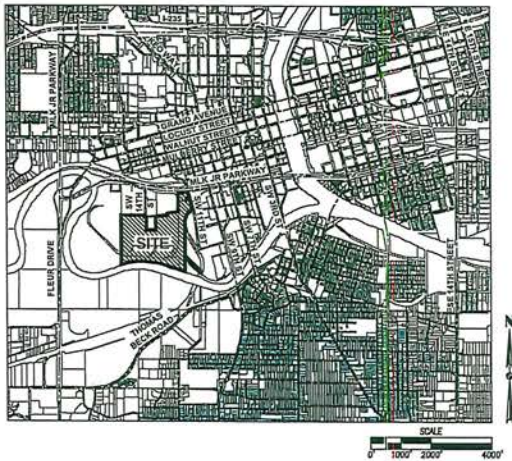
[Signature]
DATE

BY CLERK GENERAL DATE IS REFERRED TO THIS PAGE OF THESE DOCUMENTS BY THIS DATE

CONCEPTUAL PLAN, FIRST AMENDMENT GRAY'S STATION

A PLANNED UNIT DEVELOPMENT IN THE CITY OF DES MOINES, COUNTY OF POLK, STATE OF IOWA

VICINITY MAP (1"=2000')



DEVELOPER

HUBBELL REALTY COMPANY
CONTACT: JOE PIETRUSZYNSKI
3650 WESTON PARKWAY
WEST DES MOINES, IOWA 50266
PH. (515) 243-3228

OWNER

NORFOLK SOUTHERN RY
TAX DEPT
3 COMMERCIAL PLACE, SUITE 29
NORFOLK, VIRGINIA 23510
CITY OF DES MOINES
400 ROBERT D RAY DRIVE
DES MOINES, IOWA 50309

SHEET INDEX

- 1 PLANNED UNIT DEVELOPMENT (COVER SHEET)
- 2 PLANNED UNIT DEVELOPMENT (BASE MAP)

CITY PRE-APPLICATION MEETING DATE

MAY 2, 2017

ZONING

- EXISTING:**
- C-3B CENTRAL BUSINESS MIXED USE DISTRICT
 - FW FLOODWAY DISTRICT
 - DOWNTOWN OVERLAY DISTRICT
 - GAMBLING GAMES PROHIBITION OVERLAY DISTRICT

PROPOSED:

- P.U.D.

PLAN DSM LAND USE

DOWNTOWN MIXED USE

ENGINEER

CIVIL DESIGN ADVANTAGE, LLC
CONTACT: RYAN HARDISTY
4121 NW URBANDALE DRIVE
URBANDALE, IOWA 50222
PH. (515) 369-4400

NEIGHBORHOOD MEETINGS & MEETING DATE

The Gray's Station site is located such that it does not have many citizen neighbors to speak of. Hubbell Realty Company determined that the best way to solicit input related to the potential vision and development of the Gray's Station neighborhood was to invite community leaders to participate in a Steering Committee for the neighborhood. This Steering Committee included members of City Staff, members of the Urban Land Institute Iowa District Council, downtown neighborhood associations, a City Council member, and other individuals whose input would assist Hubbell Realty Company to create a unique new neighborhood for Des Moines. This group met twice in the Summer of 2016 to engage in setting the vision for the neighborhood and to review early concepts for Gray's Station. Their participation has influenced the resulting Conceptual Development Plan for Gray's Station.

Steering Committee Meeting #1 - June 14, 2016

- o Location: RDC Planning & Design 301 and Avenue Des Moines, Iowa 50309
- o Time: 2 PM - 4 PM

Steering Committee Meeting #2 - August 15, 2016

- o Location: RDC Planning & Design 301 and Avenue Des Moines, Iowa 50309
- o Time: 2 PM - 4 PM

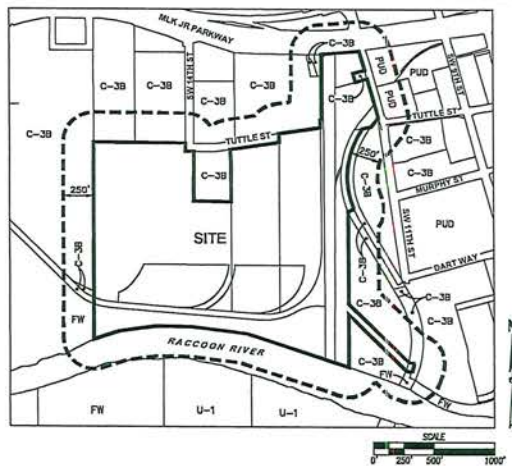
Neighborhood Open House - August 15, 2016

- o Location: Cityville on 8th/ Common Room 550 SW 8th/ ST Des Moines, Iowa 50309
- o Time: 5:30 PM - 8:30 PM

NOTES

1. THIS SITE IS SUBJECT TO THE TREE REMOVAL AND MITIGATION ORDINANCE OF THE CITY CODE. HUBBELL REALTY COMPANY IS ENTERING INTO A DEVELOPMENT AGREEMENT WITH THE CITY OF DES MOINES THAT OUTLINE A CUSTOM APPROACH TO TREE REMOVAL AND MITIGATION FOR THE GRAY'S STATION PLANNED UNIT DEVELOPMENT. NO TREE SURVEY OR CANOPY AREA METHOD INFORMATION WAS SUBMITTED FOR REVIEW. THIS INFORMATION WILL BE PROVIDED WITH THE FINAL DEVELOPMENT PLAN.
2. ALL REQUIRED STORM-WATER MANAGEMENT CAN BE ACHIEVED UNDER THE PROPOSED CONCEPT. (NO DESIGN SUBMITTED)
3. ANY DEVELOPMENT PLAN REVIEW IS SUBJECT TO PROVIDING A TRAFFIC IMPACT ANALYSIS REVIEWED BY THE CITY TRAFFIC ENGINEER.
4. THE SITE SHALL BE PLATTED TO PRESERVE PUBLIC TRANSPORTATION CORRIDORS IN GENERAL CONFORMANCE WITH THE MASTER PLAN.
5. ALL SANITARY SEWER CONNECTIONS ARE AVAILABLE TO SERVICE THE PROPOSED CONCEPT.
6. NO LESS THAN SEVENTY-FIVE PERCENT (75%) OF THE LOW-MEDIUM DENSITY RESIDENTIAL UNITS SHALL HAVE A MINIMUM OF 2 STORIES.
7. NO MORE THAN TWENTY-FIVE PERCENT (25%) OF THE LOW-MEDIUM DENSITY RESIDENTIAL UNITS SHALL HAVE A MINIMUM HEIGHT OF 1 STORY.
8. NO MORE THAN TWO 1-STORY LOW-MEDIUM DENSITY RESIDENTIAL UNITS SHALL BE CONSTRUCTED ON ADJOINING SITES.
9. THE 75%-25% MINIMUM HEIGHT RATIO FOR THE LOW-MEDIUM DENSITY RESIDENTIAL UNITS SHOULD BE ACHIEVED WITH EACH PHASE OF THE PUD PROJECT. SOME VARIATION CAN BE CONSIDERED WITH EACH DEVELOPMENT PLAN.
10. AN EMPHASIS ON TALLER BUILDING HEIGHTS SHALL BE PROVIDED FOR LOW-MEDIUM DENSITY RESIDENTIAL UNITS THAT FRONT PRIMARY STREETS.

ZONING OF ADJACENT PROPERTIES (1"=500')



P.U.D. PROPERTY DESCRIPTION

OUTLOTS Y AND Z AND A PART OF LOTS 6, 7 AND 8, CENTRAL DES MOINES INDUSTRIAL PARK, AN OFFICIAL PLAT AND PART OF LOTS 81, 82, 83 AND 84 IN FACTORY ADDITION, AN OFFICIAL PLAT, ALL BEING IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID LOT 8, CENTRAL DES MOINES INDUSTRIAL PARK HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHERING 570161.83 AND EASTING 160281.57; THENCE SOUTH 81° (DEGREES) 40' (MINUTES) 50" (SECONDS) EAST ALONG THE NORTHERLY LINE OF SAID LOT 9, A DISTANCE OF 338.63 FEET TO THE SOUTHWEST CORNER OF LOT 2, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 69°46'52" EAST CONTINUING ALONG SAID NORTHERLY LINE, 418.55 FEET TO THE SOUTHEAST CORNER OF SAID LOT 2 ALSO BEING A POINT ON THE WEST RIGHT OF WAY LINE OF SW 14TH STREET; THENCE SOUTH 00°01'02" EAST CONTINUING ALONG SAID NORTHERLY LINE, 89.98 FEET TO THE SOUTHWEST CORNER OF STREET LOT 8, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°35'21" EAST CONTINUING ALONG SAID NORTHERLY LINE, 70.00 FEET TO THE NORTHWEST CORNER OF LOT 8, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 02°00'09" WEST CONTINUING ALONG SAID NORTHERLY LINE, 423.70 FEET TO THE SOUTHWEST CORNER OF SAID LOT 8; THENCE SOUTH 89°19'18" EAST CONTINUING ALONG SAID NORTHERLY LINE, 307.20 FEET TO THE SOUTHEAST CORNER OF SAID LOT 8; ALSO BEING THE NORTHEAST CORNER OF SAID LOT 6; THENCE NORTH 02°01'11" WEST ALONG THE WESTERLY LINE OF SAID LOT 7, CENTRAL DES MOINES INDUSTRIAL PARK, 333.38 FEET; THENCE NORTHERLY CONTINUING ALONG SAID WESTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 604.20 FEET, WHOSE ARC LENGTH IS 63.82 FEET AND WHOSE CHORD BEARS NORTH 02°29'07" WEST, 83.77 FEET; THENCE NORTH 02°58'05" WEST CONTINUING ALONG SAID WESTERLY LINE, 15.78 FEET TO THE NORTHWEST CORNER OF SAID LOT 7 ALSO BEING THE SOUTH RIGHT OF WAY LINE OF TUTTLE STREET; THENCE NORTH 81°04'49" EAST ALONG THE NORTH LINE OF SAID LOT 7, A DISTANCE OF 411.78 FEET TO THE NORTHEAST CORNER OF SAID LOT 7; THENCE NORTH 02°05'49" WEST ALONG THE WEST LINE OF SAID LOT 6, CENTRAL DES MOINES INDUSTRIAL PARK, 72.11 FEET TO THE NORTHWEST CORNER OF SAID LOT 6; THENCE NORTH 81°52'47" EAST ALONG THE NORTH LINE OF SAID LOT 6, A DISTANCE OF 347.62 FEET TO THE SOUTHEAST CORNER OF LOT 4, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE NORTH 02°03'45" WEST ALONG THE WESTERLY LINE OF SAID OUTLOT Y, CENTRAL DES MOINES INDUSTRIAL PARK, 63.43 FEET; THENCE NORTH 87°29'10" EAST CONTINUING ALONG SAID WESTERLY LINE, 10.37 FEET; THENCE NORTH 07°24'49" WEST CONTINUING ALONG SAID WESTERLY LINE, 554.69 FEET TO THE NORTHEAST CORNER OF LOT 5, SAID CENTRAL DES MOINES INDUSTRIAL PARK AND HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHERING 375907.16 AND EASTING 1004689.46; THENCE SOUTH 89°04'22" EAST ALONG THE NORTH LINE OF SAID OUTLOT Y, 15.38 FEET TO THE NORTHEAST CORNER OF SAID OUTLOT Y; THENCE NORTH 02°21'12" WEST ALONG THE WEST LINE OF SAID LOT 85, FACTORY ADDITION, 8.69 FEET TO THE SOUTHERLY RIGHT OF WAY LINE OF MARTIN LUTHER KING JR PARKWAY; THENCE NORTH 88°10'40" EAST ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 100.23 FEET; THENCE NORTH 84°40'04" EAST CONTINUING ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 184.44 FEET TO THE WESTERLY RIGHT OF WAY LINE OF SW 11TH STREET; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 173.21 FEET; THENCE SOUTH 74°42'29" WEST, 90.77 FEET; THENCE SOUTH 15°25'34" EAST, 75.29 FEET; THENCE NORTH 74°22'27" EAST, 89.61 FEET TO SAID WESTERLY RIGHT OF WAY LINE; THENCE SOUTH 15°27'22" EAST ALONG SAID WESTERLY RIGHT OF WAY LINE, 344.37 FEET TO A CORNER ON THE EASTERLY LINE OF SAID LOT 81; THENCE SOUTH 39°15'32" WEST ALONG SAID EASTERLY LINE, 97.08 FEET; THENCE SOUTH 79°27'19" WEST, 73.58 FEET TO THE EASTERLY LINE OF THE WEST HALF OF SAID LOT 81; THENCE SOUTHERLY ALONG SAID EASTERLY LINE AND A CURVE CONCAVE EASTERLY WHOSE RADIUS IS 718.25 FEET, WHOSE ARC LENGTH IS 712.02 FEET AND WHOSE CHORD BEARS SOUTH 06°02'51" WEST, 683.06 FEET; THENCE SOUTH 67°48'12" WEST, 51.41 FEET TO A POINT ON THE EAST LINE OF THE WEST 100 FEET OF SAID LOT 80, FACTORY ADDITION; THENCE SOUTH 07°01'40" EAST ALONG SAID EAST LINE, 757.72 FEET; THENCE SOUTH 45°36'19" EAST, 692.68 FEET TO THE WESTERLY LINE OF SAID LOT 81; THENCE SOUTH 76°09'28" EAST, 50.00 FEET TO SAID EASTERLY LINE OF THE WEST HALF OF LOT 81, HAVING A STATE PLANE IOWA SOUTH COORDINATE VALUE OF NORTHERING 573217.87 AND EASTING 1625461.84; THENCE SOUTHERLY ALONG SAID EASTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 740.65 FEET, WHOSE ARC LENGTH IS 78.04 FEET AND WHOSE CHORD BEARS SOUTH 17°00'55" WEST, 78.01 FEET; THENCE NORTH 70°20'25" WEST, 50.00 FEET TO SAID WESTERLY LINE OF LOT 81; THENCE NORTH 45°36'19" WEST, 663.55 FEET TO SAID EAST LINE OF THE WEST 100 FEET OF LOT 80; THENCE SOUTH 07°01'40" EAST ALONG SAID EAST LINE, 439.00 FEET TO THE NORTH BANK AND MEANDER LINE OF THE RACCOON RIVER; THENCE NORTH 71°44'57" WEST AND ALONG SAID MEANDER LINE AND THE SOUTHERLY LINE OF SAID OUTLOT Z, CENTRAL DES MOINES INDUSTRIAL PARK, 888.50 FEET; THENCE NORTH 83°14'04" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 463.82 FEET; THENCE NORTH 81°03'53" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 281.24 FEET; THENCE SOUTH 82°00'06" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 298.82 FEET; THENCE SOUTH 72°02'02" WEST CONTINUING ALONG SAID SOUTHERLY LINE, 228.32 FEET TO THE SOUTHWEST CORNER OF SAID OUTLOT Z; THENCE NORTH 02°03'17" EAST ALONG THE WEST LINE OF SAID CENTRAL DES MOINES INDUSTRIAL PARK, 1947.05 FEET TO THE POINT OF BEGINNING AND CONTAINING 3,247.186 SQUARE FEET) AND IS SUBJECT TO ANY AND ALL EASEMENTS OF RECORD.

PUD CONCEPTUAL PLAN APPROVAL

REVIEWED BY THE PLAN & ZONING COMMISSION ON JULY 6, 2017, AND JULY 20, 2017.

REVIEWED BY THE CITY COUNCIL ON JULY 24, 2017 (ROLL CALL NUMBER 17-1295), AND AUGUST 2, 2017 ROLL CALL NUMBER 17-1307).

APPROVED BY THE CITY COUNCIL ON AUGUST 14, 2017, BY ROLL CALL NUMBER 17-1407, ORDINANCE NO. 15,800.

IN ACCORDANCE WITH SECTION 134-895 OF THE DES MOINES CODE, AS AMENDED.

DATE _____ PLANNING DIRECTOR _____

I HEREBY CERTIFY THAT THIS ENGINEERING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A duly LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF IOWA.

RYAN A. HARDISTY, P.E. DATE _____

20811

BY LICENSE RENEWAL DATE IS DECEMBER 31, 2023 PAGES OR SHEETS COVERED BY THIS SEAL: SHEETS 1 & 6

DATE: _____

REVISIONS: _____

4121 NW URBANDALE DRIVE
URBANDALE, IOWA 50222
PHONE: (515) 369-4400 FAX: (515) 369-4410

TECH: _____

ENGINEER: _____

GRAY'S STATION

PLANNED UNIT DEVELOPMENT (COVER SHEET)

DES MOINES, IOWA

1/2

2202.113

Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa

Sheet 2B of 20



- 16. Sign, temporary advertising means a sign which identifies or directs attention to:
 - a) A publication, business, service, activity, product, campaign or attention manufacturer, sold, offered or conducted upon the premises where such sign is located, and/or
 - b) The ideological or noncommercial views of an occupant of the premises.
 - 17. Sign, not message a sign located upon or above a roof or parapet of a building.
 - 18. Sign, sandwich board means a sign with two faces that are adjacent to the top and deployed at an angle, which is not permanently anchored or secured.
 - 19. Sign, service means a sign identifying restaurants and other service facilities.
 - 20. Sign, temporary means a sign intended for a limited period of display.
 - 21. Sign, type A means a sign which conforms to all aspects with the following:
 - a) It shall be either illuminated or internally lighted with nonhazardous white light.
 - b) It shall have not more than two sign faces, such sign faces to be parallel.
 - 22. Sign, type B means a sign which shall conform to all of the requirements for type A sign, except that type B signs may be internally illuminated.
 - 23. Sign, type C includes type A and type B signs and shall conform to all aspects with the following:
 - a) It may contain exposed lamps or lead pipe tubes or any combination thereof, provided that the exposed lamp does not exceed 11 watts or the lead pipe tube does not exceed 10 milliwatts. Where inside heated lamps or exposed lamps with a diffusing screen are used, no lamp shall exceed 25 watts.
 - b) Identification of illumination by reflected light or by colored reflection is prohibited.
 - 24. Sign, window means a sign within a building which is located and placed so as to enhance an intent for the sign to be viewed from a public right-of-way.
- B. Directional Sign and High Density Residential Sign**
1. Sign shall be in accordance with the following:
 - a) Any combination of the sign types type A, type B or type C identification under advertising signs per occupant of a building not to exceed one and one-half square feet in area per linear foot of building footage per occupant, and not to exceed a maximum of 100 square feet per occupant, shall be permitted. Existing signage consisting of 25 percent or less of the total building footage, the amount or amount of that building shall be permitted, in lieu of the above, one square foot in area per linear foot of building per lot and not to exceed a maximum of 100 square feet per occupant. Such signs shall be permitted for public or private occupancy attached to and supported by a building.
 - b) All building and sign signs shall project no more than seven feet from the building, unless such projection is over the street right-of-way, in which case such sign shall project no more than 30 inches. Such projecting sign shall be not less than ten feet from the ground in areas in which there is only pedestrian traffic and not less than 15 feet from the ground in areas in which there is vehicular traffic. Signs shall not be more than 20 square feet in area per occupant and shall not extend more than four feet above the mainline. Projecting signs may be anchored to the roof for building.
 - c) The permitted types of free-standing signs are non-illuminated and sandwich board signs. Pole signs and portable signs other than sandwich board signs are not permitted.
 - d) One sandwich board sign shall be permitted per building front for identification and non-political advertising. Such signs shall comply with ingress and egress requirements outlined in the city building code. Sandwich board signs are permitted upon the sidewalk within the public right-of-way, subject to the following:
 - (1) Such signs are permitted only during the hours the business is open to the public, and must be located adjacent to the premises.
 - (2) Such signs shall be placed no closer than two feet from the back of the street curb.
 - (3) Such signs shall be no more than ten square feet in area on each side, no more than two feet in height, and no more than three feet in width.
 - (4) There shall be no electrical signs to such signs.
 - e) One non-illuminated building or wall identification sign per occupant, not to exceed two square feet in area, shall be permitted on building where occupants have no storefront facade, and shall be located immediately adjacent to the principal entrance to said occupant's premises.
 - f) Two non-political type B identification sandwich signs shall be permitted on facade marquee. The total sign area per occupant shall not exceed 100 square feet.

- c) Off-premise advertising signs are not permitted.
 - Median Density Residential Sign
 1. Sign shall be in accordance with the following:
 - a) One sign per type A, type B or type C identification sign not to exceed one square foot in area shall be permitted for each of the following uses: Single-family detached dwellings, unimproved group residences, and family homes.
 - b) One sign per type A identification sign not to exceed 24 square feet in area per sign face shall be permitted for each of the following uses: Churches, cathedrals and temples, and similar places of worship; public and private schools (including boarding schools, nursery schools, and child care centers); day-care facilities; and publicly owned parks, playgrounds, golf courses, and recreation areas.
 - c) One non-illuminated building, internally or externally illuminated, not to exceed 24 square feet in area, shall be permitted per building. The sign shall extend higher than eight feet above ground level.
 - d) One sign per type B identification sign not to exceed one square foot in area shall be permitted for each of the following uses:
 - (1) One sign per type B identification sign not to exceed one square foot in area shall be permitted for each of the following uses: Churches, cathedrals and temples, and similar places of worship; public and private schools (including boarding schools, nursery schools, and child care centers); day-care facilities; and publicly owned parks, playgrounds, golf courses, and recreation areas.
 - (2) In addition to the signage permitted by subsection a) through d) of this section, a home occupation may be permitted on additional square feet of signage.
 - e) In lieu of the identification sign and the non-illuminated building, a church or school may combine into one sign type B or type B identification sign and an illuminated bulletin board. The sign may be internally or externally illuminated. It shall not exceed 75 square feet per sign face nor extend more than eight feet above the ground, and shall not be located in any required front or side yard.
- VI. Parking Requirements**
- A. Residential Areas**
1. Surface parking spaces shall not exceed 3.75 spaces per 1000 GSF of commercial area.
 2. Surface parking shall have an emphasis of trees, shrubs and other plantings placed around the perimeter of any parking area and where large parking lots to create a more attractive area. The landscape plan should generally enhance the visual appearance of the building, parking area and any pedestrian areas. Design of surface parking landscape enhancements to be reviewed by the Planning & Zoning Commission under City Council.
 3. Vehicular use areas with 10 or more car stalls require interior landscaping as detailed below:
 - a) Interior landscaping shall of a minimum consist of one planted tree not smaller than 8" x 17" for every 25 motor spaces containing 1 motor space and 3 shrubs.
 - b) Required landscaping and planting may be clustered but there may be no more than 25 spaces in a single row, or 25 spaces in a double row without an intervening planting bed.
 - c) Trees planted in Parking lot Interior Planting area to be a minimum of 1" caliper.
 - d) Use of evergreen trees is required to increase shade and reduce "urban heat island" effect.
 - e) A minimum of one (1) evergreen tree and three (3) shrubs shall be required for every twenty-five (25) spaces exclusive of tree planting required in perimeter landscaping areas.
 - f) Corner islands beyond the 7' perimeter landscaping strip may be covered toward interior planting requirements when they are contiguous with perimeter landscaping areas.
 - g) The 7' perimeter landscaping area may not be covered toward interior planting requirements.
 - h) Perimeter landscaping areas in excess of 7' may partially be covered toward interior planting requirements.
 - i) Interior planting islands that are at least 10' x 17' should be spaced no farther than 20 parking spaces or 100' apart.
 4. Vehicular use areas require perimeter landscaping area as detailed below:
 - a) Vehicular use areas including up to 30 cars require a 7' perimeter landscaping area (7' car stacking, 7' landscape area) measured from the right-of-way line along parking lots having public right-of-ways. (Public land cannot be counted toward minimum of this subsection).

- b) Vehicular use areas of 40 car stalls and over require a 7' perimeter landscaping area (7' car stacking, 7' landscape area) measured from the right-of-way line along parking lots having public right-of-ways. (Public land cannot be counted toward minimum of this subsection).
 - c) Barrier walls of concrete or stone shall be a minimum of 2' in height and a minimum of 4' in width, and shall be securely installed around the perimeter landscaping area and be maintained in good condition.
 - d) Set shall be placed the entire width of perimeter landscaping area, including the car stacking area.
 - e) The landscaping area shall contain 1 tree and 10 shrubs (2" caliper) maximum unless restricted height, and be no less than 18" tall per 40 linear feet of perimeter landscaping area with groundcover or a decorative wall or fence made of stone, brick, masonry concrete block, perforated cast-in-place concrete panels, wrought iron, or a combination of these materials, located on the lot line. Wood is not allowed except for an accent material. Chain link fences are not allowed. Walls shall be no taller than 30", and no steeper than 3:1.
 - f) Groundcover or low planting is required in the car stacking area where maintenance will be provided. Otherwise grass, or hardworn gravel mulch, is required.
 - g) Iron bars are recommended every 10' throughout the perimeter landscaping area.
 - h) Trees shall be planted a minimum of 2' from curb.
 - i) Unequal tree spacing is acceptable with groupings of lighter spacing and larger spacings between groupings.
 - j) The siting or siting of perimeter area lines with parking lines is encouraged.
 - k) Trees planted in the perimeter landscaping area are a separate requirement, and do not count toward other tree planting in the parking lot interior plantings.
- 5. Parking lots adjacent to public streets shall be screened by walls, fences, hedges, landscaping or a combination thereof. Unscreened sections shall be landscaped to meet the Minimum Landscaping Standards for parking facilities, C.A., I.C.A.**
- B. Residential Areas**
- 1. Streets designated as "Local" or "Collector" shall include parallel parking as depicted in the approved conceptual development plan.**
- C. Structure Parking**
1. Facades of above grade structured parking lots, podium parking beneath commercial or residential uses adjacent to any public right-of-way shall be architecturally and aesthetically consistent with the remainder of the building they support.
- VI. Mass Transit Integration**
- A. Development shall accommodate long term transit connections within and between neighborhoods.
 - B. The Des Moines Area Regional Transit (DART) Authority shall review all development plans and design decisions for transit stops and shelters.
 - C. Developers and DART shall work together to establish feasible transportation linkages on a case by case basis.
- VI. Public Amenities**
- A. Bicycle and Pedestrian Facilities:**
1. Development shall provide paved-on-bike lanes and off-street trails that connect to existing urban system.
 2. Development will include a Pedestrian Bridge connection to Gray's Lake over the Racoon River.
 3. Pedestrian bridge is anticipated to be located near the southeast corner of the divided PUD boundary and west of the existing pump station associated with the enhanced stormwater basin.
 4. Development shall provide the inclusion of bicycle sharing services (e.g., B-Cycle).
 5. Sidewalk along streets, in common open space areas, and between buildings shall maintain a minimum width of 5'.
- B. Public Open Space**
1. Openways
 - a) Openways shall maintain an approximate width of 40' to 50'. Exceptions may occur where infrastructure requirements necessitate and will be reviewed with the Final Development Plan.
 - b) Openways shall include landscaping.
 - c) Openways shall include pedestrian pathways with a minimum width of 5'.
 - d) Openways may include elevated (e.g., Pergola, Shade Structure, etc.) and/or artistic elements where deemed appropriate by the developer and approved in the Final Development Plan.
 2. Neighborhood Plaza
 - a) Neighborhood Plaza shall be generally located per the approved conceptual development plan.
 - b) Neighborhood Plaza shall include landscaping.
 - c) Neighborhood Plaza shall include pedestrian pathways with a minimum width of 5'.
 - d) Neighborhood Plaza may include elevated (e.g., Shade, Chaise, Pergola, etc.) and/or artistic elements where deemed appropriate by the developer.
 3. Pedestrian Bridge Landscaping Plaza
 - a) The Pedestrian Bridge Landscaping Plaza shall generally be located as depicted in the approved conceptual development plan.
 - b) The Plaza shall be designed to accommodate pedestrian and bicycle traffic associated with daily use of the Pedestrian Bridge walkway.
 - c) The Plaza shall be designed as an inviting public space that is flexible enough to accommodate neighborhood events.
 4. Enhanced Stormwater Basin
 - a) The Enhanced Stormwater Basin (ESB) shall generally be located as depicted in the approved conceptual development plan on Street 55.
 - b) The ESB shall be designed to detain stormwater from the watershed which it serves.
 - c) The ESB shall be designed to improve the quality of stormwater that flows through it.
 - d) The ESB shall be designed to be aesthetically pleasing.
 - e) The ESB shall be designed by the developer. The ESB design shall be approved by the City of Des Moines. ESB shall be constructed and owned by the City of Des Moines.



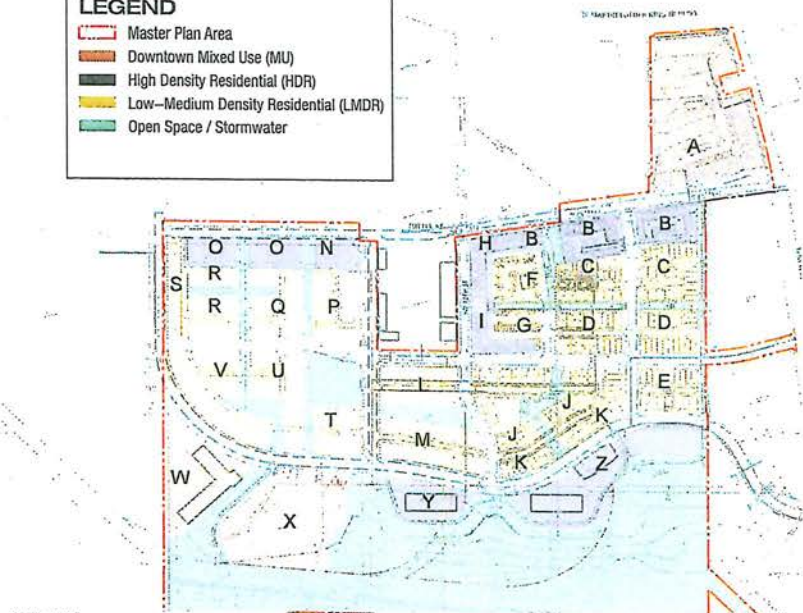
Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa

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LEGEND

- Master Plan Area
- Downtown Mixed Use (MU)
- High Density Residential (HDR)
- Low-Medium Density Residential (LMDR)
- Open Space / Stormwater



Principal Uses	Plan Subarea																							
	MU	HDR	MDR	MDR	MDR	MDR	MDR	HDR	MDR	MDR	MDR	MDR	HDR	HDR	MDR	MDR	MDR	MDR	MDR	MDR	MU	MU	HDR	HDR
Residential Use Category																								
1-unit dwelling		X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
2-unit dwelling			X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
3-unit dwelling			X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Multi-unit dwelling (4+) units	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Efficiency/Micro Units	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Bed & Breakfast	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Group Living	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Group Living for Health Reasons	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Hotel/Apartment Hotel	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Residential Care, Large	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Residential Care, Small	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Civic & Institutional Category																								
Assembly, Neighborhood (<10,000 sf)	X	X						X														X	X	X
Assembly, General (>10,000 sf)	X	X						X														X	X	X
Government/Higher Education/Hospital	X	X						X														X	X	X
Library/Museum	X	X						X														X	X	X
Police/Fire	X	X						X														X	X	X
School: Pre-K, elementary, Intermediate	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Transit Station	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Open Space Category																								
Community Garden	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Intensive Park Uses	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Park	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Urban Farm	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Retail Use Category																								
Neighborhood Retail	X	X						X														X	X	X
General Retail	X	X						X														X	X	X
Beer/Wine/Liquor Sales	X	X						X														X	X	X
Convenience Store	X	X						X														X	X	X
Service Use Category																								
Neighborhood Service	X	X						X														X	X	X
General Service	X	X						X														X	X	X
Adult Day Care	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Child Day Care (>12 children)	X	X						X														X	X	X
Community Service	X	X						X														X	X	X
Drinking Places	X	X						X														X	X	X
Eating Places	X	X						X														X	X	X
Entertainment Assembly	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Private Club	X	X						X														X	X	X
Tattoo/Piercing Parlor	X	X						X														X	X	X
Employment Use Category																								
Office	X	X						X														X	X	X
Craftsman/Studio	X	X						X														X	X	X

X Permitted
All uses not listed on this sheet are prohibited.

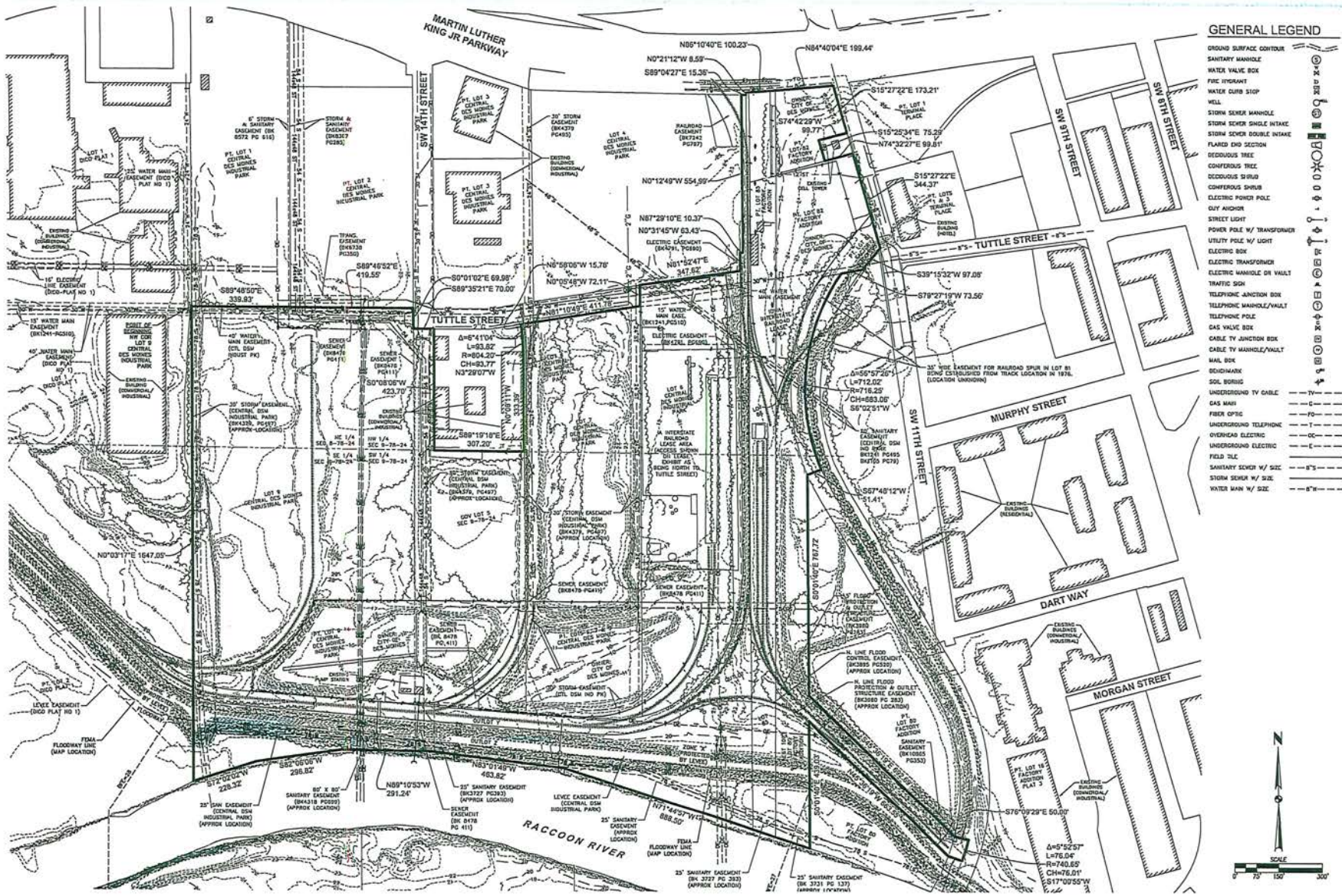
Accessory & Temporary Uses

Category	Plan Subarea																							
	MU	HDR	MDR	MDR	MDR	MDR	MDR	HDR	HDR	MDR	MDR	MDR	MDR	MDR	MDR	MU	MU	HDR	HDR					
Residential Accessory Use																								
Accessory Dwelling Unit			X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Home Child Care			X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Home Occupation	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Room Rental - Long-Term (> 3 months)	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Room Rental - Short-Term (< 21 days)	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Commercial Accessory Use																								
Drive-Through Facility	X	X																						
Outdoor Café	X	X					X						X	X								X	X	X
Outdoor Display	X	X											X	X								X	X	X
Parking	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Temporary Use Category																								
Volting Place	X	X																				X	X	X
Temporary Outdoor Event	X	X																				X	X	X
Farmers Market	X	X																				X	X	X

X Permitted
All uses not listed on this sheet are prohibited.

PERMITTED LAND USES
OCTOBER 19, 2022

Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa
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GENERAL LEGEND

- GROUND SURFACE CONTOUR
- SANITARY MANHOLE
- WATER VALVE BOX
- FIRE HYDRANT
- WATER CURB STOP
- WELL
- STORM SEWER MANHOLE
- STORM SEWER SINGLE INTAKE
- STORM SEWER DOUBLE INTAKE
- FLARED END SECTION
- DECIDUOUS TREE
- CONIFEROUS TREE
- DECIDUOUS SHRUB
- CONIFEROUS SHRUB
- ELECTRIC POWER POLE
- DAY ANCHOR
- STREET LIGHT
- POWER POLE W/ TRANSFORMER
- UTILITY POLE W/ LIGHT
- ELECTRIC BOX
- ELECTRIC TRANSFORMER
- ELECTRIC MANHOLE ON VAULT
- TRAFFIC SIGN
- TELEPHONE JUNCTION BOX
- TELEPHONE MANHOLE/VAULT
- TELEPHONE POLE
- GAS VALVE BOX
- CABLE TV JUNCTION BOX
- CABLE TV MANHOLE/VAULT
- MAIL BOX
- BOUNDARY
- SOIL BOUNDARY
- UNDERGROUND TV CABLE
- GAS MAIN
- FIBER OPTIC
- UNDERGROUND TELEPHONE
- OVERHEAD ELECTRIC
- UNDERGROUND ELECTRIC
- FIELD TILE
- SANITARY SEWER W/ SIZE
- STORM SEWER W/ SIZE
- WATER MAIN W/ SIZE

PLAN MAP:
 EXISTING CONDITIONS
 MAY 3, 2017

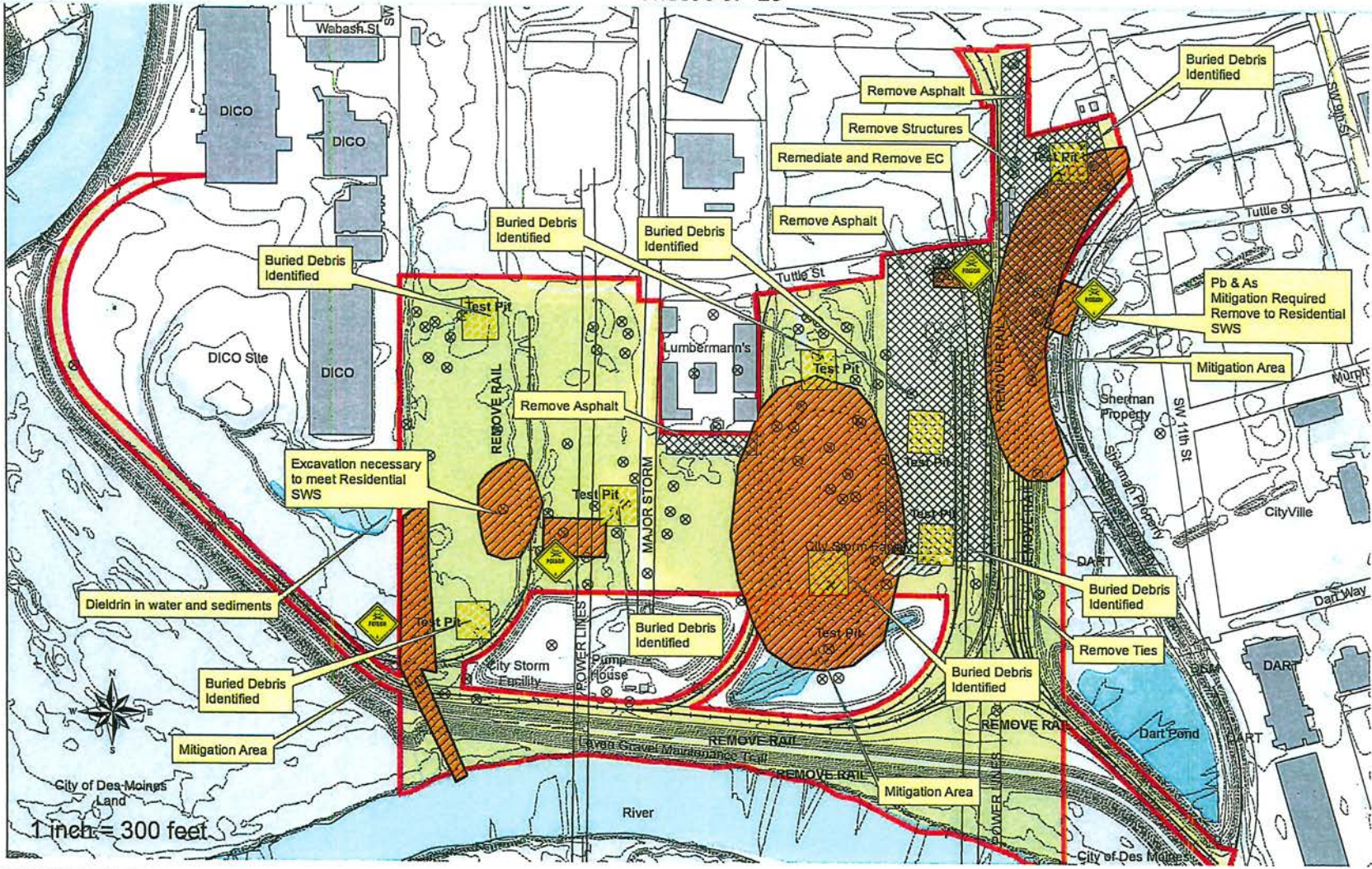
CIVIL DESIGN ADVANTAGE

 HUBBELL
 HUBBELL REALTY COMPANY

 RDG
 PLANNING & DESIGN



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa
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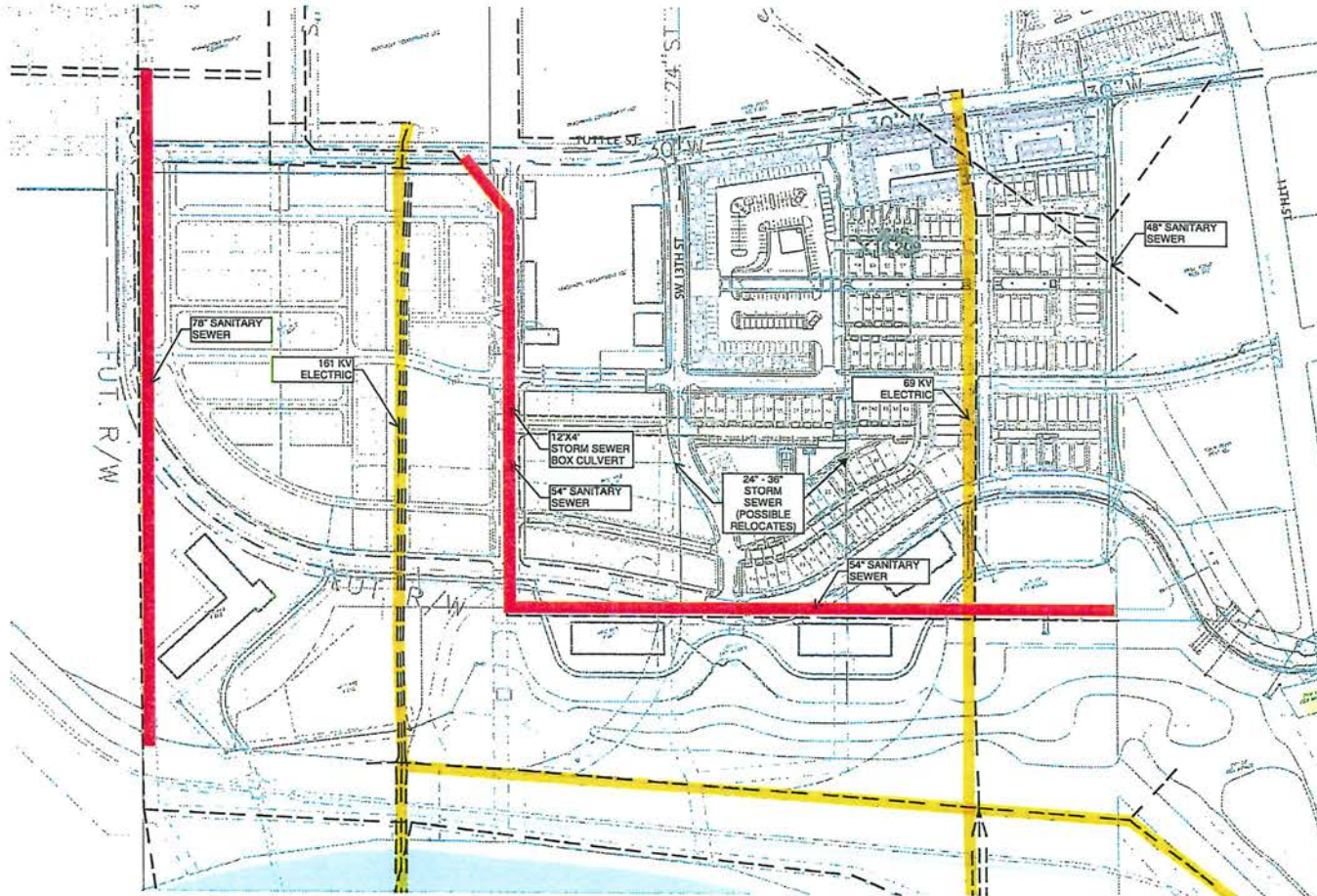


NOTES:
 1. Refer to supplemental digital content provided to the City of Des Moines for additional environmental conditions findings.

ENVIRONMENTAL CONDITIONS
 OCTOBER 19, 2022



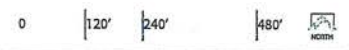
Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa
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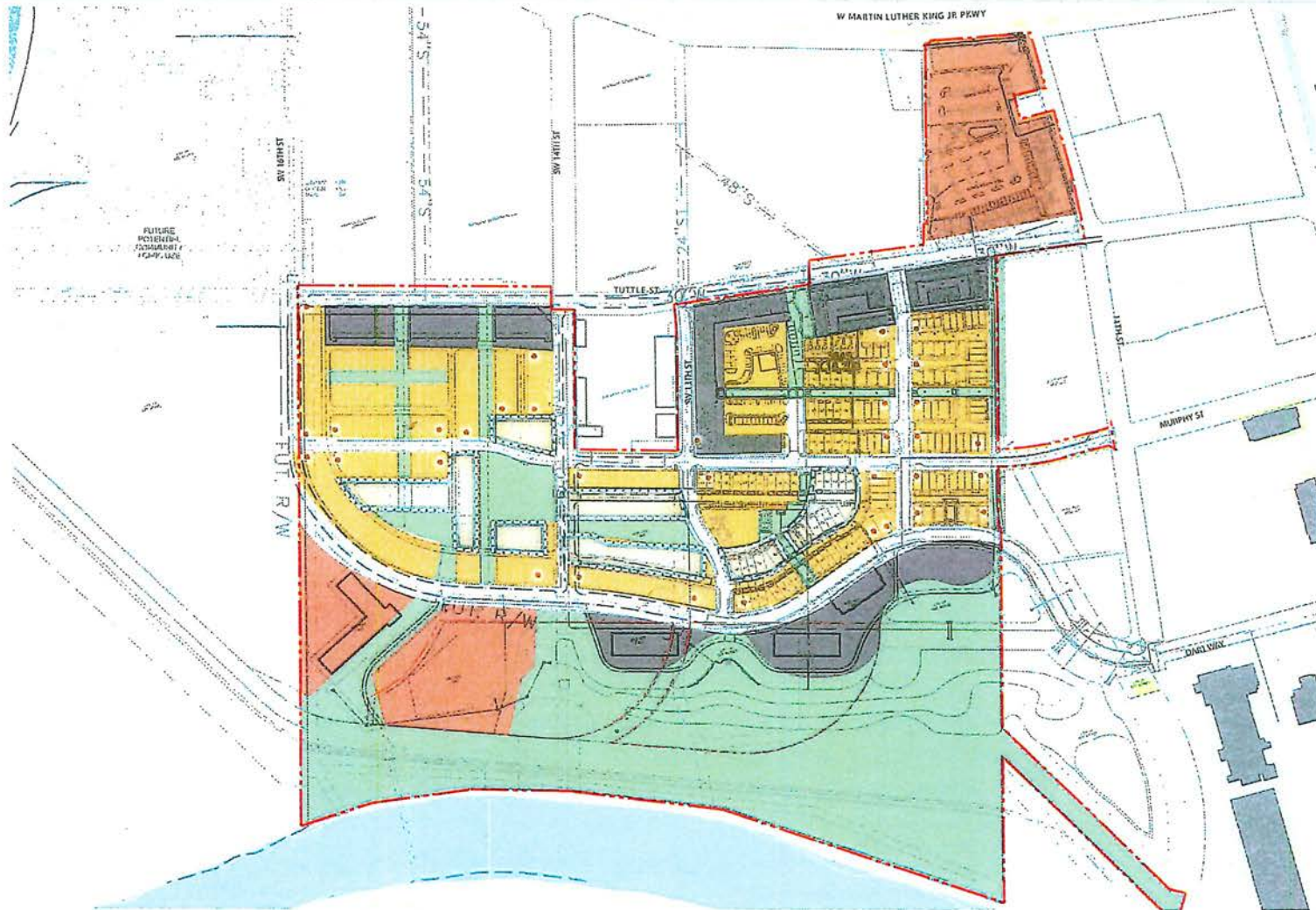
LEGEND:
— EXISTING SANITARY LINE TO REMAIN
- - - EXISTING OVERHEAD POWER LINES TO REMAIN

NOTES:
 1. The conceptual plan takes into account significant existing infrastructure features that are currently infeasible to move based on economic factors. Unless economic conditions change, future development plans should respect and sensitively integrate the existing infrastructure highlighted in the adjacent diagram and attached legend.

EXISTING INFRASTRUCTURE CONSTRAINTS
 OCTOBER 19, 2022



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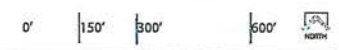


NOTES:
 1. The master plan at left is for illustrative purposes only and is provided to show general, conceptual intent and character of the development. Individual parcels, building footprints, landscape design, open space design and parking design will be determined and designed as a part of future development plans for each phase.

LEGEND

- Master Plan Area
- Downtown Mixed Use (MU)
- High Density Residential (HDR)
- Low-Medium Density Residential (LMDR)
- Open Space / Stormwater
- 2 story minimum building height at corner lots where indicated with orange circle
- Detached Urban Townhomes Subarea (Other uses permitted as per the Permitted Land Uses Chart)

CONCEPTUAL ILLUSTRATIVE MASTER PLAN
 OCTOBER 19, 2022



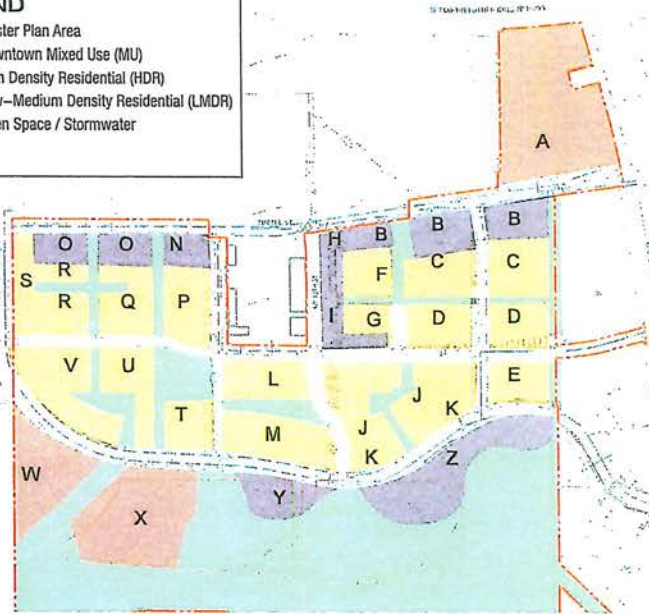
Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa
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GRAY'S STATION

LEGEND

- Master Plan Area
- Downtown Mixed Use (MU)
- High Density Residential (HDR)
- Low-Medium Density Residential (LMDR)
- Open Space / Stormwater



Hubbell Gray's Station
 Master Plan Concept
 Conceptual Land Use Summary & Estimated Residential Units

AREA NAME	LAND USE	ACRES	MINIMUM DENSITY (DU/AC)	MAXIMUM DENSITY (DU/AC)
A	Mixed Use	5.3	30	50
B	HDR	1.9	30	50
C	MDR	2.0	8	35
D	MDR	1.9	8	35
E	MDR	1.0	8	35
F	MDR	0.7	8	35
G	MDR	0.7	8	35
H	HDR	0.1	18	30
I	MDR	0.8	8	35
J	MDR	1.2	7	22
K	MDR	1.7	7	22
L	MDR	1.6	8	35
M	MDR	1.6	8	35
N	HDR	0.4	18	30
O	HDR	1.2	40	55
P	MDR	1.0	8	35
Q	MDR	1.2	8	35
R	MDR	1.1	8	35
S	MDR	0.7	8	35
T	MDR	0.7	8	35
U	MDR	1.1	8	35
V	MDR	2.0	8	35
W	Mixed Use	2.0	30	50
X	Mixed Use	3.5	30	50
Y	HDR	1.0	50	70
Z	HDR	2.7	50	70
TOTAL		39.2		

Illustrative Estimate

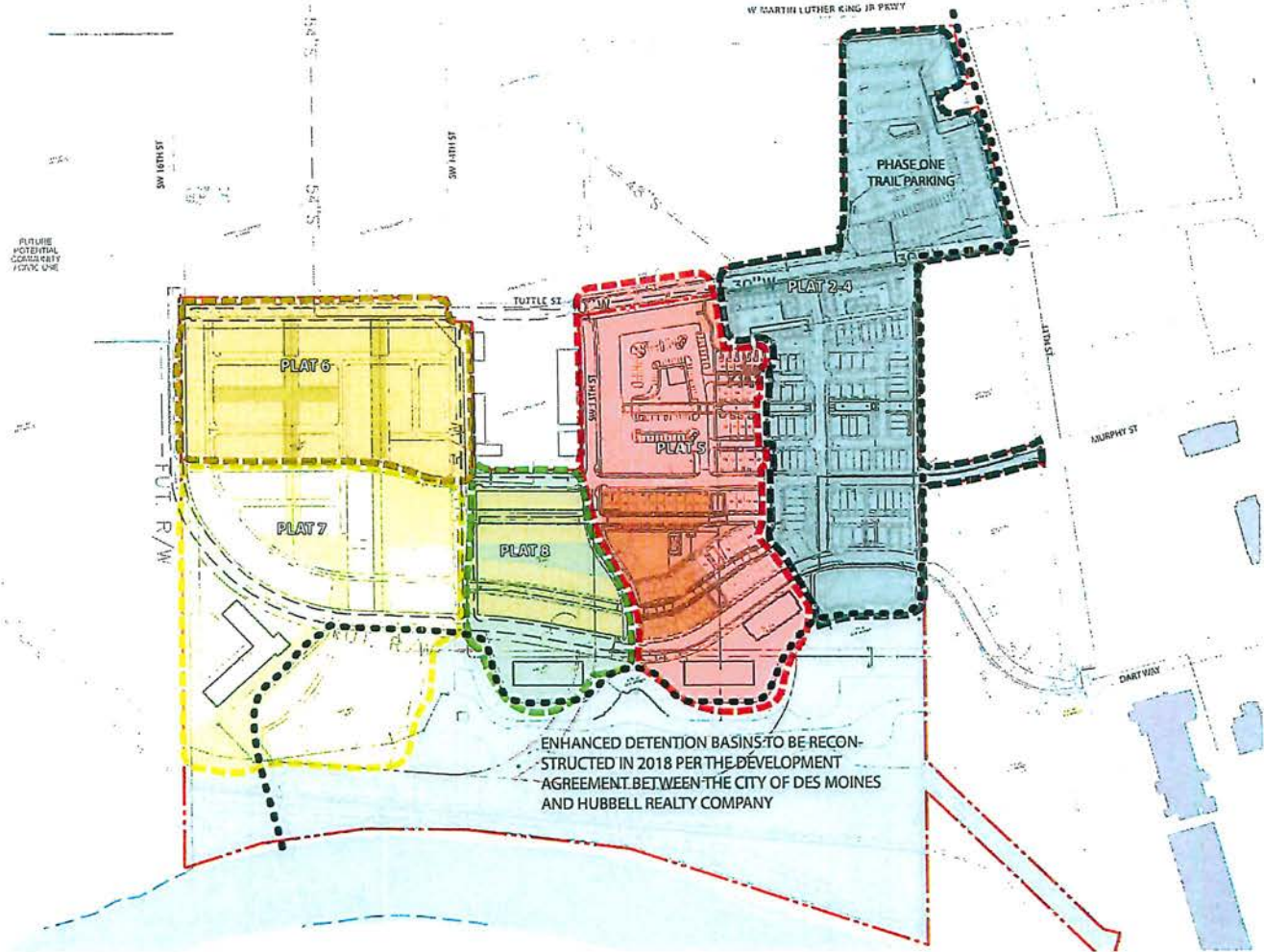
Illustrated Units	Illustrated Net Density	Assumed Multi-family residential floors	City Classification
193	37	4	High
79	41	3	High
34	17		High
34	18		High
16	16		Med
10	14		Med
11	16		Med
5	35	3	High
12	16		Med
13	11		Low-Med
22	13		Med
32	19		High
32	20		High
15	33	3	High
65	54	3	High
22	22		High
22	18		High
24	21		High
16	22		High
10	14		Med
18	16		Med
38	19		High
85	42	4	High
142	41	4	High
55	54	5	High
148	55	5	High
TOTAL	1151	29.3	

Gray's Station will achieve a minimum of 1,100 dwelling units.

MINIMUM NET DENSITY (EXCLUDING SIGNIFICANT OPEN SPACE, ROADS) **28**

Notes: 1. The assumed number of multi-family units is based on an average 1000 sf dwelling unit.

Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa
 Sheet 10 of 20



ENHANCED DETENTION BASINS TO BE RECONSTRUCTED IN 2018 PER THE DEVELOPMENT AGREEMENT BETWEEN THE CITY OF DES MOINES AND HUBBELL REALTY COMPANY

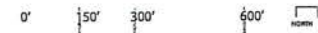
NOTES:
 Variables that will impact the anticipated phasing of the Gray's Station plan include the timing of:
 1) Redevelopment of the DICO site, and
 2) Infrastructure, improvements and street extensions completed by the City of Des Moines.

LEGEND

- Master Plan Area
- Phase One Trail - Conceptual Alignment

Anticipated Phasing:

- Plat 2-4
- Plat 5
- Plat 8
- Plat 6
- Plat 7

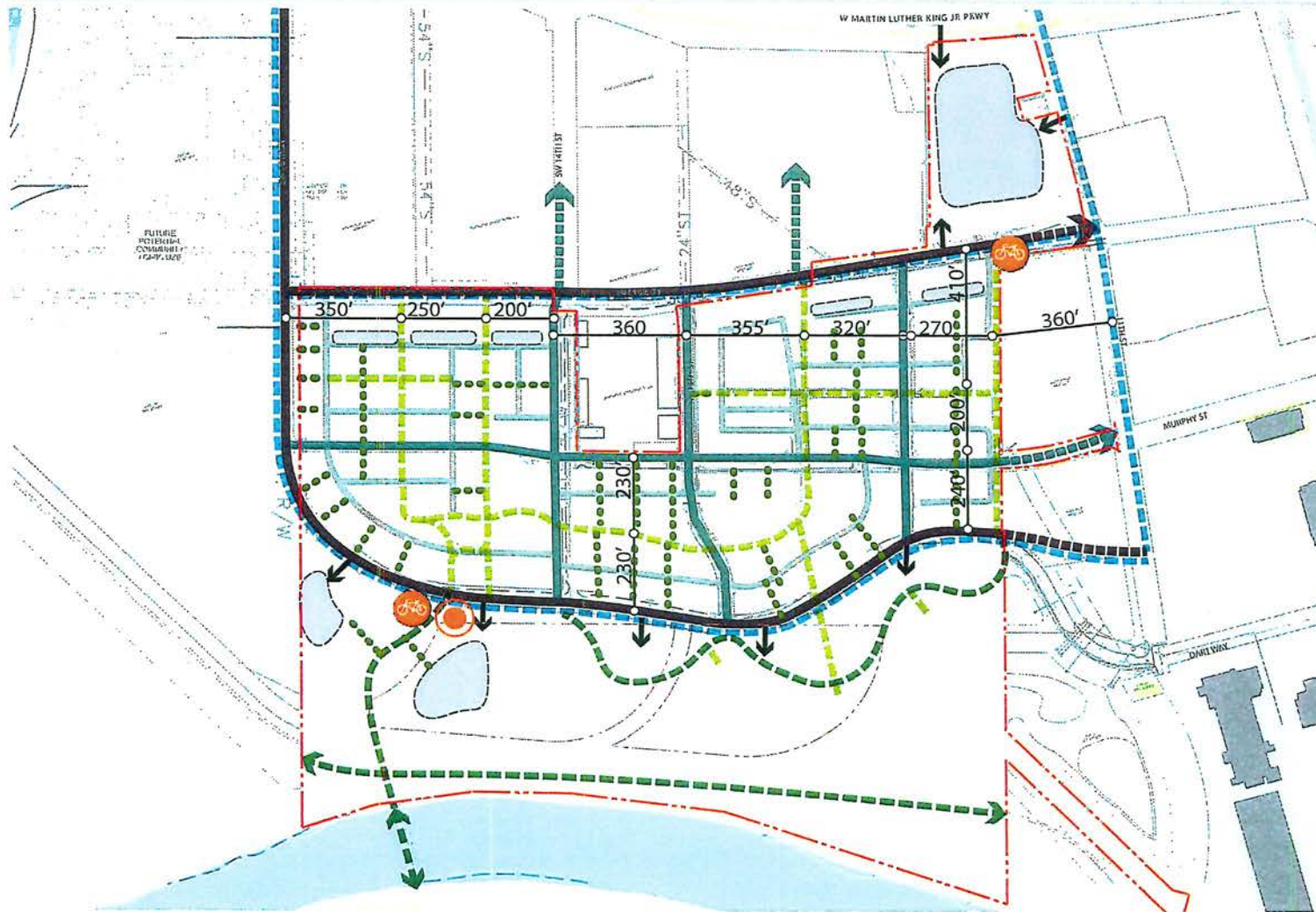


CONCEPTUAL PHASING DIAGRAM
 OCTOBER 19, 2022

Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa
 Sheet 11 of 20



GRAY'S STATION



NOTES:

- The location of the following items are approximate and provided to show general plan intent of the circulation network:
 - Block lengths between streets and greenways;
 - Greenway locations;
 - Road centerlines;
 - Alley network;
 - Access drive location;
 - Bus stop locations;
 - Bike share locations;
 - Off-street parking areas.
- The proposed circulation network outside of the master plan area is shown for illustrative purposes and to demonstrate the development's intent to tie into future improvements outside the scope of this master plan.
- The Conceptual Plan was completed while the City's Transportation Master Plan (MOVE DSM) was under development. The intent of this Conceptual Plan is to comply with the MOVE DSM plan based on the preliminary goals that have been publicly presented such as:
 - Support other City goals of safety, quality of life, and economic development.
 - Ensure that Des Moines is best positioned to provide transportation options for all citizens in every stage of life.

LEGEND

- Master Plan Area
- Proposed Collector Street
Anticipated 2 lanes travel, 2 lanes parking, bicycle facilities
- Proposed Local Street
Anticipated 2 lanes travel, 1 lane parking
- Proposed Alley
- Proposed Bicycle/Pedestrian Route – Primary
- Proposed Greenways
- Proposed On-Street Bike Facility
- Existing Regional Trail
- Existing Local Trail
- Proposed Access Drive
- Proposed Off-street Surface Parking Area
- Proposed Bus Stop
- Proposed Bike Share

CONCEPTUAL CIRCULATION DIAGRAM
 OCTOBER 19, 2022



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa
 Sheet 12 of 20



GRAY'S STATION

CONCEPTUAL STREET SECTION: SOUTHERN ROAD/16TH STREET (COLLECTOR STREET)



CONCEPTUAL STREET SECTIONS: TUTTLE STREET - CYCLE TRACK (COLLECTOR STREET)



CONCEPTUAL ALLEY SECTION



CONCEPTUAL STREET SECTION: LOCAL STREET AT 14TH STREET



CONCEPTUAL STREET SECTION: LOCAL STREETS (EXCLUDING 14TH STREET)

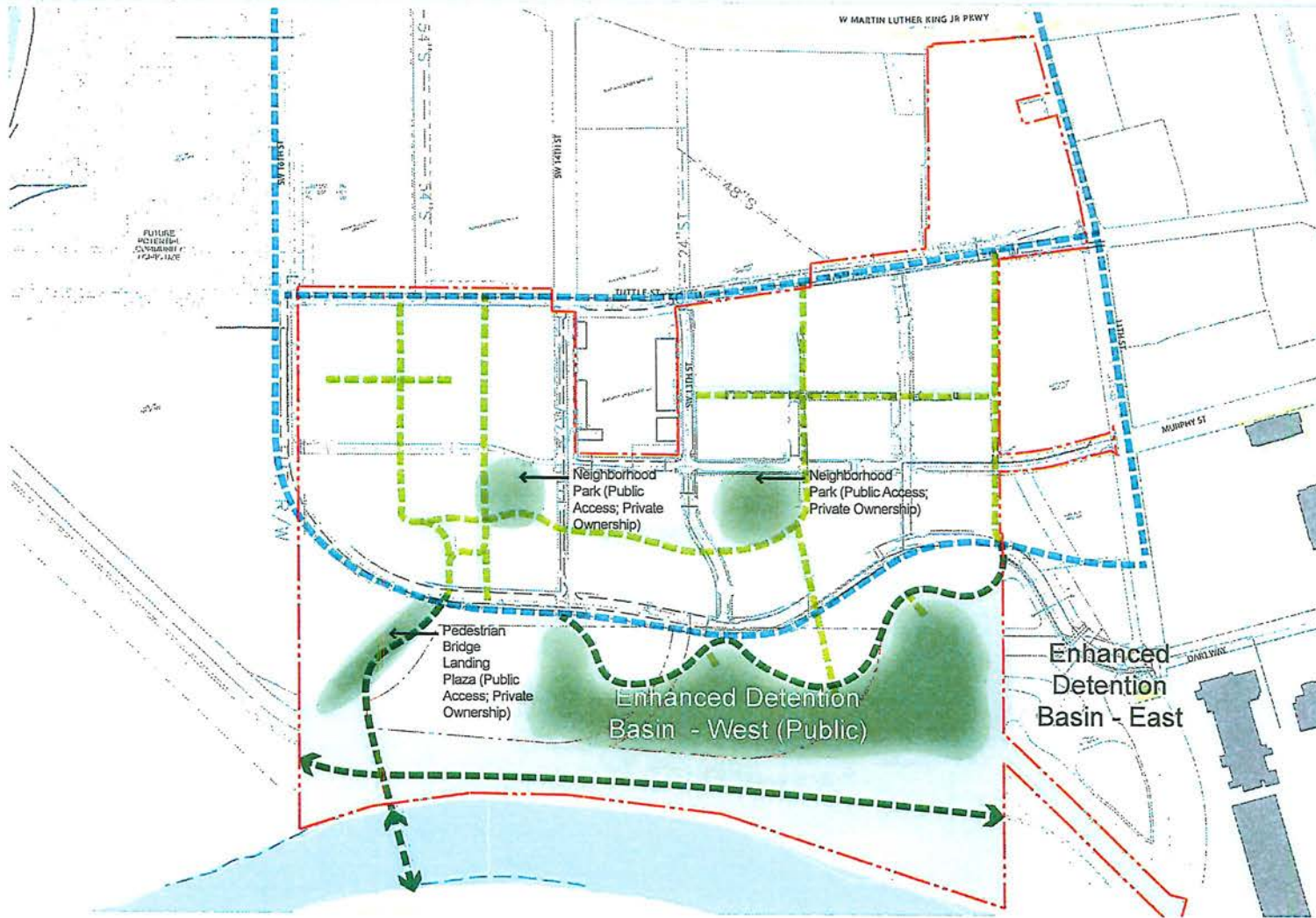


NOTE: BUILDINGS SHOWN IN ALL SECTIONS FOR GENERAL MASSING ONLY. ARCHITECTURAL DESIGN IS YET TO BE DETERMINED. FINAL DEVELOPMENT PLANS TO BE REVIEWED AND

CONCEPTUAL STREET AND ALLEY SECTIONS
 OCTOBER 19, 2022



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa
 Sheet 13 of 20



LEGEND

- Master Plan Area
- Proposed Public or Public Access Area
- Proposed Bicycle/Pedestrian Route – Primary
- Proposed Greenways
- Proposed On-Street Bike Facility
- Existing Regional Trail
- Existing Local Trail

NOTE: See Sheet 2A for land dedication and maintenance plans for open space areas.

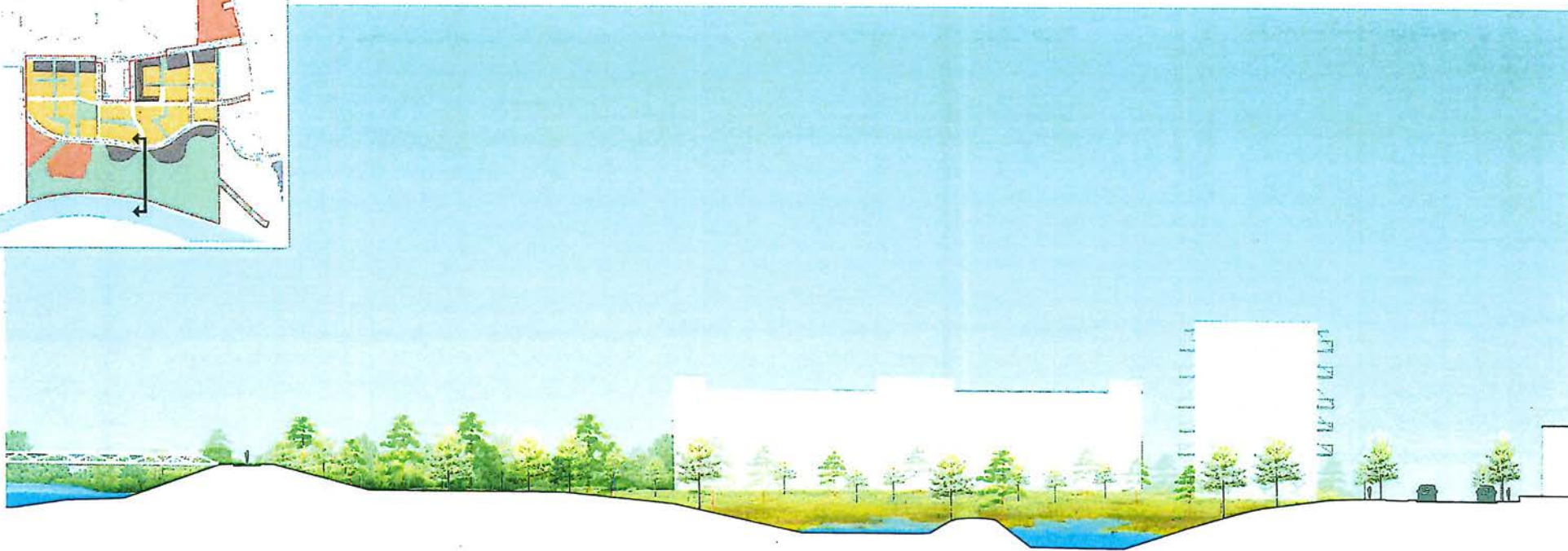
CONCEPTUAL OPEN SPACE PLAN
 OCTOBER 19, 2022



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa
Sheet 14 of 20



GRAY'S STATION



NOTE: BUILDINGS SHOWN FOR GENERAL MASSING ONLY. ARCHITECTURAL DESIGN IS YET TO BE DETERMINED. SUBJECT TO INDIVIDUAL FINAL DEVELOPMENT PLANS TO BE REVIEWED AND APPROVED BY THE PLANNING & ZONING COMMISSION AND CITY COUNCIL.

CONCEPTUAL SECTION ELEVATION THROUGH ENHANCED DETENTION BASIN
OCTOBER 19, 2022



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa

Sheet 15 of 20



GRAY'S STATION

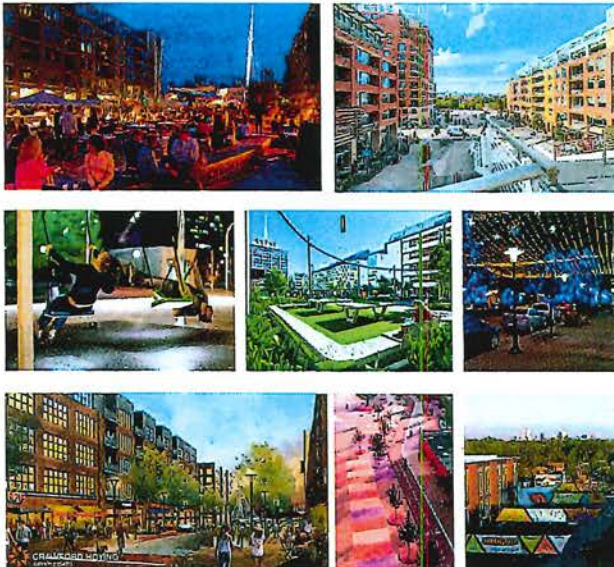
OPEN SPACE CHARACTER

The open space system at Gray's Station is intended to support a diverse, urban population with a focus on wellness; opportunities for social and physical connections; and areas for connections to nature and solitude.

PEDESTRIAN BRIDGE LANDING PLAZA

The following images and conceptual programming represent the intended character and feel of the plaza area anticipated in the Gray's Station neighborhood, but detailed programming and design of the park systems will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Gathering • People-Watching • Outdoor Dining • Festival Space • Bike Connectivity



NEIGHBORHOOD PARKS

The following images and conceptual programming represent the intended character and feel of the neighborhood parks anticipated in the Gray's Station neighborhood, but detailed programming and design of the park systems will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Dog Walking • Playing • Learning • Picnicking • Gathering • Community Gardening • Community Space



GREENWAYS

The following images and conceptual programming represent the intended character and feel of the greenways anticipated in the Gray's Station neighborhood, but detailed programming and design of the park systems will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Connecting • Dog Walking • Playing • Biking • Neighborhood Gatherings



ENHANCED STORMWATER DETENTION BASINS

The following images and conceptual programming represent the intended character and feel of the enhanced regional stormwater detention basins anticipated in the Gray's Station neighborhood, but detailed programming and design of the park systems will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Restoring • Connecting • Learning • Unpaved Trails • Signage • Seating

The proposed enhanced detention basins will provide an attractive, educational water feature with public access and improved water quality within the Raccoon River. It is proposed to have sufficient volume to meet the water quality volume (WQV) requirements for the proposed Gray's Station development and have enough volume to address the 100-year storm event for the upstream watershed areas under full buildout conditions.

- The East Detention Basin area, formerly known as DART pond, is off-site, but is being planned for improvements concurrently with this redevelopment area. WQV requirements for areas within the proposed development will be met through permanent pool storage in the pool and marsh zones of the basin.
- The West Detention Basin area has sufficient volume to meet WQV requirements for areas within the proposed development through permanent pool storage in the pool and marsh zones of the basin.
- Permanent and well-maintained temporary access will be provided to the stormwater pump station at all times and coordinated with the City of Des Moines to ensure operation and maintenance of the station.
- A new security fence will be constructed around the stormwater pump station.

Though its primary purpose is to improve water quality, the enhanced detention basins also serve as a place to enhance the health of the community and to offer educational opportunities for the larger community. At a Master Plan level, the area features paved and unpaved trails, wetland overlook areas, and seating areas. The proposed inclusion of interpretive signage and art would allow people of all ages to understand the role of the area for the region. The use of micro-grading for the basins allows the area to be carefully shaped. This shaping breaks down the scale of the regional detention, creates places that are enjoyable, and removes the utilitarian feel of the existing basins.



PROPOSED OPEN SPACE CHARACTER IMAGERY
OCTOBER 19, 2022

Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa

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GRAY'S STATION

PRECEDENT HOUSING CHARACTER IMAGERY

As a large neighborhood, the proposed housing character for Gray's Station is intended to be diverse and urban in form/character. The following images represent the intended character and feel of the multi-family and mixed-use residential housing types. The design of the buildings including architectural details, materials, final massing/heights and colors will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Each character image represents varied features as represented by the following categories that correspond to the colors under each image:

- Relationship to exterior open space (plaza, park, roof deck, etc.)
- Relationship to street
- Relationship to alley
- Building articulation/massing
- Architectural character
- Landscape character

DOWNTOWN MIXED USE (NE CORNER)



RESIDENTIAL

MULTI-FAMILY / MEDIUM-HIGH DENSITY



DOWNTOWN MIXED USE (BRIDGE LANDING)



PROPOSED BUILDING CHARACTER IMAGERY
OCTOBER 19, 2022



Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa
 Sheet 17 of 20



GRAY'S STATION

PRECEDENT HOUSING CHARACTER IMAGERY

As a large neighborhood, the proposed housing character for Gray's Station is intended to be diverse and urban in form/character. The following images represent the intended character and feel of the medium density housing types. The design of the buildings including architectural details, materials, final massing/heights and colors will be completed during later phases of the development. Final Development Plans to be reviewed by the Planning & Zoning Commission and by City Council.

Each character image represents varied features as represented by the following categories that correspond to the colors under each image:

- Relationship to exterior open space (plaza, park, roof deck, etc.)
- Relationship to street
- Relationship to alley
- Building articulation/massing
- Architectural character
- Landscape character

MEDIUM DENSITY RESIDENTIAL



PROPOSED BUILDING CHARACTER IMAGERY
 OCTOBER 19, 2022

Conceptual Plan Gray's Station, a Planned Unit Development in the City of Des Moines, County of Polk, State of Iowa
Sheet 18 of 20



DOWNTOWN MIXED USE - PROTOTYPICAL BUILDING

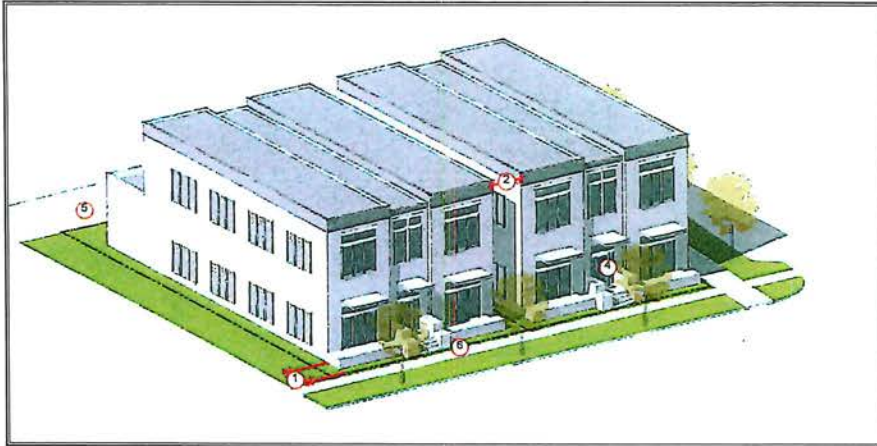
1. Primary facades of mixed-use and commercial structures shall be primarily parallel to the public right-of-way except in the subareas of W and X.
2. Buildings shall have entrances oriented toward primary street(s) and public plaza(s).
3. At least one building entrance for residential uses should directly access the street or public plaza when a residential use is located above street-level retail or commercial uses.
4. All buildings should have frontage on principal street(s), public plaza, park or greenway of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).
5. For commercial and mixed-use buildings, at least 70 percent of the building frontage should be within one foot of the property line (with exceptions as outlined in the Design Guidelines).
6. Facades of above grade structured parking (e.g., podium parking beneath commercial or residential uses) adjacent to any public right-of-way shall be architecturally and aesthetically consistent with the remainder of the building they support.
7. Minimum height for all Downtown Mixed Use buildings shall be four stories.
8. All Downtown Mixed Use buildings shall have a parapet or flat roof. Tower elements are allowed.



HIGH DENSITY RESIDENTIAL - PROTOTYPICAL BUILDING

1. For residential buildings, a maximum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
2. Buildings shall have entrances oriented toward primary street(s) and public plaza(s).
3. All buildings should have frontage on principal street(s), public plaza, park or greenway of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).
4. Facades of above grade structured parking (e.g., podium parking beneath commercial or residential uses) adjacent to any public right-of-way shall be architecturally and aesthetically consistent with the remainder of the building they support.
5. Minimum height for all uses shall be as follows:
 - a) Three stories for Subareas B, H, N, and O
 - b) Eight stories for Subareas Y and Z
6. All Downtown Mixed Use buildings shall have a parapet or flat roof. Tower elements are allowed.

PROPOSED PROTOTYPICAL FORMS
OCTOBER 19, 2022



MEDIUM DENSITY RESIDENTIAL - PROTOTYPICAL MULTI-UNIT DWELLING UNIT

1. For all residential buildings, a maximum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
2. Attached and Detached residential buildings shall have side-yard building separation(s) (i.e., building face to building face) that are at minimum 5 feet and at maximum 10 feet except where walking paths occur between buildings.
 - a. Where walking paths occur between buildings the maximum width between buildings shall increase above the maximum separation by the width of the walking path.
3. Greenways shall include pedestrian pathways with a minimum width of 5'
4. Buildings shall have entrances oriented toward primary street(s), public plaza(s), parks, greenways or alleys.
5. All garages shall be oriented to an alley.
6. All buildings should have frontage on principal street(s), public plaza, park, greenways or alleys of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).



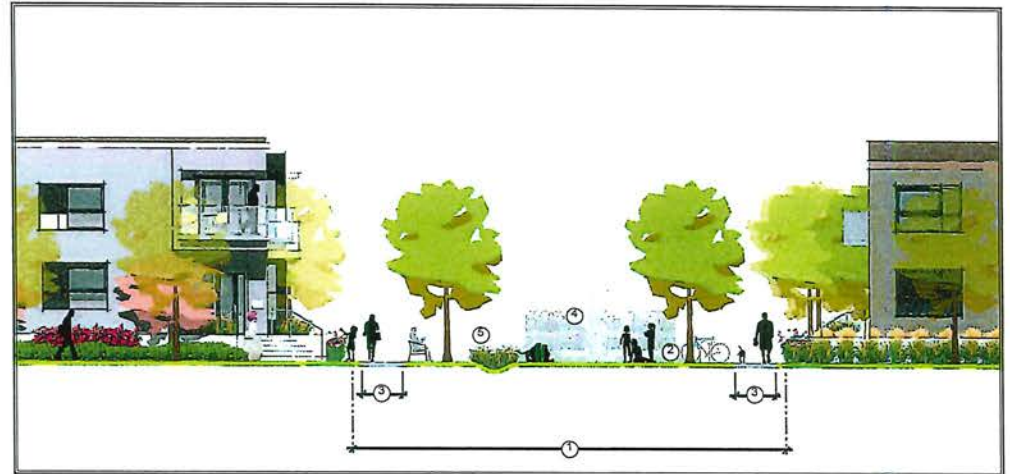
MEDIUM DENSITY RESIDENTIAL - PROTOTYPICAL DETACHED FORM (WITH FOURPLEX)

1. For all residential buildings, a maximum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
2. Attached and Detached residential buildings shall have side-yard building separation(s) (i.e., building face to building face) that are at minimum 5 feet and at maximum 10 feet except where walking paths occur between buildings.
 - a. Where walking paths occur between buildings the maximum width between buildings shall increase above the maximum separation by the width of the walking path.
3. Greenways shall include pedestrian pathways with a minimum width of 5'
4. Buildings shall have entrances oriented toward primary street(s), public plaza(s), parks, greenways or alleys.
5. All garages shall be oriented to an alley.
6. Accessory Dwelling Units shall be allowed.
7. All buildings should have frontage on principal street(s), public plaza, park, greenways or alleys of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).



MEDIUM DENSITY RESIDENTIAL - PROTOTYPICAL ATTACHED FORM

1. For all residential buildings, a maximum setback of 15 feet from the public right-of-way is permitted unless constrained by utility easements.
2. Attached and Detached residential buildings shall have side-yard building separation(s) (i.e., building face to building face) that are at minimum 5 feet and at maximum 10 feet except where walking paths occur between buildings.
 - a. Where walking paths occur between buildings the maximum width between buildings shall increase above the maximum separation by the width of the walking path.
3. Greenways shall include pedestrian pathways with a minimum width of 5'
4. Buildings shall have entrances oriented toward primary street(s), public plaza(s), parks, greenways or alleys.
5. All garages shall be oriented to an alley.
6. Accessory Dwelling Units shall be allowed.
7. All buildings should have frontage on principal street(s), public plaza, park, greenways or alleys of not less than 70 percent of the lot (with exceptions as outlined in the Design Guidelines).



GREENWAY - PROTOTYPICAL FORM

Green connections are encouraged to minimize impact of storm water, provide urban pathways for wildlife, and promote recreation, wellness, and pedestrian scale mobility.

1. Greenways shall maintain an approximate width of 40' to 50'. Exceptions may occur where infrastructure requirements necessitate and will be reviewed with the Final Development Plan.
2. Greenways shall include landscaping.
3. Greenways shall include pedestrian pathways with a minimum width of 5'. Pathway location may vary depending on context.
4. Greenways may include structural (e.g., Pergola, Shade Structure, Community Garden Features, etc.) and/or artistic elements where deemed appropriate by the developer.
5. Greenways may include stormwater quality areas, where appropriate.

SITE DEVELOPMENT PLAN FOR: GRAY'S STATION TELUS

DES MOINES, IOWA

VICINITY - MAP

NOT TO SCALE



DES MOINES, IOWA

OWNER

HRC NPS I LLC
6900 WESTOWN PKWY
WEST DES MOINES, IA 50266

APPLICANT

HUBBELL REALTY COMPANY
CONTACT: KRIS SADDORIS
6900 WESTOWN PKWY
WEST DES MOINES, IA 50266
PH: (515) 243-3228

ENGINEER

CIVIL DESIGN ADVANTAGE
4121 NW URBANDALE DRIVE
URBANDALE, IA 50322
CONTACT: RYAN HARGISTY
EMAIL: RYANH@CDA-ENG.COM
PH: (515) 369-4400

SURVEYOR

CIVIL DESIGN ADVANTAGE, LLC
4121 NW URBANDALE DRIVE
URBANDALE, IA 50322
CONTACT: CHARLIE MCLOTHLEN
EMAIL: CHARLEM@CDA-ENG.COM
PH: (515) 369-4400

ARCHITECT

SIMONSON & ASSOCIATES, INC.
1717 INGERSOLL AVE SUITE #117
DES MOINES, IA 50329
CONTACT: STEPHANIE POOLE
EMAIL: SPOOLE@SIMONSONASSOC.COM
PH: (515) 440-5626

SUBMITTAL DATES

FIRST SUBMITTAL: 10/18/2022
SECOND SUBMITTAL: 01/04/2023

ZONING

GRAY'S STATION PUD - ZON2017-00087

EXISTING/ PROPOSED USE

EXISTING: UNDEVELOPED
PROPOSED: HOUSEHOLD LIVING - 8+ HOUSEHOLDS PER LOT

DEVELOPMENT SUMMARY

IDEAL SITE AREA: 3.48 ACRES (151,395 SF)

EXISTING IMPERVIOUS AREA: 0.00 ACRES (0 SF)

DEVELOPMENT SUMMARY

DEVELOPMENT SUMMARY

DEVELOPMENT SUMMARY

DEVELOPMENT SUMMARY

DATE OF SURVEY

08/05/2022

BENCHMARKS

- CITY BENCHMARK #725: BRASS CAP IN THE NORTHEAST TRAFFIC SIGNAL BASE AT THE SOUTHWEST CORNER OF MARTIN LUTHER KING DRIVE AND SOUTHWEST 11TH STREET. ELEVATION=28.81
- CITY BENCHMARK #5561: BRASS TABLET IN THE CONCRETE WALL 43.6 FEET WEST OF THE EAST FACE NEAR THE SANITARY SEWER PUMP STATION. ELEVATION=25.48

LEGAL DESCRIPTION

LOT 67, GRAY'S STATION PLAT 5, AN OFFICIAL PLAT IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND CONTAINING 3.48 ACRES (151,395 S.F.).

TAX ABATEMENT - SUSTAINABILITY

- BIKE RACKS FOR PUBLIC USE THAT PROVIDE A MINIMUM OF 10% OF THE AUTOMOBILE PARKING PROVIDED.
- 50% INCREASE IN REQUIRED LANDSCAPING.
- PRIMARY ENTRY WITHIN 1/4 MILE OF THE EAST TRANSIT STOP.
- REDEVELOPMENT OF A PREVIOUSLY DEVELOPED SITE.

INDEX OF SHEETS

NO.	DESCRIPTION
CO.1	COVER SHEET
C1.0-1.2	DIMENSION PLAN
C2.0-2.6	GRADING PLAN
C3.0-3.2	UTILITY PLAN
C4.0	DETAILS
L02.02 & L02.04	RDG LANDSCAPE PLAN - SITE LAYOUT PLAN
L04.02 & L04.04	RDG LANDSCAPE PLAN - SITE PLANTING PLAN
L05.01	RDG LANDSCAPE PLAN - DETAILS

GENERAL LEGEND

PROPOSED	EXISTING
PROPERTY BOUNDARY	SANITARY MANHOLE
SECTION LINE	WATER VALVE BOX
CENTER LINE	FIRE HYDRANT
RIGHT OF WAY	WATER CURB STOP
BUILDING SETBACK	WELL
PERMANENT EASEMENT	STORM SEWER MANHOLE
TEMPORARY EASEMENT	STORM SEWER SINGLE INTAKE
TYPE SW-501 STORM INTAKE	STORM SEWER DOUBLE INTAKE
TYPE SW-502 STORM INTAKE	FLARED END SECTION
TYPE SW-503 STORM INTAKE	DECIDUOUS TREE
TYPE SW-505 STORM INTAKE	CONIFEROUS TREE
TYPE SW-508 STORM INTAKE	DECIDUOUS SHRUB
TYPE SW-512 STORM INTAKE	CONIFEROUS SHRUB
TYPE SW-513 STORM INTAKE	ELECTRIC POWER POLE
TYPE SW-401 STORM MANHOLE	GUY ANCHOR
TYPE SW-402 STORM MANHOLE	STREET LIGHT
FLARED END SECTION	POWER POLE W/ TRANSFORMER
TYPE SW-301 SANITARY MANHOLE	UTILITY POLE W/ LIGHT
STORM/SANITARY CLEANOUT	ELECTRIC BOX
WATER VALVE	ELECTRIC TRANSFORMER
FIRE HYDRANT ASSEMBLY	ELECTRIC MANHOLE OR VAULT
SIGN	TRAFFIC SIGN
DETECTABLE WARNING PANEL	TELEPHONE JUNCTION BOX
WATER CURB STOP	TELEPHONE MANHOLE/VAULT
SANITARY SEWER	TELEPHONE POLE
SANITARY SERVICE	CABLE VALVE BOX
STORM SEWER	CABLE TV JUNCTION BOX
WATERMAIN WITH SIZE	CABLE TV MANHOLE/VAULT
WATER SERVICE	MAIL BOX
SAWCUT (FULL DEPTH)	BENCHMARK
SILT FENCE	SOIL BORING
USE AS CONSTRUCTED	UNDERGROUND TV CABLE
MINIMUM PROTECTION ELEVATION	GAS MAIN
	FIBER OPTIC
	UNDERGROUND TELEPHONE
	OVERHEAD ELECTRIC
	UNDERGROUND ELECTRIC
	FIELD TILE
	SANITARY SEWER W/ SIZE
	STORM SEWER W/ SIZE
	WATER MAIN W/ SIZE



UTILITY WARNING

ANY UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY AND RECORDS OBTAINED BY THIS SURVEYOR. THE SURVEYOR MAKES NO GUARANTEE THAT THE UTILITIES SHOWN COMPRISE ALL THE UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UTILITIES SHOWN ARE IN THE EXACT LOCATION SHOWN.



CIVIL DESIGN ADVANTAGE

4121 NW URBANDALE DRIVE, URBANDALE, IA 50322
PH: (515) 369-4400

PROJECT NO. 2205.350

SEAL AND APPROVALS

APPROVED APPROVED WITH CONDITIONS - SEE EXHIBIT "A"

IN ACCORDANCE WITH SECTION 133-9, 2019 IOWA MUNICIPAL CODE, AS AMENDED.

NO CHANGES TO THIS PLAN UNLESS APPROVED BY WRITING FROM THE DEVELOPER'S SERVICES DIRECTOR.

DEVELOPMENT SERVICES DIRECTOR: _____ DATE: _____

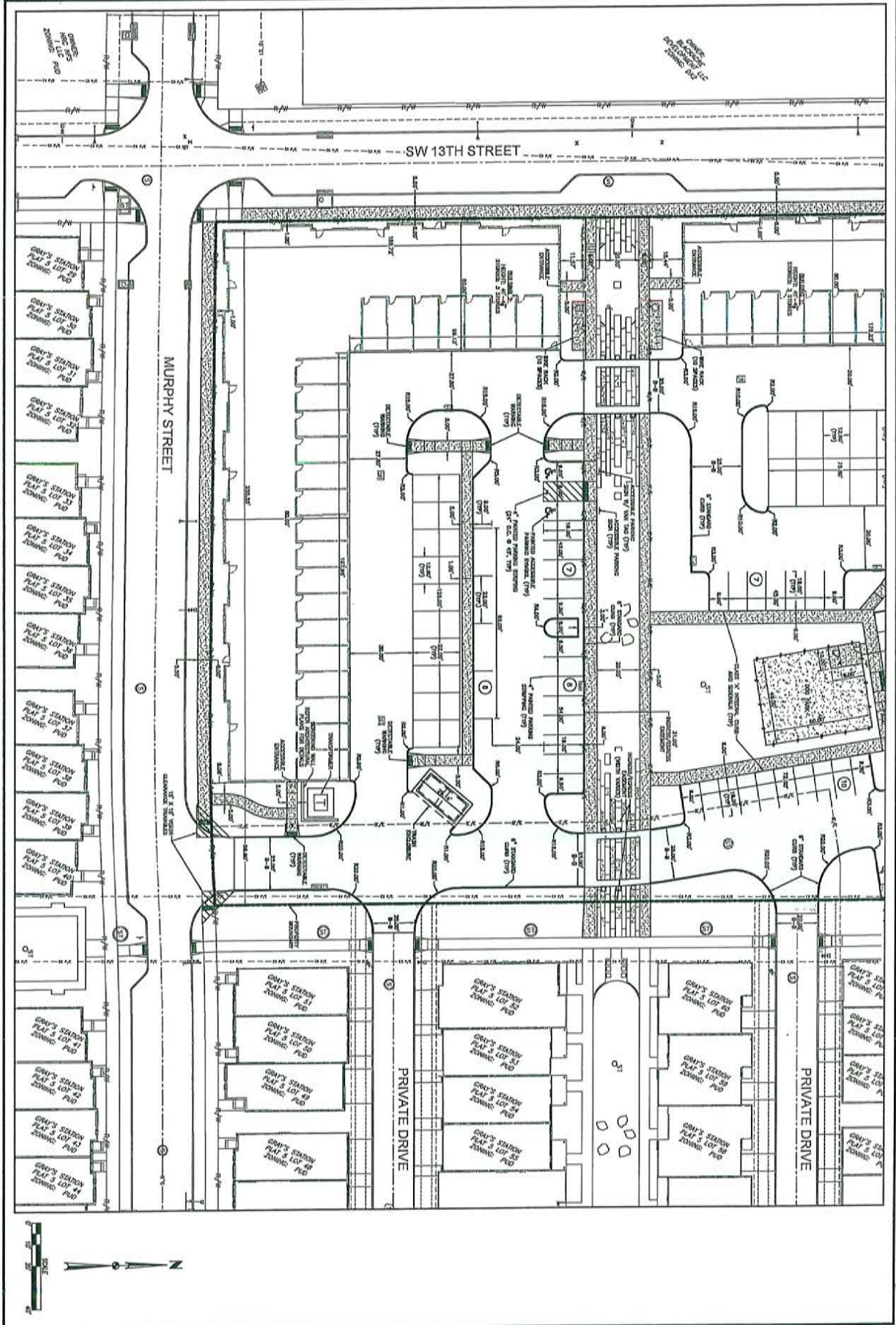
THE PROJECT REQUIRES AN IOWA NPDES PERMIT #2 AND CITY OF DES MOINES GRADING PERMIT. CIVIL DESIGN ADVANTAGE WILL PROVIDE THE PERMITS AND THE INITIAL STORM WATER POLLUTION PREVENTION PLAN (SWPPP) FOR THE CONTRACTORS USE DURING CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR UPDATING THE SWPPP THROUGHOUT CONSTRUCTION AND MEETING LOCAL, STATE AND FEDERAL REQUIREMENTS.

ALL CONSTRUCTION MATERIALS, DUMPSTERS, DETACHED TRAILERS OR SIMILAR ITEMS ARE PROHIBITED ON PUBLIC STREETS OR WITHIN THE PUBLIC R.O.W.

THE 2022 EDITION OF THE SDAS STANDARD SPECIFICATIONS, THE PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) AND ALL CITY SUPPLEMENTALS, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.

I HEREBY CERTIFY THAT THIS ENGINEERING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF IOWA.

DATE: _____
PROJECT NUMBER: 2205.350
ISSUE DATE: DECEMBER 21, 2023
PAGES OF SHEETS COVERED BY THIS SEAL: _____
ALL C SERIES SHEETS



22005.930
C12

GRAY'S STATION TELUS
 DIMENSION PLAN

DES MOINES, IOWA

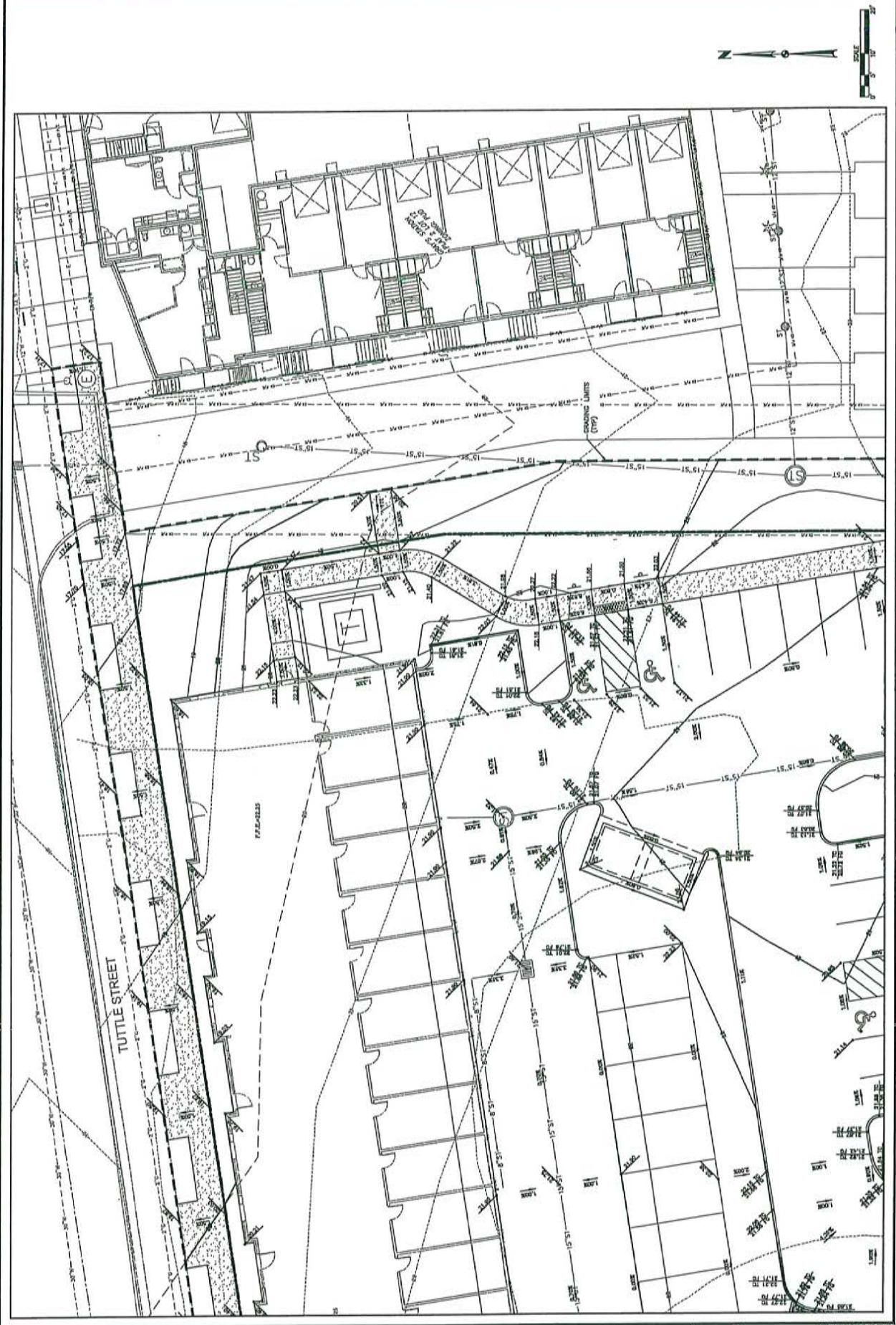


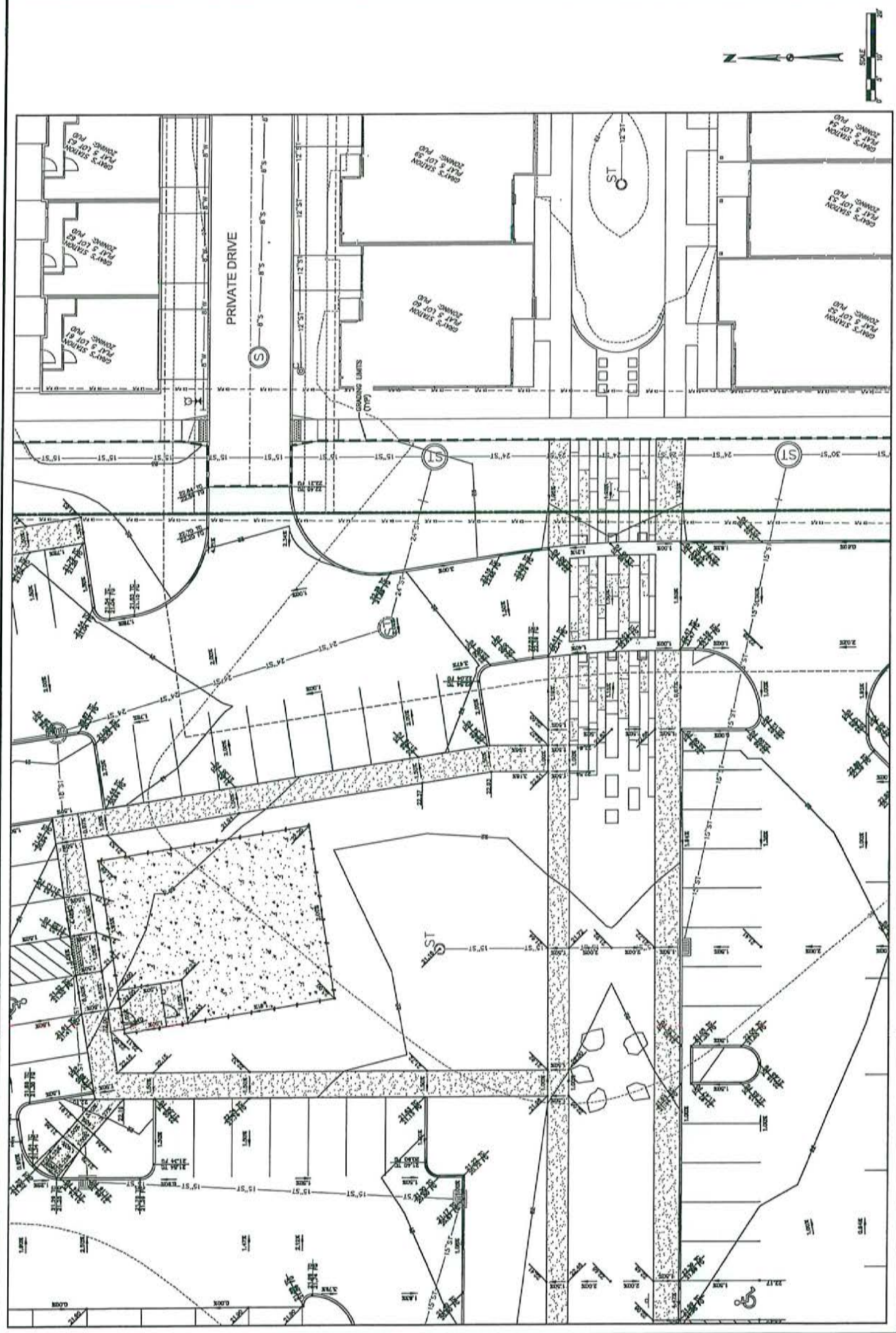
ENGINEER:

4121 NW URBANDALE DRIVE
 URBANDALE, IA 50322
 PHONE: (515) 369-4400

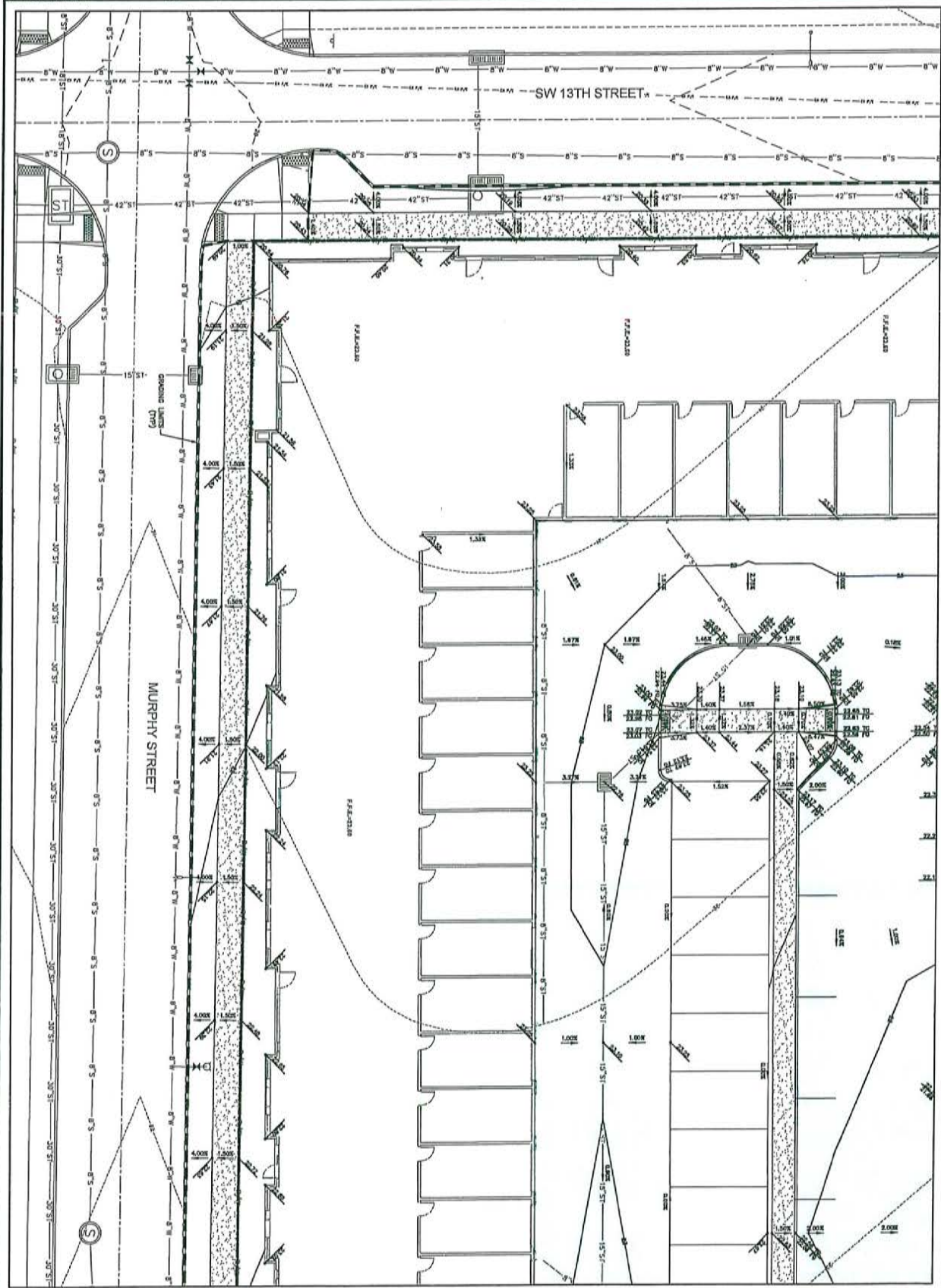
TECH:

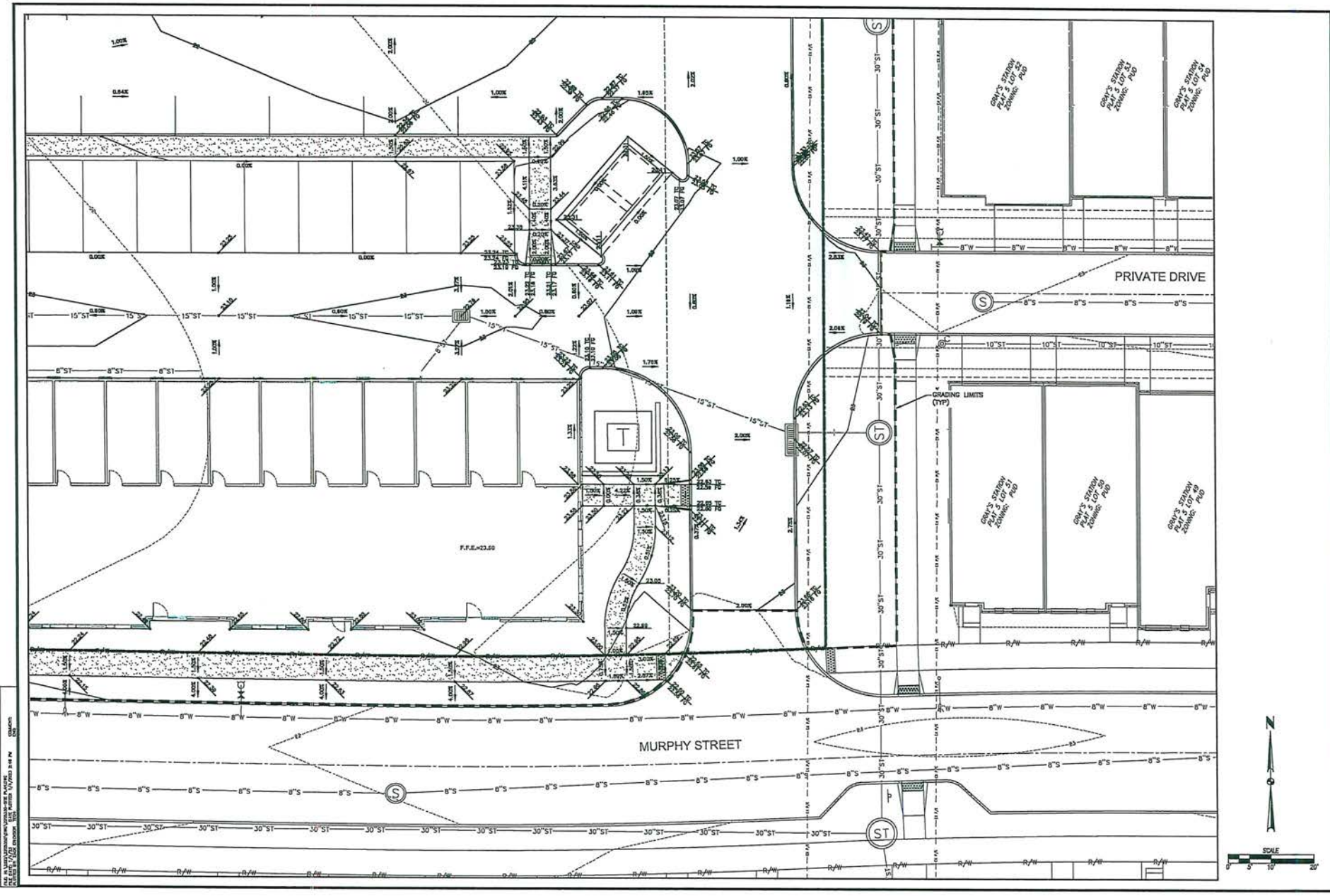
REVISIONS	DATE
SECOND SUBMITTAL	01/24/2022
FIRST SUBMITTAL	01/19/2022





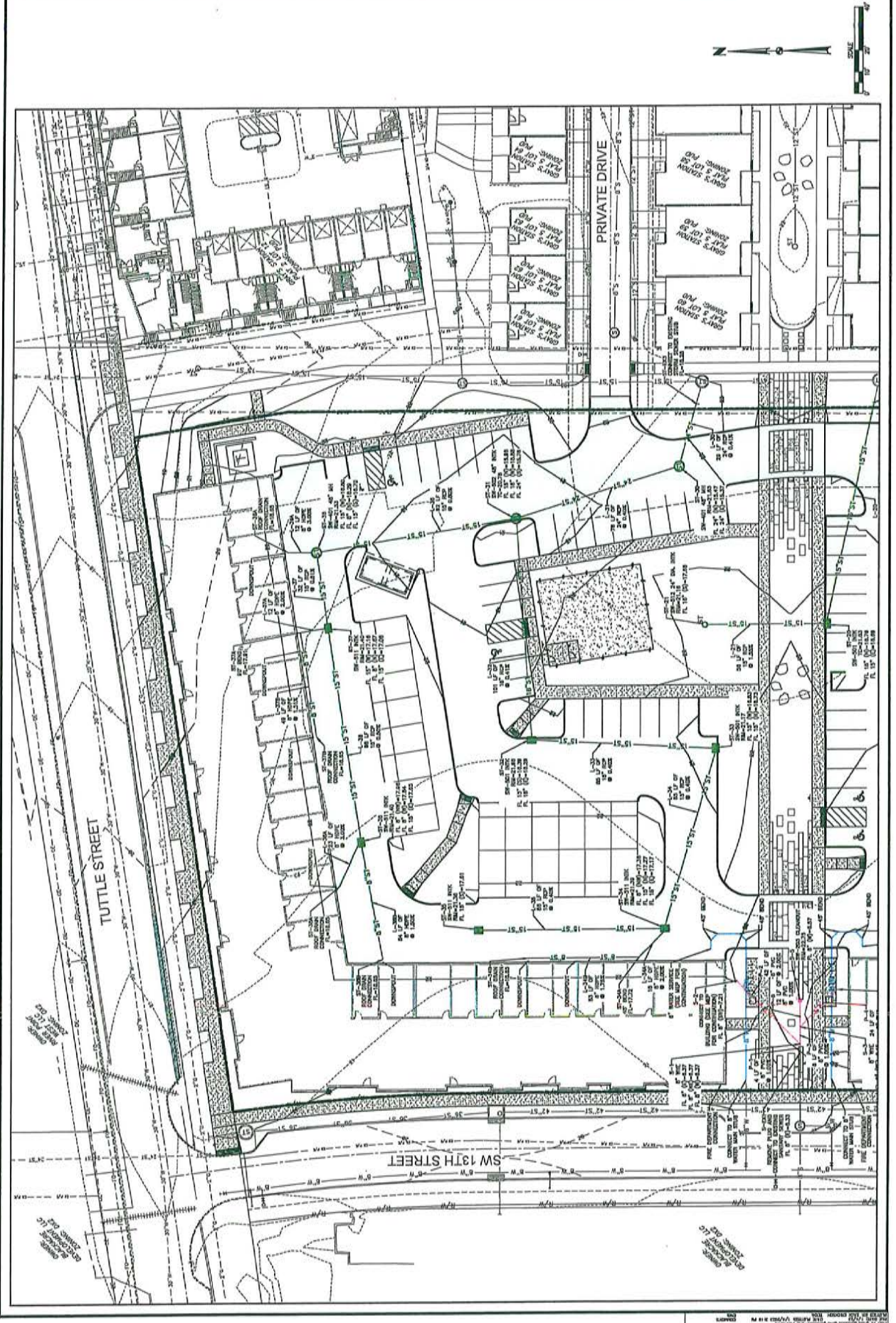
THIS PLAN IS THE PROPERTY OF CIVIL DESIGN ADVANTAGE, INC. AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF CIVIL DESIGN ADVANTAGE, INC.

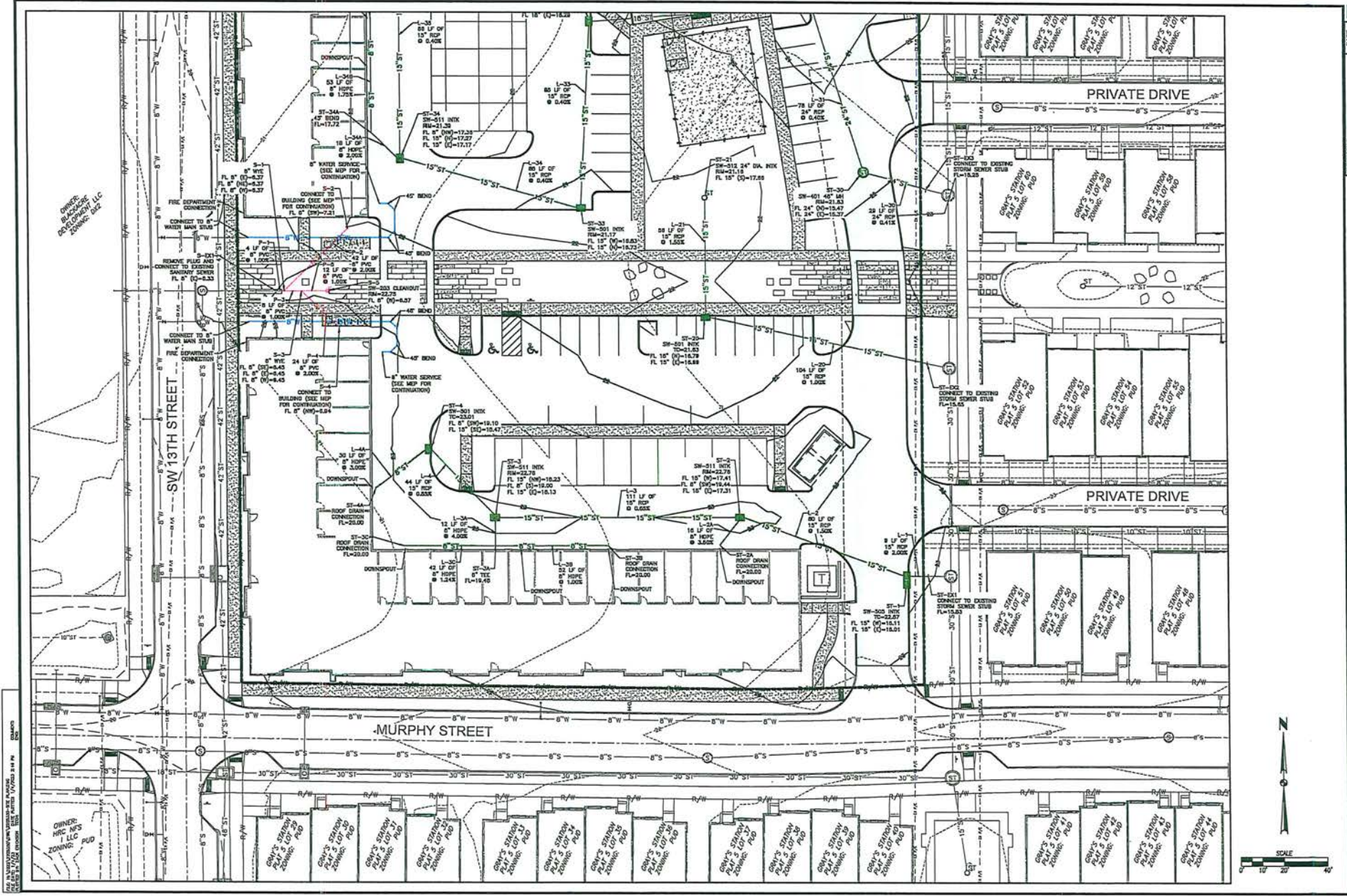




ALL DIMENSIONS UNLESS OTHERWISE NOTED
 ALL DIMENSIONS TO CENTERLINE UNLESS NOTED OTHERWISE
 ALL DIMENSIONS TO FACE UNLESS NOTED OTHERWISE
 ALL DIMENSIONS TO CENTERLINE UNLESS NOTED OTHERWISE

DATE		EA CIVIL DESIGN ADVANTAGE DES MOINES, IOWA	GRAY'S STATION TELUS GRADING PLAN	ENGINEER: TECH: ENGINEER:
REVISIONS			4121 NW URBANDALE DRIVE URBANDALE, IA 50322 PHONE: (515) 389-4460	
SECOND SUBMITTAL	3/10/2020			
FIRST SUBMITTAL	10/19/2019			
C2.6	2205.350			





DATE: 11/11/2020
 DRAWN BY: J. WILSON
 CHECKED BY: J. WILSON
 PROJECT: 2205.350

OWNER:
 HRC AFS
 I LLC
 ZONING: PUD

OWNER:
 HRC AFS
 I LLC
 ZONING: PUD



NO.	DATE	REVISIONS

4121 NW URBANDALE DRIVE
 URBANDALE, IA 50322
 PHONE: (515) 389-4400

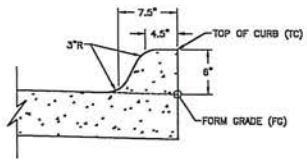
TECH: _____
 ENGINEER: _____



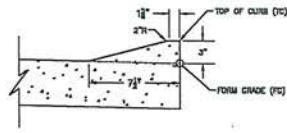
DES MOINES, IOWA
 CIVIL DESIGN ADVANTAGE

GRAY'S STATION TELUS
 UTILITY PLAN

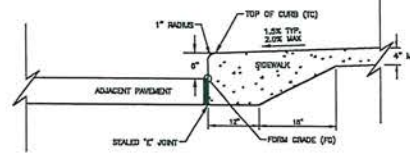
C3.2
 2205.350



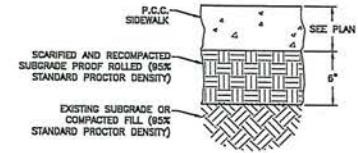
1 6" INTEGRAL CURB
NOT TO SCALE



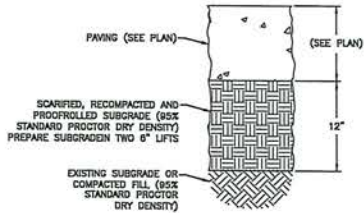
2 3" INTEGRAL ROLLED CURB
NOT TO SCALE



3 CLASS 'A' INTEGRAL CURB AND SIDEWALK
NOT TO SCALE

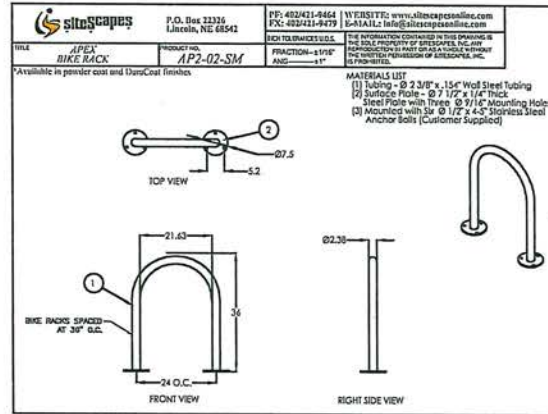


4 SIDEWALK
NOT TO SCALE

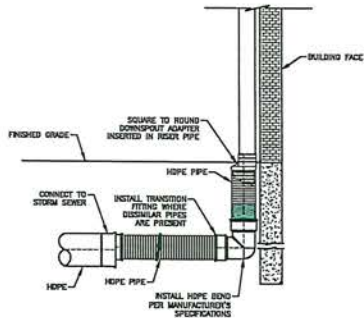


NOTES:
1. SUBGRADE PREPARATION SHALL EXTEND 24" BEYOND THE EDGE OF PAVING OR BACK OF CURB, EXCEPT WHEN ABUTTING EXISTING PAVEMENT.

5 12" SUBGRADE PREPARATION
NOT TO SCALE



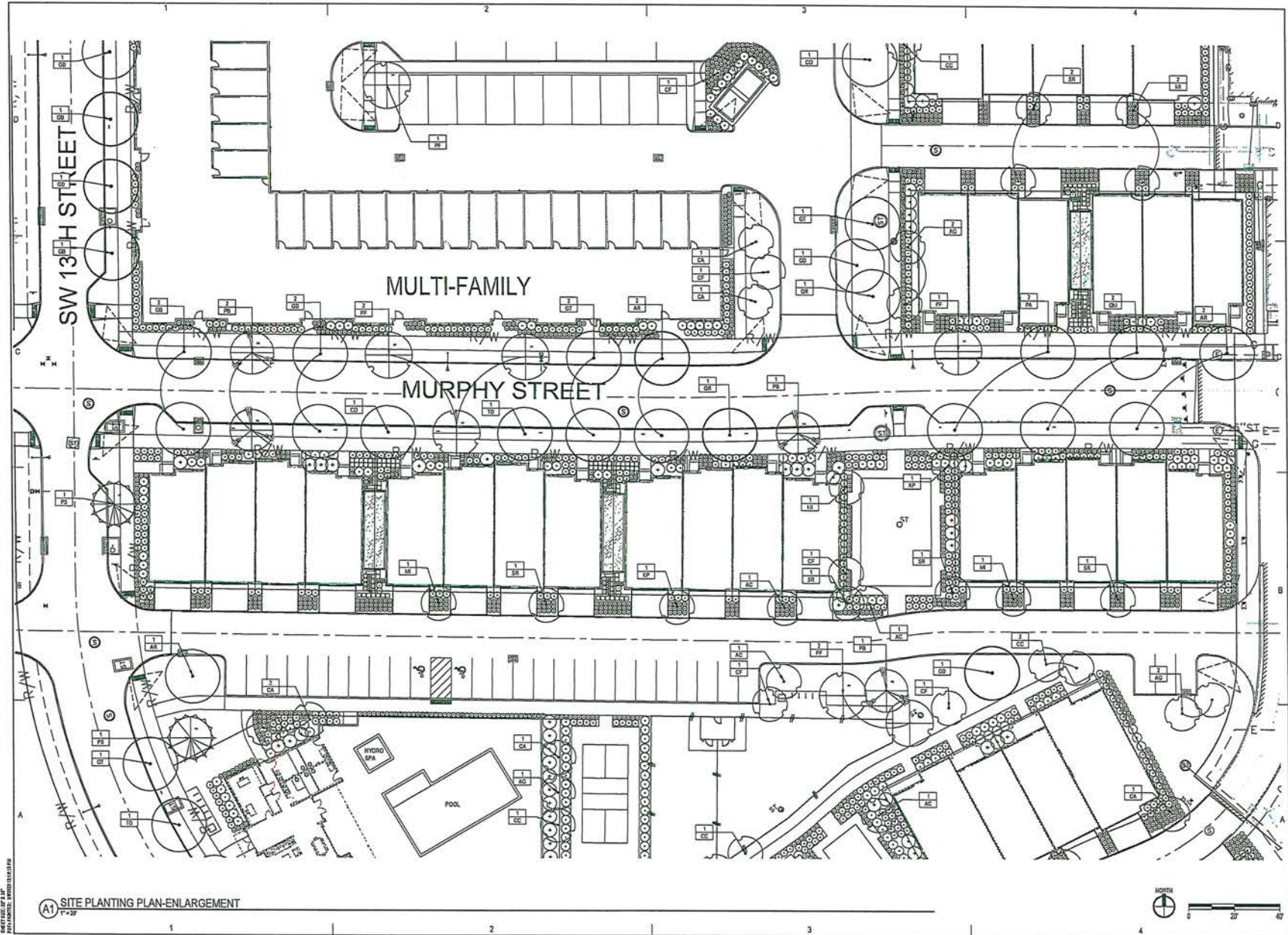
6 BIKE RACK
NOT TO SCALE



7 ROOF DRAIN CONNECTION WITH TEE
NOT TO SCALE

SEE NOTES AND SPECIFICATIONS FOR ALL MATERIALS AND FINISHES. ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

DATE	01/04/2022
REVISIONS	
SECOND SUBMITTAL	
FIRST SUBMITTAL	
TECH:	
ENGINEER:	
4121 NW URBANDALE DRIVE URBANDALE, IA 50322 PHONE: (515) 989-4400	
GRAY'S STATION TELUS DETAILS	
DES MOINES, IOWA	
C4.0	
2205.350	



SCALE: 1/4" = 1'-0"

(A1) SITE PLANTING PLAN-ENLARGEMENT
7-22

GRAY'S STATION PLAT 5

PRELIMINARY - NOT FOR CONSTRUCTION

DESIGN DEVELOPMENT

DATE	DESCRIPTION	BY	CHKD

SITE PLANTING PLAN

L04.04

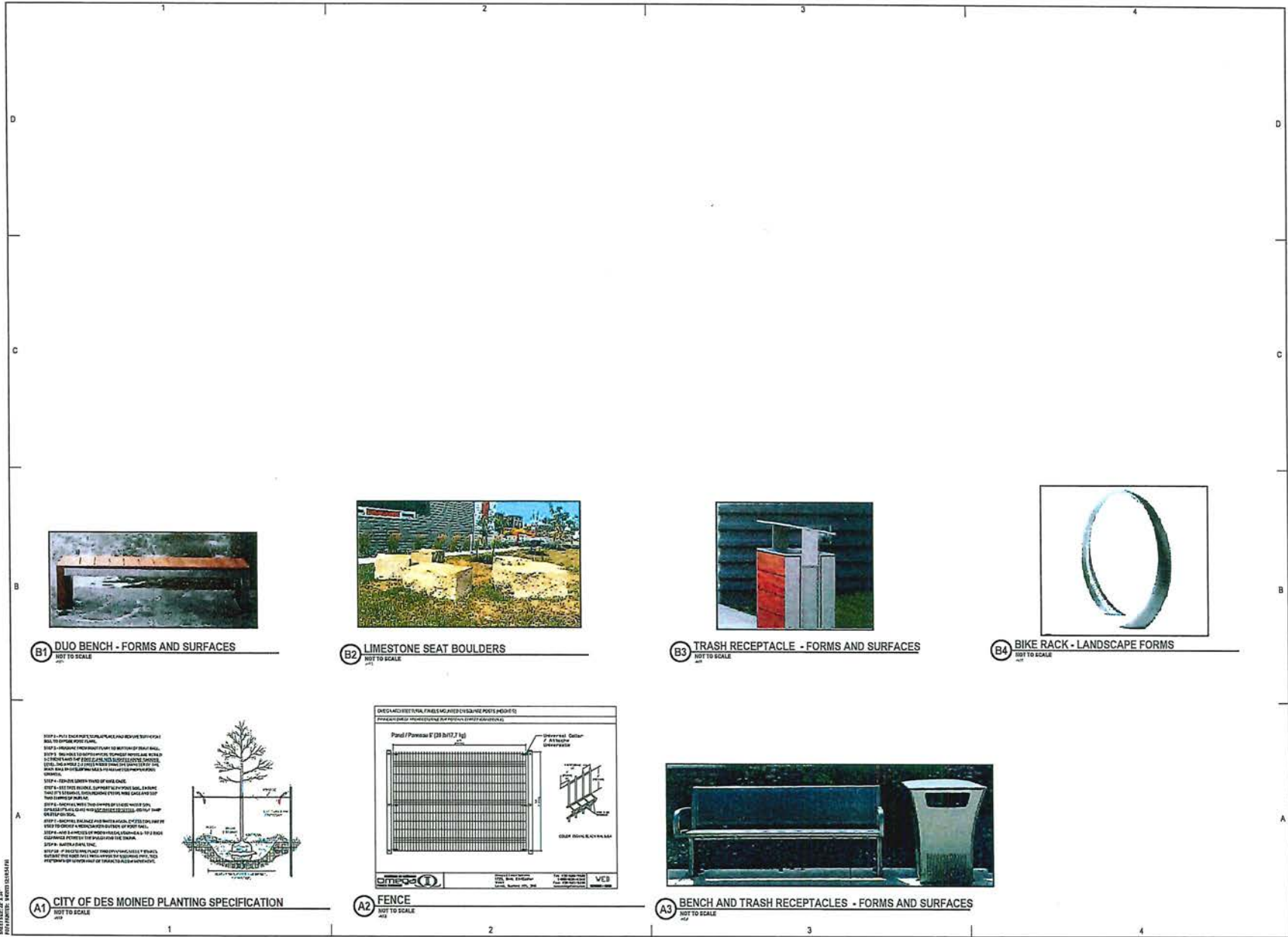
HUBBELL REALTY

KEY PLAN

ARCHITECT
RDG Planning & Design
10000 SW 15th Ave.
Suite 200
Miami, FL 33199
(305) 555-1234

LANDSCAPE ARCHITECT
Hubbell Realty
10000 SW 15th Ave.
Suite 200
Miami, FL 33199
(305) 555-1234





DESIGN DEVELOPMENT

PRELIMINARY - NOT FOR CONSTRUCTION

GRAY'S STATION PLAT 5

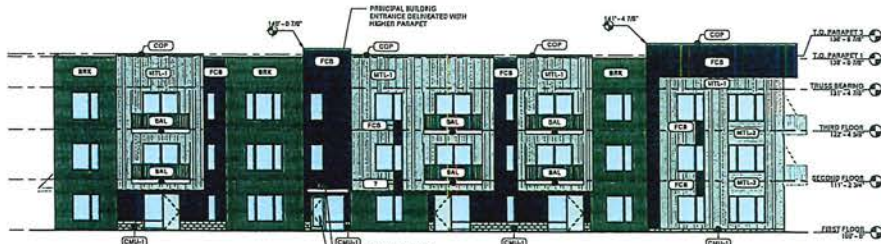


ARCHITECT
 2010-2011
 LANDSCAPE ARCHITECT
 2011-2012
 2012-2013

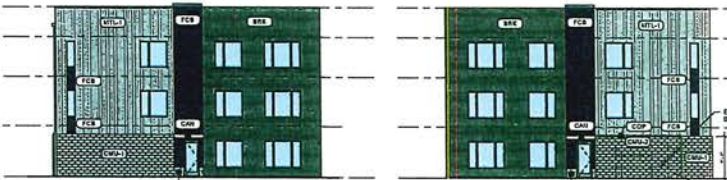
HUBBELL REALTY

REVISION	DATE

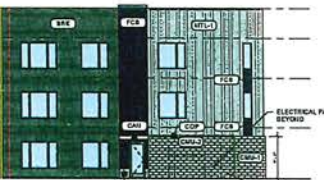
PROJECT: GRAY'S STATION PLAT 5
 LOCATION: 10000 100th Ave, Des Moines, IA 50319
 CLIENT: HUBBELL REALTY
 ARCHITECT: RDG PLANNING & DESIGN
 LANDSCAPE ARCHITECT: RDG PLANNING & DESIGN
 DATE: 10/2013



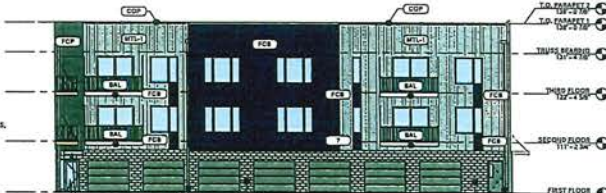
1 BUILDING #2 SW 13TH STREET
SCALE 3/8" = 1'-0"



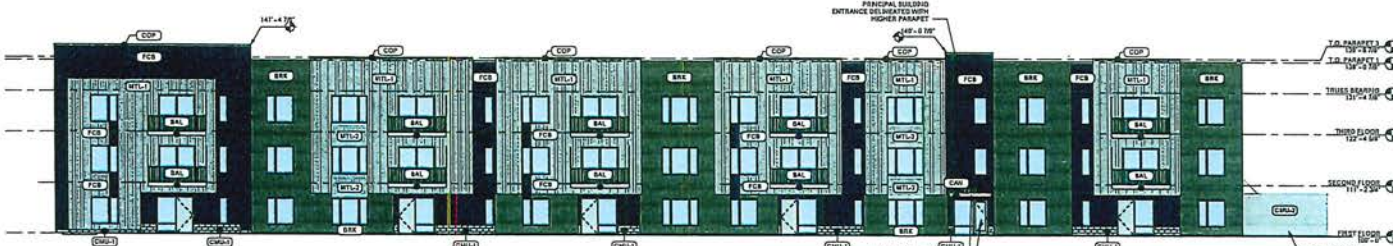
6 BUILDING #2 - SIDE (GREEN BELT)
SCALE 3/8" = 1'-0"



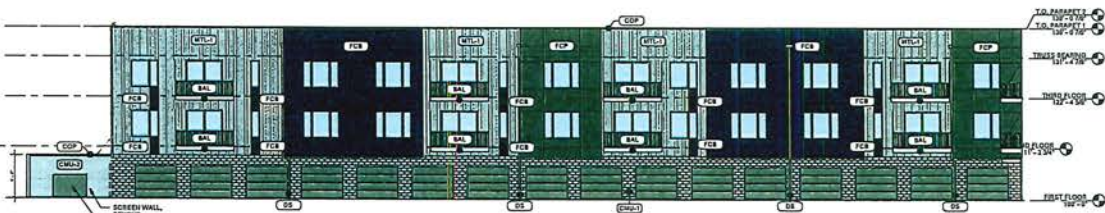
4 BUILDING #2 SIDE (EAST END)
SCALE 3/8" = 1'-0"



3 BUILDING #2 - INTERNAL ELEVATION (EAST)
SCALE 3/8" = 1'-0"



2 BUILDING #2 - MURPHY STREET
SCALE 3/8" = 1'-0"



5 BUILDING #2 - INTERNAL ELEVATION (SOUTH)
SCALE 3/8" = 1'-0"

EXTERIOR MATERIAL LEGEND	
MAT. LABEL	MATERIAL DESCRIPTION
BRK	PRE-FABRICATED BALCONY CLEAR ANODIZED BRAY
BAL	PULL DOWN COLORED BRAY, NATURAL, MORTAR, COLOR - GRAY
CMU	PRE-FABRICATED CONCRETE, 8" EQUAL, COLOR, CLEAR ANODIZED
CMU1	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU2	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU3	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU4	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU5	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU6	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU7	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU8	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU9	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU10	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU11	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU12	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU13	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU14	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU15	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU16	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU17	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU18	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU19	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU20	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU21	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU22	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU23	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU24	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU25	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU26	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU27	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU28	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU29	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU30	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU31	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU32	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU33	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU34	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU35	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU36	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU37	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU38	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU39	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU40	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU41	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU42	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU43	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU44	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU45	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU46	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU47	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU48	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU49	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU50	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU51	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU52	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU53	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU54	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
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CMU57	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU58	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU59	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU60	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU61	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU62	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU63	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU64	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
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CMU66	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU67	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU68	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU69	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU70	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU71	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU72	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU73	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU74	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
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CMU77	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
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CMU80	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU81	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU82	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU83	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU84	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU85	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU86	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU87	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU88	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU89	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU90	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU91	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU92	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU93	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU94	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU95	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU96	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU97	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU98	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU99	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH
CMU100	CMU 1/2" VENEER, CMU BLOCK, NATURAL, WITH NATURAL, SCOTCH

GENERAL EXTERIOR ELEVATION NOTES

1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER SPECIFIC LINE DOCUMENTS.
2. ALL CONDUIT RUNS & SERVICE LINES SHALL BE GROUPOED TOGETHER IN A NEAT & ORDERLY MANNER AND SHOULD PENETRATE THE WALL AT THE LOWEST POINT AND ARE NOT TO CROSS OVER THE TOP OF PARAPETS. ALL OTHER CONDUITS & SERVICES FOR EXTERIOR VEHICLES & EQUIPMENT SHALL BE FULLY CONCEALED WITHIN THE BUILDING CONSTRUCTION.
3. ALL HORIZONTAL TRANSITIONS BETWEEN DIFFERENT MATERIALS SHALL BE FINISHED CONTINUOUSLY FROM UP THROUGH UPPER MATERIAL AND EXTEND OVER TO EXTERIOR FACE OF LOWER MATERIAL WITH DRIP EDGE.
4. ANY RETURN OR BAND ELEVATIONS NOT SHOWN SHALL BE SIMILAR IN MATERIAL AND MAKEUP TO ADJACENT CONDITIONS OR OTHER SIMILAR CONDITIONS.
5. NO EXPOSED MASONRY/MATERIAL CODES. MASONRY RETURNING AT TOP OF WALL SHALL BE FINISHED WITH FINISH MATERIAL. FINISH MATERIAL SHALL PROVIDE CLOSURE AT THE INTERFACE OF MASONRY FOR AN EFFECTIVELY SEALED JOINT.
6. MATERIALS ABOVE CANOPIES & AWNINGS TO BE CONTINUED DOWN BEHIND THEM UNLESS NOTED OTHERWISE.
7. ANY EXPOSED ROOFING MEMBRANE VISIBLE FROM A PUBLIC RIGHT OF WAY MUST MATCH THE PRIMARY WALL MATERIAL COLOR.
8. INSTALL VERTICAL EXPANSION JOINTS PER STRUCTURAL OR MANUFACTURER'S RECOMMENDATIONS AND AT THE HEAD OF DOOR JAMBSPANS OR AT CONTIGUOUS CORNERS OR WHERE REQUIRED PER STANDARD MATERIAL PRACTICE TO REDUCE STRESS CRACKING. JOINT CONTROL JOINTS PERMANENT JOINT AT WALL STRUCTURAL BEARING LOCATIONS. VERIFY JOINTS WITH ARCHITECT.
9. GASKET & SEAL ALL TRANSITION-CONTROL-EXPANSION AT ALL EXTERIOR MATERIALS.
10. PROVIDE CAST REGLETS JUST IN FROM THE OUTER EDGE ON UNDERSIDE OF FACE MATERIAL ON DRIP EDGE AT ALL OVERHANGS & SOFFITS.
11. ISOLATE AND SEAL TO ENSURE ADEQUATE AIR INTRUSION BARRIERS AROUND ALL EXTERIOR PENETRATIONS TO PREVENT WATER AND SPILLABLE PIPE FLECKING.
12. WHERE EXTERIOR FINISH ADJACENT TO BUILDING, TOP OF FINISH TO BE 1/2" LOWER THAN INTERIOR FINISH FLOOR. SLOPING CONTINUOUSLY AWAY FROM BUILDING A MINIMUM OF 1/4" PER FOOT FROM DOOR 1/8" MINIMUM SLOPE TO PUBLIC WALK, REFER TO CURB.
13. WHERE NO EXTERIOR FINISH ADJACENT TO BUILDING, TOP OF FINISHED LANDSCAPE A MINIMUM OF 1/4" BELOW INTERIOR FINISH FLOOR AND 2" BELOW EXTERIOR WALL FLASHING/EDEGE. SLOPING CONTINUOUSLY AWAY FROM BUILDING A MINIMUM OF 1/4" PER FOOT FROM DOOR 1/8" MINIMUM SLOPE TO PUBLIC WALK, REFER TO CURB.
14. ADDRESS SHALL BE PROVIDED ON STREET SIDE OF BUILDING AND ON MONUMENT EDGE. WHERE A MINIMUM 3" HIGH CONTRASTING COLOR NUMBERS WITH MINIMUM 1/2" STROKE.
15. IF ANY SPACER ON SIGN BASE IS SHOWN IT IS FOR REFERENCE ONLY. FINAL DESIGN AND ALLOWANCES SHALL BE DETERMINED WITH APPROVED SIGNAGE PERMITS.

MISC EXTERIOR MATERIAL FINISHES

- TYPICAL U.O.A:
- EXPOSED CONCRETE: BRUSHED SANDFINISH & SUBSEQUENT WITH CEMENT SLURRY
 - EXPOSED UNITS: GRANULATED & PAINTED TO MATCH ADJACENT MATERIAL ABOVE.
 - METAL BALCONIES & RAILINGS: GRANULATED & PAINTED WHITE.
 - FLASHING & SOFFITS: METAL, PRE-FINISHED TO MATCH ADJACENT ROOF COLOR.
 - GASKETS & SEALANTS: COLOR MATCHED TO ADJACENT MATERIAL.
 - HOLDING METAL SHIELDS & FRAMES: PAINTED TO MATCH ADJACENT MATERIAL.
 - OVERHEAD PANEL DOORS: METAL, PRE-FINISHED TO MATCH ADJACENT MATERIAL.
 - SIDEWAYS: METAL, PRE-FINISHED TO MATCH GRAY.
 - LIGHT FIXTURES: METAL, PRE-FINISHED HOUSING TO MATCH ASL, GROUND FRAME/SLIP.
 - MECHANICAL EQUIPMENT: SCREENED BY PARAPETS.
 - EXTERIOR UTILITY EQUIPMENT: IF ALLOWED, PAINTED TO MATCH ADJACENT MATERIAL.
 - TRIM SIZE, TYP. U.O.A:
 - WINDOW & DOORS 5'-0"
 - VERTICAL 5'-0"
 - HORIZONTAL 8'-0"

MATERIAL CALCULATIONS (COPY OF DEE MOORE)		SIDE ELEVATION (WEST END) 2,888.8 SF TOTAL		SIDE ELEVATION (EAST END) 2,888.8 SF TOTAL			
W/O PENETRATIONS:	1,884.8 SF	W/O PENETRATIONS:	1,884.8 SF	W/O PENETRATIONS:	1,884.8 SF		
GLASS =	375.22 SF	19.9%	0.0%	GLASS =	375.22 SF	13.0%	0.0%
BRICK =	852.33 SF	29.5%	38.5%	BRICK =	852.33 SF	29.5%	38.5%
FIBER CEMENT =	203.34 SF	7.0%	13.0%	FIBER CEMENT =	203.34 SF	7.0%	13.0%
METAL PANELS =	372.11 SF	12.9%	34.0%	METAL PANELS =	372.11 SF	12.9%	34.0%
CMU =	249.18 SF	8.7%	15.5%	CMU =	249.18 SF	8.7%	15.5%
SW 13TH STREET (STREET FACING) 4,231.42 SF TOTAL		W/O PENETRATIONS: 4,231.42 SF		MURPHY STREET (STREET FACING) 4,841.61 SF TOTAL		W/O PENETRATIONS: 4,841.61 SF	
GLASS =	1,474.83 SF	34.9%	0.0%	GLASS =	2,284.71 SF	47.2%	0.0%
BRICK =	1,508.54 SF	35.6%	33.6%	BRICK =	2,920.59 SF	60.3%	28.0%
FIBER CEMENT =	1,115.09 SF	26.3%	25.0%	FIBER CEMENT =	1,810.82 SF	37.4%	35.0%
METAL PANELS =	1,079.15 SF	25.3%	42.8%	METAL PANELS =	1,252.31 SF	25.9%	45.1%
CMU =	84.23 SF	2.0%	0.2%	CMU =	128.43 SF	2.7%	0.3%
INTERNAL ELEVATION (EAST) 2,331.19 SF TOTAL		W/O PENETRATIONS: 2,331.19 SF		INTERNAL ELEVATION (SOUTH) 4,837.87 SF TOTAL		W/O PENETRATIONS: 4,837.87 SF	
GLASS =	1,168.79 SF	50.1%	0.0%	GLASS =	2,265.78 SF	46.8%	0.0%
BRICK =	8.51 SF	0.4%	0.0%	BRICK =	8.51 SF	0.2%	0.0%
FIBER CEMENT =	581.25 SF	25.0%	27.0%	FIBER CEMENT =	1,845.10 SF	38.1%	43.0%
METAL PANELS =	1,157.88 SF	49.6%	34.0%	METAL PANELS =	1,848.28 SF	38.2%	37.0%
CMU =	424.58 SF	18.2%	18.0%	CMU =	807.45 SF	16.7%	17.0%

WHICH RECESS BY MATERIAL:

BRICK:	5'-0"
CMU VENEER:	5'-0"
FIBER CEMENT PANEL:	5'-0"
FIBER CEMENT LAP:	3/4"
METAL PANEL:	5'-0"

SIMONSON
ARCHITECTURE & INTERIOR DESIGN, LLC
1112 PINE STREET, SUITE 100
DES MOINES, IA 50319
515.281.1111
www.simonsonarch.com

Hubbell
HUBBELL REALTY COMPANY

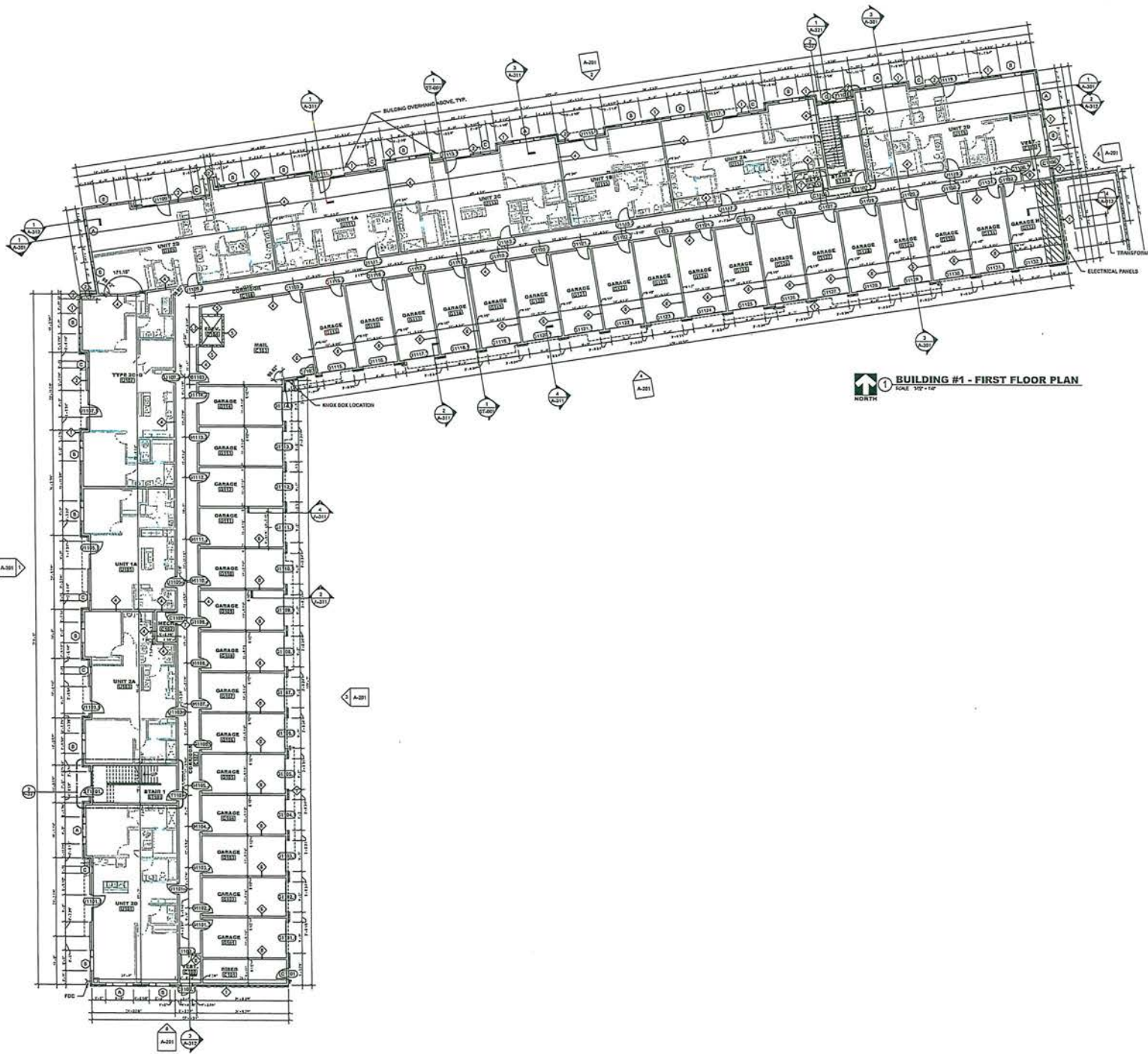
TELUS CONDOS
DES MOINES, IA

Job No. 21162
Scale: 1/8" = 1'-0"
Date: 11/11/2020

FIG. 50
SP

BLOG #2 - EXTERIOR ELEVATIONS
COLORED

M-202



BUILDING #1 - FIRST FLOOR PLAN
SCALE: 1/8" = 1'-0"

- GENERAL FLOOR PLAN NOTES**
- REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER SHOPPING DOCUMENTS.
 - ALL SITE INFO, DIMENSIONS, FINISHES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE TO BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO OPI, THE SUPERVISION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS AND CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
 - DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
 - ALL CONCRETE, PIPE, WIRING, ROUGH-IN, ETC. SHOULD BE HELD AS TIGHT AS POSSIBLE TO THE ROOF OR FLOOR DECK OR SHOULD BE KEPT WITHIN INTERIOR WALL CHASES. NOTING SHALL BE BELOW THE BOTTOM EDGE OF MAIN BUILDING FRAME. NOTING SHALL BE EXPOSED IN FINISHED SPACES. NOTING SHALL PREVENT A SMOOTH CONTIGUOUS FINISH WALL OR CEILING AS NOTED ON THE DRAWING AND NO FINISHED PAN IN EXTERIOR WALLS UNLESS PRIOR APPROVAL IS GRANTED BY THE ARCHITECT OR OWNER. PLACE/IDENT COORDINATION BETWEEN TRADES IS REQUIRED.
 - INSTALL VERTICAL EXPANSION JOINT W/STAIR AT COLUMN/DOOR LINES AND AT THE HEAD OF DOOR JAMB/OPENING OR AS OTHERWISE NOTED ON WALLS. REQUIRED PER STRUCTURAL MATERIAL PRACTICE TO REDUCE STRESS/CRAACKS. NO CONTROL JOINT/EXPANSION JOINT IN LOAD BEARING WALL. STAY AWAY 2'-0" MIN.
 - CHECK & SEAL ALL CONTROL/EXPANSION, SAWTOOTH JOINTS AT ALL INTERIOR & EXTERIOR MASONRY & CONCRETE.
 - PROVIDE LOWER LEVEL AND/OR UNDERFLOOR DRAINAGE SYSTEM TO REDUCE HYDROSTATIC UPLIFT AND PROTECT AGAINST MOISTURE SEEPAGE. PROVIDE FOUNDATION WALL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURES AND PROTECT AGAINST MOISTURE SEEPAGE. BOTH SYSTEMS SHALL OUPLET TO APPROPRIATE REEF.
 - PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT PONDING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

BUILDING #1 UNIT MIX:

FIRST FLOOR

UNIT 1A	2
UNIT 1B	1
UNIT 1C	1
UNIT 1D	2
UNIT 1E	1
UNIT 1F	1
UNIT 1G	1
UNIT 1H	1
UNIT 1I	1
UNIT 1J	1
UNIT 1K	1
UNIT 1L	1
UNIT 1M	1
UNIT 1N	1
UNIT 1O	1
UNIT 1P	1
UNIT 1Q	1
UNIT 1R	1
UNIT 1S	1
UNIT 1T	1
UNIT 1U	1
UNIT 1V	1
UNIT 1W	1
UNIT 1X	1
UNIT 1Y	1
UNIT 1Z	1
TOTAL	18 (11 REG GARAGES, 1 ADA GARAGE)

SECOND FLOOR

UNIT 2A	4
UNIT 2B	4
UNIT 2C	4
UNIT 2D	4
UNIT 2E	4
UNIT 2F	4
UNIT 2G	4
UNIT 2H	4
UNIT 2I	4
UNIT 2J	4
UNIT 2K	4
UNIT 2L	4
UNIT 2M	4
UNIT 2N	4
UNIT 2O	4
UNIT 2P	4
UNIT 2Q	4
UNIT 2R	4
UNIT 2S	4
UNIT 2T	4
UNIT 2U	4
UNIT 2V	4
UNIT 2W	4
UNIT 2X	4
UNIT 2Y	4
UNIT 2Z	4
TOTAL	112

THIRD FLOOR

UNIT 3A	4
UNIT 3B	4
UNIT 3C	4
UNIT 3D	4
UNIT 3E	4
UNIT 3F	4
UNIT 3G	4
UNIT 3H	4
UNIT 3I	4
UNIT 3J	4
UNIT 3K	4
UNIT 3L	4
UNIT 3M	4
UNIT 3N	4
UNIT 3O	4
UNIT 3P	4
UNIT 3Q	4
UNIT 3R	4
UNIT 3S	4
UNIT 3T	4
UNIT 3U	4
UNIT 3V	4
UNIT 3W	4
UNIT 3X	4
UNIT 3Y	4
UNIT 3Z	4
TOTAL	112

TOTALS

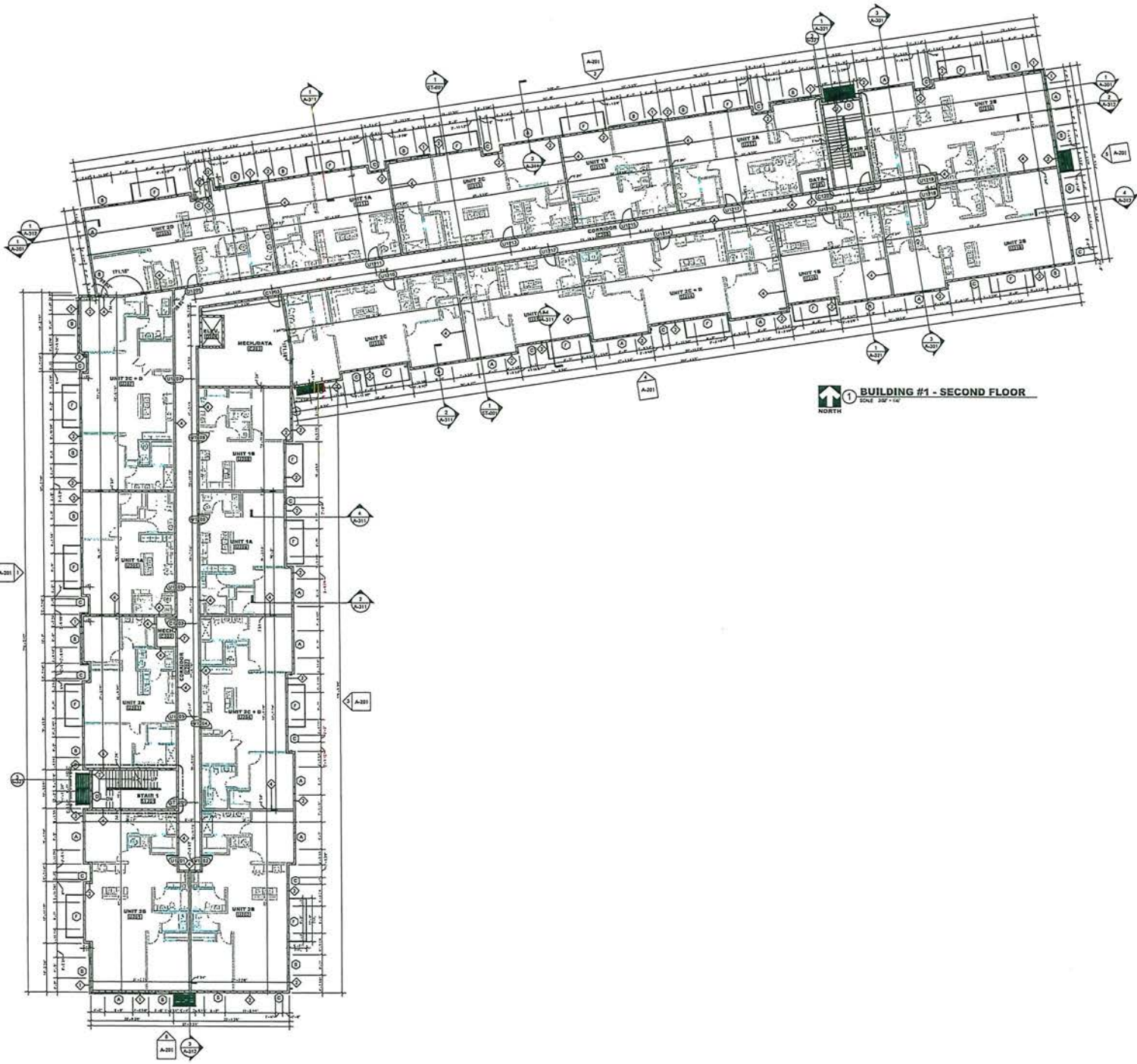
UNIT 1A	12	17 1-BEDROOMS - 51K
UNIT 1B	1	
UNIT 1C	1	
UNIT 1D	2	
UNIT 1E	1	
UNIT 1F	1	
UNIT 1G	1	
UNIT 1H	1	
UNIT 1I	1	
UNIT 1J	1	
UNIT 1K	1	
UNIT 1L	1	
UNIT 1M	1	
UNIT 1N	1	
UNIT 1O	1	
UNIT 1P	1	
UNIT 1Q	1	
UNIT 1R	1	
UNIT 1S	1	
UNIT 1T	1	
UNIT 1U	1	
UNIT 1V	1	
UNIT 1W	1	
UNIT 1X	1	
UNIT 1Y	1	
UNIT 1Z	1	
TOTAL	48	(24 GARAGES)



TELUS CONDOS, IA
DES MOINES, IA

DATE	2/1/2024
BY	J. HUBBELL
CHECKED	J. HUBBELL
SCALE	AS SHOWN
PROJECT	TELUS CONDOS, IA
DATE	2/1/2024
BY	J. HUBBELL
CHECKED	J. HUBBELL
SCALE	AS SHOWN
PROJECT	TELUS CONDOS, IA

DATE: 2/1/24
SCALE: 1/8" = 1'-0"
PROJECT: BUILDING #1 - FIRST FLOOR PLAN
OWNER: TELUS CONDOS, IA



GENERAL FLOOR PLAN NOTES

1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINE DOCUMENTS.
2. ALL SITE INFO, FURNITURE, FIXTURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE TO BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO CIVIL, FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS AND CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
4. ALL CONDUITS, PIPES, WIRING, ROUNDRUN, ETC. SHOULD BE HELD AS TIGHT AS POSSIBLE TO THE ROOF OR FLOOR DECK OR SHOULD BE KEPT WITHIN INTERIOR WALL CHASERS. NOTHING SHALL BE HELD TO THE BOTTOM EDGE OF HANG BELOWS FRAMES. NOTHING SHALL BE EXPOSED BY FINISHED SPACES. NOTHING SHALL PENETRATE A FINISH CONTIGUOUS FINISHED WALL OR CEILING AS NOTED ON THE DRAWINGS AND NO PLUMBING SHALL BE EXTERIOR WALLS UNLESS PRIOR APPROVAL IS GRANTED BY THE ARCHITECT OR OWNER. PLACE/MENT COORDINATION BETWEEN TRADES IS REQUIRED.
5. INITIAL VERTICAL EXPANSION JOINT SIGNAL AT COLUMN/GRID LINES AND AT THE HEAD OF DOOR JAMB/DOORFRAMES OR AS OTHERWISE NOTED ON OTHERS REQUIRED FOR STRAINED MATERIAL PRACTICES TO REDUCE STRESS CRACKING. NO CONTROL JOINT/EXPANSION JOINT IN LOAD BEARING AREAS. STAY AWAY 2" MIN.
6. CALLS & SEAL ALL CONTROL-EXPANSION, SAWCUT JOINTS AT ALL INTERIOR & EXTERIOR MASONRY & CONCRETE.
7. PROVIDE LOWER LEVEL AND/OR UNDER FLOOR DRAINAGE SYSTEM TO REDUCE HYDROSTATIC UPLIFT AND PROTECT AGAINST MOISTURE SEEPAGE. FINISH FLOORING SHALL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURE AND PROTECT AGAINST MOISTURE SEEPAGE. BOTH SYSTEMS SHALL OUTLET TO APPROPRIATE TRAPS.
8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT POONDING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

BUILDING #1 UNIT MIX:

FIRST FLOOR	
UNIT 1A	1
UNIT 1B	1
UNIT 1C	1
UNIT 1D	1
UNIT 1E-D	1
TOTAL	5 (2) RED GARAGES, 1 ADA GARAGES

SECOND FLOOR	
UNIT 1A	1
UNIT 1B	1
UNIT 1C	1
UNIT 1D	1
UNIT 1E-D	1
TOTAL	5

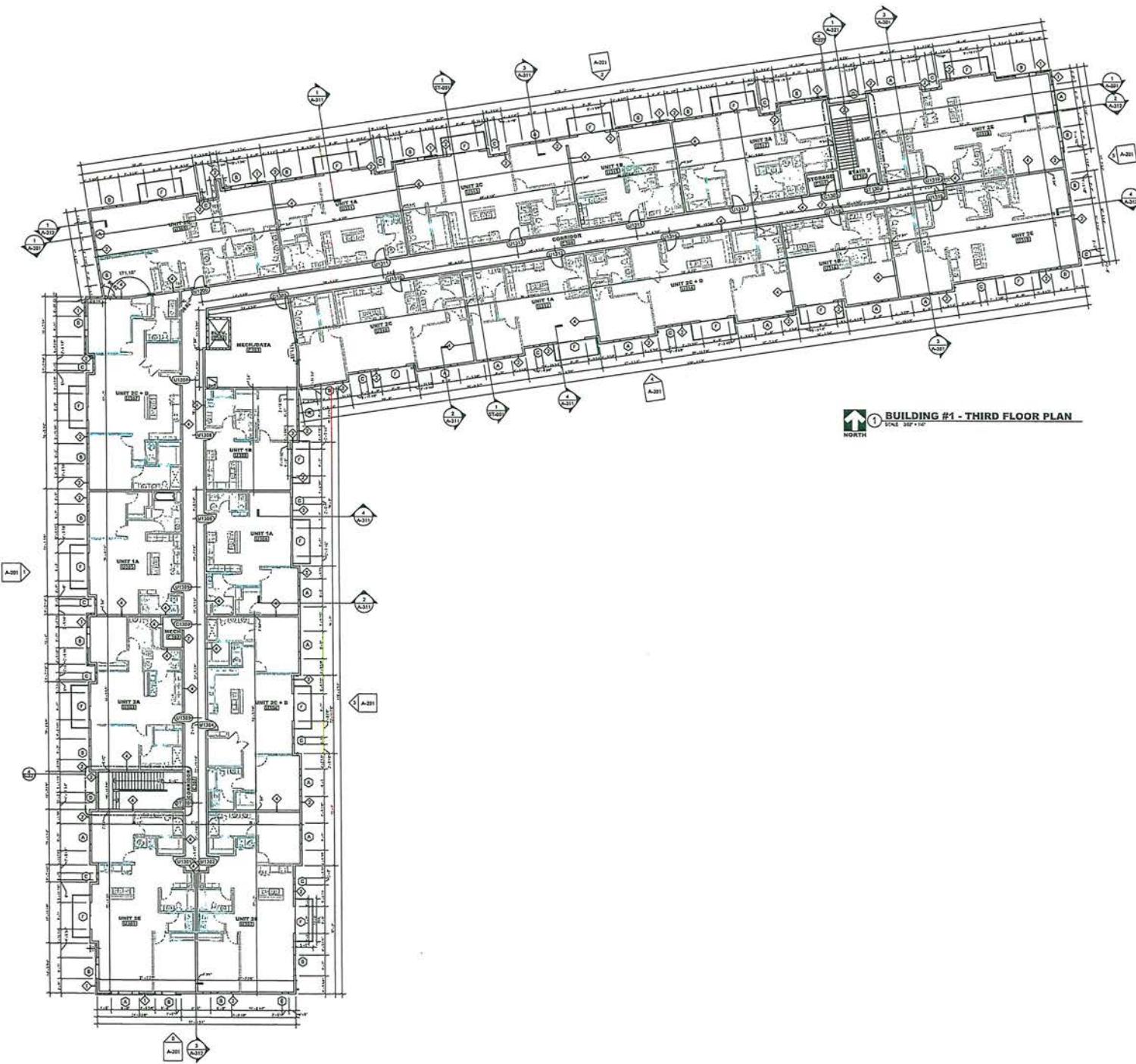
THIRD FLOOR	
UNIT 1A	1
UNIT 1B	1
UNIT 1C	1
UNIT 1D	1
UNIT 1E-D	1
TOTAL	5

TOTALS	
UNIT 1A	3
UNIT 1B	3
UNIT 1C	3
UNIT 1D	3
UNIT 1E-D	3
TOTAL	15 (2) RED ROOMS + 20% (2) GARAGES



TELUS CONDOS, IA
DES MOINES, IA

DATE	DESCRIPTION	BY
10/25/2024	ISSUE FOR PERMITS	MM
10/20/2024	ISSUE FOR PERMITS	MM
10/09/2024	ISSUE FOR PERMITS	MM



BUILDING #1 - THIRD FLOOR PLAN
 NORTH
 SCALE 3/8" = 1'-0"

- GENERAL FLOOR PLAN NOTES**
1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER SUPPLIER DOCUMENTS.
 2. ALL SITE AND FURNITURE REVISED AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO ME, FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS AND CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
 3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
 4. ALL CORNERS, PIPES, WINDOWS, ROUGH-IN, ETC. SHOULD BE HELD AS TIGHT AS POSSIBLE TO THE ROOF OR FLOOR DECK OR SHOULD BE KEPT WITHIN INTERIOR WALL CORNERS. WORKING SHALL BE SECURED TO THE BOTTOM EDGE OF AN EXISTING FRAMED, ACTING SHALL BE EXPOSED IN FINISHED SPACES. NOTHING SHALL PREVENT A SMOOTH CONTINUOUS FINISH WALL OR CEILING AS NOTED ON THE DRAWINGS AND NO PLUMBING RUN IN EXTERIOR WALLS UNLESS PRIOR APPROVAL IS GRANTED BY THE ARCHITECT OR OWNER. PLACE/NOT COORDINATION BETWEEN TRADES IS REQUIRED.
 5. PERFORM VERTICAL EXPANSION JOINT MINIMAL AT COLUMNED UNITS AND AT THE HEAD OF SOFFIT ANCHORINGS OR AS OTHERWISE NOTED ON DRAWING. REQUIRED FOR FINISHED MATERIAL PRACTICES TO REDUCE STRESS CRACKING. DO CONTROL JOINT/EXPANSION JOINT IN LOAD BEARING AREAS, STAY AWAY 2'-0" MIN.
 6. CABLE & SEAL ALL CONTROL/EXPANSION, SAWCUT JOINTS AT ALL INTERIOR & EXTERIOR MASONRY & CONCRETE.
 7. PROVIDE LOWER LEVEL AND/OR UNDER FLOOR DRAINAGE SYSTEM TO REDUCE HYDROSTATIC UPLIFT AND PROTECT AGAINST MOISTURE SEEPAGE. PROVIDE FOUNDATION WALL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURES AND PROTECT AGAINST MOISTURE SEEPAGE. BOTH SYSTEMS SHALL OUTLET TO APPROPRIATE RELIEF.
 8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT POONDING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

BUILDING #1 UNIT MIX:

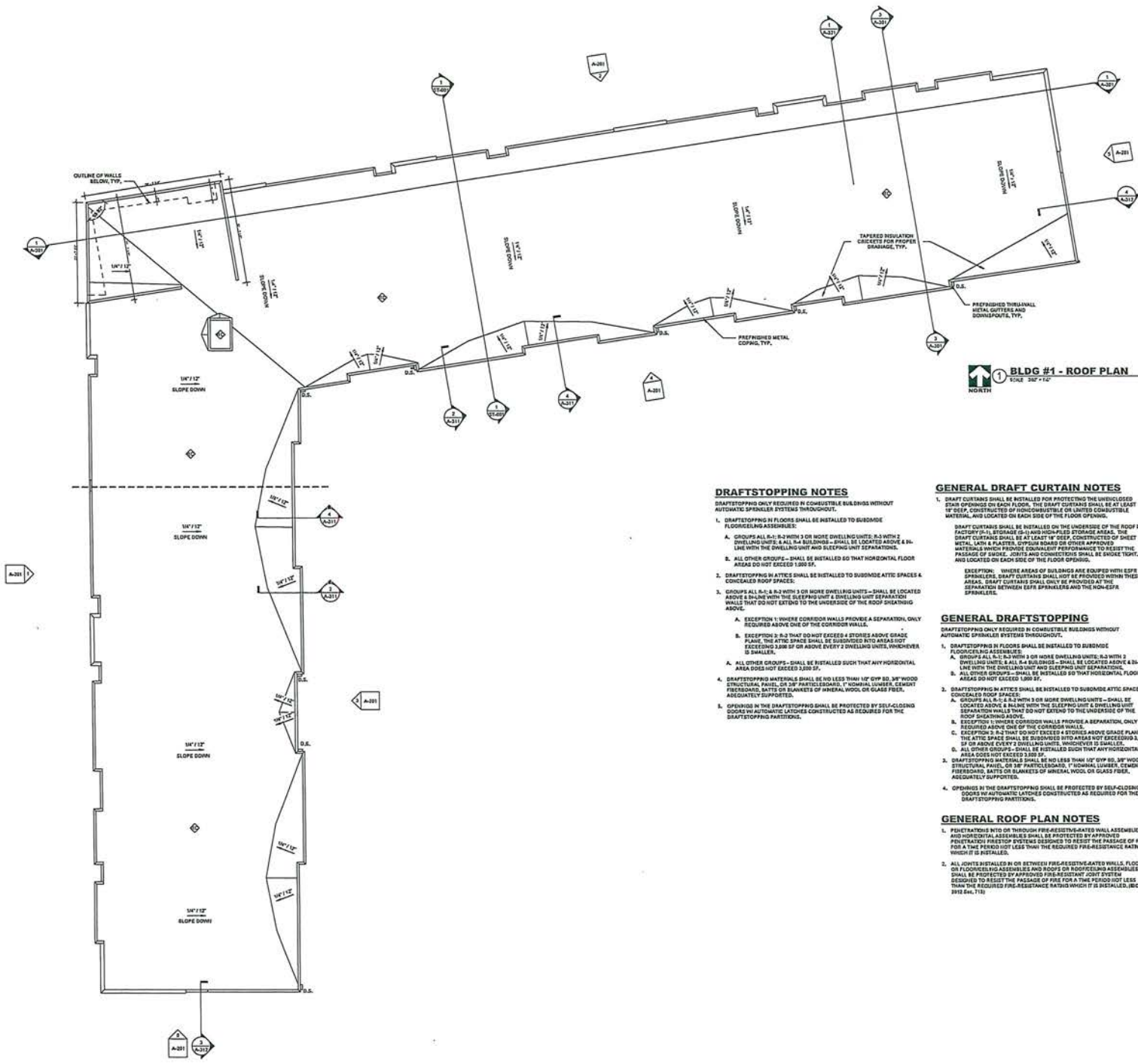
FIRST FLOOR	
UNIT 1A	1
UNIT 1B	1
UNIT 1C	1
UNIT 1D	1
UNIT 1E	1
UNIT 1F	1
UNIT 1G	1
UNIT 1H	1
UNIT 1I	1
UNIT 1J	1
UNIT 1K	1
UNIT 1L	1
UNIT 1M	1
UNIT 1N	1
UNIT 1O	1
UNIT 1P	1
UNIT 1Q	1
UNIT 1R	1
UNIT 1S	1
UNIT 1T	1
UNIT 1U	1
UNIT 1V	1
UNIT 1W	1
UNIT 1X	1
UNIT 1Y	1
UNIT 1Z	1
TOTAL	20 (21 REG GARAGES, 1 ADA GARAGES)

SECOND FLOOR	
UNIT 2A	1
UNIT 2B	1
UNIT 2C	1
UNIT 2D	1
UNIT 2E	1
UNIT 2F	1
UNIT 2G	1
UNIT 2H	1
UNIT 2I	1
UNIT 2J	1
UNIT 2K	1
UNIT 2L	1
UNIT 2M	1
UNIT 2N	1
UNIT 2O	1
UNIT 2P	1
UNIT 2Q	1
UNIT 2R	1
UNIT 2S	1
UNIT 2T	1
UNIT 2U	1
UNIT 2V	1
UNIT 2W	1
UNIT 2X	1
UNIT 2Y	1
UNIT 2Z	1
TOTAL	20 (21 REG GARAGES)

THIRD FLOOR	
UNIT 3A	1
UNIT 3B	1
UNIT 3C	1
UNIT 3D	1
UNIT 3E	1
UNIT 3F	1
UNIT 3G	1
UNIT 3H	1
UNIT 3I	1
UNIT 3J	1
UNIT 3K	1
UNIT 3L	1
UNIT 3M	1
UNIT 3N	1
UNIT 3O	1
UNIT 3P	1
UNIT 3Q	1
UNIT 3R	1
UNIT 3S	1
UNIT 3T	1
UNIT 3U	1
UNIT 3V	1
UNIT 3W	1
UNIT 3X	1
UNIT 3Y	1
UNIT 3Z	1
TOTAL	20 (17 MEERDOOMS + 3IN)

TOTALS	
UNIT 1A	1
UNIT 1B	1
UNIT 1C	1
UNIT 1D	1
UNIT 1E	1
UNIT 1F	1
UNIT 1G	1
UNIT 1H	1
UNIT 1I	1
UNIT 1J	1
UNIT 1K	1
UNIT 1L	1
UNIT 1M	1
UNIT 1N	1
UNIT 1O	1
UNIT 1P	1
UNIT 1Q	1
UNIT 1R	1
UNIT 1S	1
UNIT 1T	1
UNIT 1U	1
UNIT 1V	1
UNIT 1W	1
UNIT 1X	1
UNIT 1Y	1
UNIT 1Z	1
UNIT 2A	1
UNIT 2B	1
UNIT 2C	1
UNIT 2D	1
UNIT 2E	1
UNIT 2F	1
UNIT 2G	1
UNIT 2H	1
UNIT 2I	1
UNIT 2J	1
UNIT 2K	1
UNIT 2L	1
UNIT 2M	1
UNIT 2N	1
UNIT 2O	1
UNIT 2P	1
UNIT 2Q	1
UNIT 2R	1
UNIT 2S	1
UNIT 2T	1
UNIT 2U	1
UNIT 2V	1
UNIT 2W	1
UNIT 2X	1
UNIT 2Y	1
UNIT 2Z	1
UNIT 3A	1
UNIT 3B	1
UNIT 3C	1
UNIT 3D	1
UNIT 3E	1
UNIT 3F	1
UNIT 3G	1
UNIT 3H	1
UNIT 3I	1
UNIT 3J	1
UNIT 3K	1
UNIT 3L	1
UNIT 3M	1
UNIT 3N	1
UNIT 3O	1
UNIT 3P	1
UNIT 3Q	1
UNIT 3R	1
UNIT 3S	1
UNIT 3T	1
UNIT 3U	1
UNIT 3V	1
UNIT 3W	1
UNIT 3X	1
UNIT 3Y	1
UNIT 3Z	1
TOTAL	60 (54 REG GARAGES)

NO.	REVISION	DATE
1	ISSUED FOR CONSTRUCTION	08/28/2017
2	REVISED FOR CONSTRUCTION	11/08/2017



DRAFTSTOPPING NOTES

- DRAFTSTOPPING ONLY REQUIRED IN COMBUSTIBLE BUILDINGS WITHOUT AUTOMATIC SPRINKLER SYSTEMS THROUGHOUT.
- DRAFTSTOPPING IN FLOORS SHALL BE INSTALLED TO SUBGRADE FLOORCEILING ASSEMBLIES:
 - GROUPS ALL R-1, R-2 WITH 3 OR MORE DWELLING UNITS, R-3 WITH 2 DWELLING UNITS, & ALL R-4 BUILDINGS - SHALL BE LOCATED ABOVE & BELOW WITH THE DWELLING UNIT AND SLEEPING UNIT SEPARATIONS.
 - ALL OTHER GROUPS - SHALL BE INSTALLED SO THAT HORIZONTAL FLOOR AREAS DO NOT EXCEED 1200 SF.
 - DRAFTSTOPPING IN ATTICS SHALL BE INSTALLED TO SUBGRADE ATTIC SPACES & CONCEALED ROOF SPACES.
 - GROUPS ALL R-1, R-2 WITH 3 OR MORE DWELLING UNITS - SHALL BE LOCATED ABOVE & BELOW WITH THE SLEEPING UNIT & DWELLING UNIT SEPARATIONS WALLS THAT DO NOT EXTEND TO THE UNDERSIDE OF THE ROOF DRAINING ABOVE.
 - EXCEPTION 1 - WHERE CORRIDOR WALLS PROVIDE A SEPARATION, ONLY REQUIRED ABOVE ONE OF THE CORRIDOR WALLS.
 - EXCEPTION 2 - R-2 THAT DO NOT EXCEED 4 STORIES ABOVE GRADE PLANE, THE ATTIC SPACE SHALL BE SUBGRADED INTO AREAS NOT EXCEEDING 1200 SF OR ABOVE EVERY 2 DWELLING UNITS, WHICHEVER IS SMALLER.
 - ALL OTHER GROUPS - SHALL BE INSTALLED SUCH THAT ANY HORIZONTAL AREA DOES NOT EXCEED 1200 SF.
 - DRAFTSTOPPING MATERIALS SHALL BE NO LESS THAN 1/2" OVP 30, 3/4" WOOD STRUCTURAL PANEL, OR 2" PARTICLEBOARD, 1" NOMINAL LUMBER, CEMENT FIBERBOARD, Batts OR BLANKETS OF MINERAL WOOL OR GLASS FIBER, AS AVAILABLELY SUPPORTED.
 - OPENINGS IN THE DRAFTSTOPPING SHALL BE PROTECTED BY SELF-CLOSING DOORS OR AUTOMATIC LATCHES CONSTRUCTED AS REQUIRED FOR THE DRAFTSTOPPING MATERIALS.

GENERAL DRAFT CURTAIN NOTES

- DRAFT CURTAINS SHALL BE INSTALLED FOR PROTECTING THE UNDESIRABLE STAIR OPENINGS ON EACH FLOOR. THE DRAFT CURTAINS SHALL BE AT LEAST 18" DEEP, CONSTRUCTED OF NON-COMBUSTIBLE OR LIMITED COMBUSTIBLE MATERIAL, AND LOCATED ON EACH SIDE OF THE FLOOR OPENING.

EXCEPTION: WHERE AREAS OF BUILDINGS ARE EQUIPPED WITH ESFR SPRINKLERS, DRAFT CURTAINS SHALL NOT BE PROVIDED WITHIN THESE AREAS. DRAFT CURTAINS SHALL ONLY BE PROVIDED AT THE SEPARATION BETWEEN ESFR SPRINKLER AREAS AND THE NON-ESFR SPRINKLER.

GENERAL DRAFTSTOPPING

- DRAFTSTOPPING ONLY REQUIRED IN COMBUSTIBLE BUILDINGS WITHOUT AUTOMATIC SPRINKLER SYSTEMS THROUGHOUT.
- DRAFTSTOPPING IN FLOORS SHALL BE INSTALLED TO SUBGRADE FLOORCEILING ASSEMBLIES:
 - GROUP ALL R-1, R-2 WITH 3 OR MORE DWELLING UNITS, R-3 WITH 2 DWELLING UNITS, & ALL R-4 BUILDINGS - SHALL BE LOCATED ABOVE & BELOW WITH THE DWELLING UNIT AND SLEEPING UNIT SEPARATIONS.
 - ALL OTHER GROUPS - SHALL BE INSTALLED SO THAT HORIZONTAL FLOOR AREAS DO NOT EXCEED 1200 SF.
 - DRAFTSTOPPING IN ATTICS SHALL BE INSTALLED TO SUBGRADE ATTIC SPACES & CONCEALED ROOF SPACES.
 - GROUPS ALL R-1, R-2 WITH 3 OR MORE DWELLING UNITS - SHALL BE LOCATED ABOVE & BELOW WITH THE SLEEPING UNIT & DWELLING UNIT SEPARATION WALLS THAT DO NOT EXTEND TO THE UNDERSIDE OF THE ROOF DRAINING ABOVE.
 - EXCEPTION 1 - WHERE CORRIDOR WALLS PROVIDE A SEPARATION, ONLY REQUIRED ABOVE ONE OF THE CORRIDOR WALLS.
 - EXCEPTION 2 - R-2 THAT DO NOT EXCEED 4 STORIES ABOVE GRADE PLANE, THE ATTIC SPACE SHALL BE SUBGRADED INTO AREAS NOT EXCEEDING 1200 SF OR ABOVE EVERY 2 DWELLING UNITS, WHICHEVER IS SMALLER.
 - ALL OTHER GROUPS - SHALL BE INSTALLED SUCH THAT ANY HORIZONTAL AREA DOES NOT EXCEED 1200 SF.
 - DRAFTSTOPPING MATERIALS SHALL BE NO LESS THAN 1/2" OVP 30, 3/4" WOOD STRUCTURAL PANEL, OR 2" PARTICLEBOARD, 1" NOMINAL LUMBER, CEMENT FIBERBOARD, Batts OR BLANKETS OF MINERAL WOOL OR GLASS FIBER, AS AVAILABLELY SUPPORTED.
 - OPENINGS IN THE DRAFTSTOPPING SHALL BE PROTECTED BY SELF-CLOSING DOORS OR AUTOMATIC LATCHES CONSTRUCTED AS REQUIRED FOR THE DRAFTSTOPPING MATERIALS.

GENERAL ROOF PLAN NOTES

- PENETRATIONS INTO OR THROUGH FIRE-RATED/RESISTED WALL ASSEMBLIES AND HORIZONTAL ASSEMBLIES SHALL BE PROTECTED BY APPROVED PENETRATION FIRESTOP SYSTEMS DESIGNED TO RESTRICT THE PASSAGE OF FIRE FOR A PERIOD NOT LESS THAN THE REQUIRED FIRE-RESISTANCE RATING WHICH IS INSTALLED.
- ALL JOINTS INSTALLED IN OR BETWEEN FIRE-RATED/WALLS, FLOORS OR FLOORCEILING ASSEMBLIES AND ROOF OR ROOFCEILING ASSEMBLIES SHALL BE PROTECTED BY APPROVED FIRE-RESISTANT JOINT SYSTEMS DESIGNED TO RESTRICT THE PASSAGE OF FIRE FOR A PERIOD NOT LESS THAN THE REQUIRED FIRE-RESISTANCE RATING WHICH IS INSTALLED. (IBC 2012 E&I, 711)

GENERAL ROOF PLAN NOTES

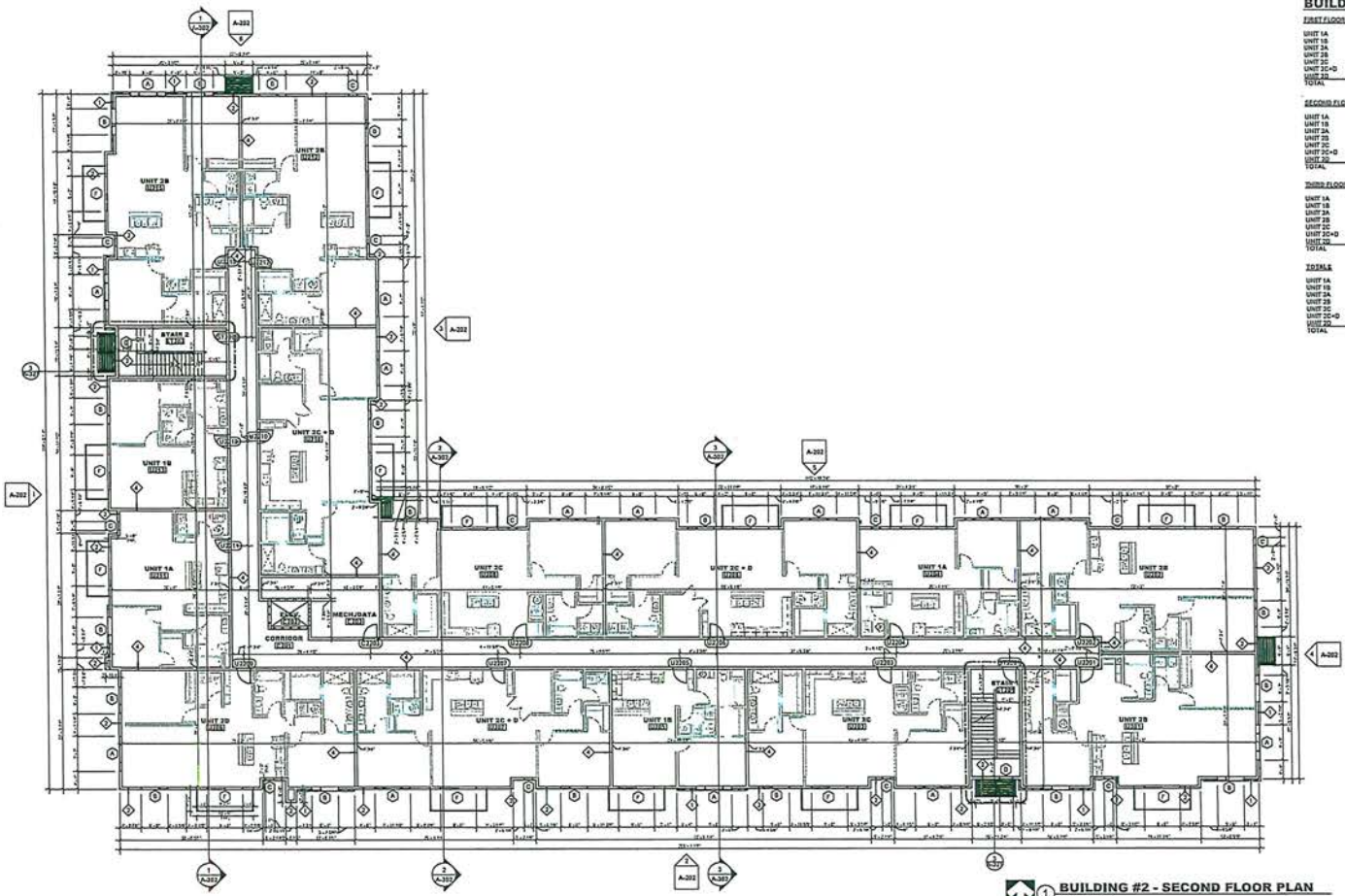
- REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINE DOCUMENTS.
- ALL FITTINGS AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO E&P, FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS FOR COORDINATION OF ALL OTHER SPECIFIC DESIGN INFORMATION & REQUIREMENTS. REFER TO STRUCTURAL DRAWINGS FOR ROOF OPENING STRUCTURAL FINISHING.
- ALL ROOF PENETRATIONS FOR MECH ROOF VENTS AND SHALL ITEMS MAY NOT BE SPECIFIED ON THE ROOF PLAN. HOWEVER, ALL ROOF FINISHING SHALL BE PROVIDED BY THE ROOFING SUBCONTRACTOR SO AS TO OBTAIN FULL ROOFING WARRANTIES. REFER TO ALL OTHER CONSTRUCTION DOCUMENTS FOR ADDITIONAL ROOF PENETRATIONS REQUIRING ROOF FINISHING WORK.
- IT SHOULD BE THE RESPONSIBILITY OF THE ROOF SUB-CONTRACTOR TO PROVIDE ALL NECESSARY DETAILS AND SYSTEMS TO ACCOUNTANTS/ENGINEERS/ARCHITECTS OF THE BUILDING AND BUILDING COMPONENTS FOR A LONG-LASTING ROOF.
- CONTRACTOR SHALL KEEP ROOF CLEAR OF ALL DEBRIS DURING ALL CONSTRUCTION WORK.
- ROOF SYSTEM AND ANY WORK ASSOCIATED WITH THE ROOF SHALL BE DONE AS NOT TO VOID ANY ROOF WARRANTIES.
- FLASHING:
 - FLASHING SHALL BE INSTALLED IN SUCH A MANNER SO AS TO PREVENT MOISTURE ENTERING THE WALL AND ROOF THROUGH JOINTS IN COPING & IN PENETRATIONS WITH PARAPET WALLS & OTHER PENETRATIONS THROUGH THE ROOF PLANE.
 - FLASHING SHALL BE INSTALLED AT WALL AND ROOF INTERSECTIONS, AT GUTTERS, WHEREVER THERE IS A CHANGE IN ROOF SLOPE OR DIRECTION AND AROUND ROOF OPENINGS, WHERE FLASHING IS METAL, THE METAL SHALL BE CORROSION RESISTANT WITH A THICKNESS OF NOT LESS THAN 1/8" GALV. OR EQUIVALENT.
 - PARAPET WALLS SHALL BE PROPERLY COPED WITH NON-COMBUSTIBLE, WEATHER RESISTANT MATERIALS OF A WIDTH NO LESS THAN THE THICKNESS OF THE PARAPET WALL.
- ROOF DRAINAGE:
 - DESIGN OF ROOF DRAINAGE SYSTEMS SHALL COMPLY WITH THE INT. PLUMBING CODE, FINAL DETAILED DESIGN OF ROOF DRAINAGE SYSTEMS TO BE BY PLUMBING CONSULTANT.
 - OVERSIZE ROOF DRAINS, GUTTERS AND/OR DOWNSPOUTS SHOULD BE PROVIDED TO ELIMINATE ANY OVERTOW OR BACKUP.
 - NOT LESS THAN 2 ROOF DRAINS SHALL BE INSTALLED IN ROOF AREAS 1000 SF IN ROOFS OVER 10000 SF IN AREA.
 - WHERE ROOF DRAINS ARE REQUIRED, SECONDARY (EMERGENCY) ROOF DRAINS OR SCUPPERS SHALL BE PROVIDED UNLESS THE ROOF PERIMETER CONSTRUCTION EXTENDS ABOVE THE ROOF IN SUCH A MANNER THAT WATER WILL BE ENTRAPPED BY THE PRIMARY DRAIN SYSTEMS FOR ANY REASON.
 - SECONDARY ROOF DRAIN SYSTEMS SHALL HAVE THE END POINT OF DISCHARGE SEPARATE FROM THE PRIMARY SYSTEM. DISCHARGE SHALL BE ABOVE GRADE, IN A LOCATION THAT WOULD NOT BE OBSERVED BY THE BUILDING OCCUPANTS OR MAINTENANCE PERSONNEL.
 - SECONDARY SCUPPERS SHALL BE SIZED TO PREVENT THE DEPTH OF PONDING WATER FROM EXCEEDING THAT FOR WHICH THE ROOF WAS DESIGNED. THEY SHALL HAVE AN OPENING DIMENSION OF NOT LESS THAN 4".
 - A GABLET OR DAPPLE SHALL BE INSTALLED ON THE ROOF SIDE OF ANY PENETRATION GREATER THAN 30" WIDE AS MEASURED PERPENDICULAR TO THE SLOPE.
 - ALL ROOF SLOPES, INCLUDING FLATTERED ROOF AREAS TO OBTAIN POSITIVE DRAINAGE, TO BE INTEGRATED INTO UNDERLYING ROOF SUPPORT DESIGN. PROVIDE FLATTERED ROOF INSULATION TO MAINTAIN MINIMUM 1/4" ROOF SLOPE AT ALL FLATTERED ROOF AREAS TO OBTAIN POSITIVE DRAINAGE. ALL ROOF SLOPES SHALL BE CONSISTENT WITH ALL PENETRATIONS AND CHANGES IN ROOF SLOPES.
- PROVIDE CONCRETE FLASH BLOCKS AT ALL DOWNPOUT LOCATIONS.
- ROOF VENTILATION INTAKE & EXHAUST VENTS SHALL BE PROVIDED IN EACH & ALL UNCLOSED ATTIC JOIN RAFTER SPACES TO CREATE CROSS VENTILATION IN ACCORDANCE WITH CODES & THE MANUFACTURER'S INSTALLATION INSTRUCTIONS.
- INSULATE AND SEAL TO ENSURE ADEQUATE AN INTRUSION BARRIER AROUND ALL EXTERIOR PENETRATIONS TO PREVENT WATER AND SPRINKLER PIPE FRICTION.
- ROOF ACCESS & GUARDS:
 - WHERE EQUIPMENT REQUIRING ACCESS OR APPLIANCES ARE LOCATED ON AN ELEVATED STRUCTURE ON THE ROOF OF A BUILDING SUCH THAT PERSONNEL WILL HAVE TO CLIMB MORE THAN 18" ABOVE GRADE TO ACCESS SUCH EQUIPMENT OR APPLIANCES, A PERMANENT INTERIOR OR EXTERIOR MEANS OF ACCESS SHALL BE PROVIDED. SUCH ACCESS SHALL NOT REQUIRE CLIMBING OVER OBSTRUCTIONS GREATER THAN 30" IN HEIGHT OR WALKING ON ROOFS HAVING A SLOPE GREATER THAN 4:12.
 - WHERE THE ROOF HATCH OPENING OR ROOF EQUIPMENT IS LOCATED WITHIN 6" OF THE ROOF EDGE, SUCH ROOF ACCESS OR ROOF EDGE SHALL BE PROTECTED BY GUARDS, GUARDS TO BE AT LEAST ABOVE WALKING SURFACE & PREVENT THE PASSAGE OF A SPHERE 3" IN DIAMETER.
 - ANY ROOF PATCHES SHALL BE INTERNALLY BROKEN.
 - SEE DETAIL FOR ACCESS PORTS FOR ADDITIONAL REQUIREMENTS.
- ACCESS TO UNOCCUPIED SPACES
 - CRACK SPACES SHALL BE PROVIDED WITH NO FEWER THAN 1 ACCESS OPENING PERCH SHALL NOT BE LESS THAN 18" X 24".
 - AN OPENING OF NOT LESS THAN 20" X 20" SHALL BE PROVIDED TO ANY ATTIC AREA HAVING A CLEAR HEIGHT OF OVER 30". CLEAR HEADROOM OF NOT LESS THAN 6' SHALL BE PROVIDED IN THE ATTIC SPACE OR ABOVE THE ACCESS OPENING.
 - ACCESS TO MECHANICAL APPLIANCES INSTALLED IN UNDER FLOOR AREAS, IN ATTIC SPACES AND ON ROOFS OR ELEVATED STRUCTURES SHALL BE IN ACCORDANCE WITH THE INT. MECHANICAL CODE.
 - ATTIC COVERING APPLIANCES SHALL BE PROVIDED WITH AN OPENING AID UNOCCUPIED PASSAGEWAY LARGE ENOUGH TO ALLOW REMOVAL OF THE LARGEST APPLIANCE. THE PASSAGEWAY SHALL NOT BE LESS THAN 20" X 24" AND NOT MORE THAN 20" IN LENGTH MEASURED ALONG THE CENTERLINE OF THE PASSAGEWAY FROM THE OPENING TO THE APPLIANCE. THE PASSAGEWAY SHALL HAVE CONTINUOUS EDGE FLOORING NOT LESS THAN 1/4" WIDE. A LEVEL SURFACE SPACE NOT LESS THAN 20" X 20" SHALL BE PRESENT AT THE FRONT OF SERVICE SIDE OF THE APPLIANCE.
- PROVIDE ROOF SHOW GUARDS/SPALES ABOVE ALL INTERVIEW POINTS TO BUILDING.
- ROOF TRUSSES DESIGN BUILD BY TRUSS MANUFACTURER.
- PROVIDE MANUFACTURED ROOF PROTECTED GAS PIPING AND CONDUIT SUPPORTS AS REQUIRED. PART CRESTS/LEDGERS "YELLOW" AT WALKWAY PAD CROSSINGS ONLY.
- FLAT LOW SLOPE (<1:12) MEMBRANE ROOF:
 - ROOF MEMBRANE TO EXTEND UP AND OVER TOP OF ANY PARAPET WALLS AND SHALL OVERLAP FRONT OF WALL 12". MEMBRANE TO EXTEND UP VERTICALLY UNDER FRONT RAILING.
 - PROVIDE WALKWAY MADE OF ALL MEMBRANE ROOFING FROM ROOF ACCESS PORTS TO EACH END OF BUILDING.
- ENGINE-PLY MEMBRANE ROOFING:
 - SINGLE-PLY MEMBRANE ROOFING SHALL HAVE A DESIGN SLOPE OF A MINIMUM OF 1/4" IN 12" FOR DRAINAGE.

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 1717 W. 14TH ST. SUITE 100
 DES MOINES, IA 50319
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HUBBEL
 HUBBEL REALTY COMPANY

TELUS CONDOS
 DES MOINES, IA

DATE	BY	REVISION



1 BUILDING #2 - SECOND FLOOR PLAN
SCALE 3/8" = 1'-0"

GENERAL FLOOR PLAN NOTES

1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER SUPPLEMENTAL DOCUMENTS.
2. ALL SITE INFO, FURNITURE, FIXTURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND ANYTHING BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO CIVIL, FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELECTRONIC SAFETY & SECURITY DOCUMENTS AND CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
4. ALL CONDUITE, PIPES, WIRING, ROUGH-IN, ETC. SHOULD BE HELD AS TIGHT AS POSSIBLE TO THE ROOF OR FLOOR DECK OR SHOULD BE SHIPPED WITHIN INTERIOR WALL CAVITIES. NOTHING SHALL BE BELOW THE BOTTOM EDGE OF MAIN BUILDING FRAMES. NOTHING SHALL BE EXPEDIENT BY FINISHED SPACES. NOTHING SHALL PREVENT A SMOOTH CONTINUOUS FINISHED WALL OR CEILING AS NOTED ON THE DRAWINGS AND NO FINISHES SHALL BE IN EXTERIOR WALLS UNLESS PRIOR APPROVAL IS GRANTED BY THE ARCHITECT OR OWNER. PLACEMENT COORDINATION BETWEEN TRADES IS REQUIRED.
5. INSTALL VERTICAL EXPANSION JOINT MINIMAL AT COLUMN GRID LINES AND AT THE HEAD OF BEAM. JAMES SPENCER IS AS OTHERWISE NOTED OR WHERE REQUIRED PER STANDARD PRACTICE TO REDUCE STRESS CRACKING. NO CONTROL JOINT/EXPANSION JOINT IN LOAD BEARING AREAS. STAY AWAY 2'-0" MIN.
6. CAULK & SEAL ALL CONTROL-EXPANDED, SAWCUT JOINTS AT ALL SITERIM & EXTERIOR MASONRY & CONCRETE.
7. PROVIDE LOWER LEVEL AND/OR UNDER FLOOR DRAINAGE SYSTEM TO REDUCE HUMIDITY UPDIP AND PROTECT AGAINST MOISTURE SEEPAGE. PROVIDE FOUNDATION WALL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURES AND PROTECT AGAINST MOISTURE SEEPAGE. BOTH SYSTEMS SHALL OUTLET TO APPROPRIATE RELIEF.
8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT POURING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

BUILDING #2 UNIT MIX:

FIRST FLOOR	
UNIT 1A	1
UNIT 2A	1
UNIT 2B	1
UNIT 2C	1
UNIT 2D	1
UNIT 2E	1
UNIT 2F	1
TOTAL	7 (11 RED GARAGES, 2 ADA GARAGES)

SECOND FLOOR	
UNIT 1A	1
UNIT 1B	1
UNIT 2A	1
UNIT 2B	1
UNIT 2C	1
UNIT 2D	1
UNIT 2E	1
UNIT 2F	1
TOTAL	7

THIRD FLOOR	
UNIT 1A	1
UNIT 1B	1
UNIT 2A	1
UNIT 2B	1
UNIT 2C	1
UNIT 2D	1
UNIT 2E	1
UNIT 2F	1
TOTAL	7

TOTALS	
UNIT 1A	3 (11 1-BEDROOMS + 26A)
UNIT 1B	3
UNIT 2A	3
UNIT 2B	3
UNIT 2C	3
UNIT 2D	3
UNIT 2E	3
UNIT 2F	3
TOTAL	21 (13 GARAGES)

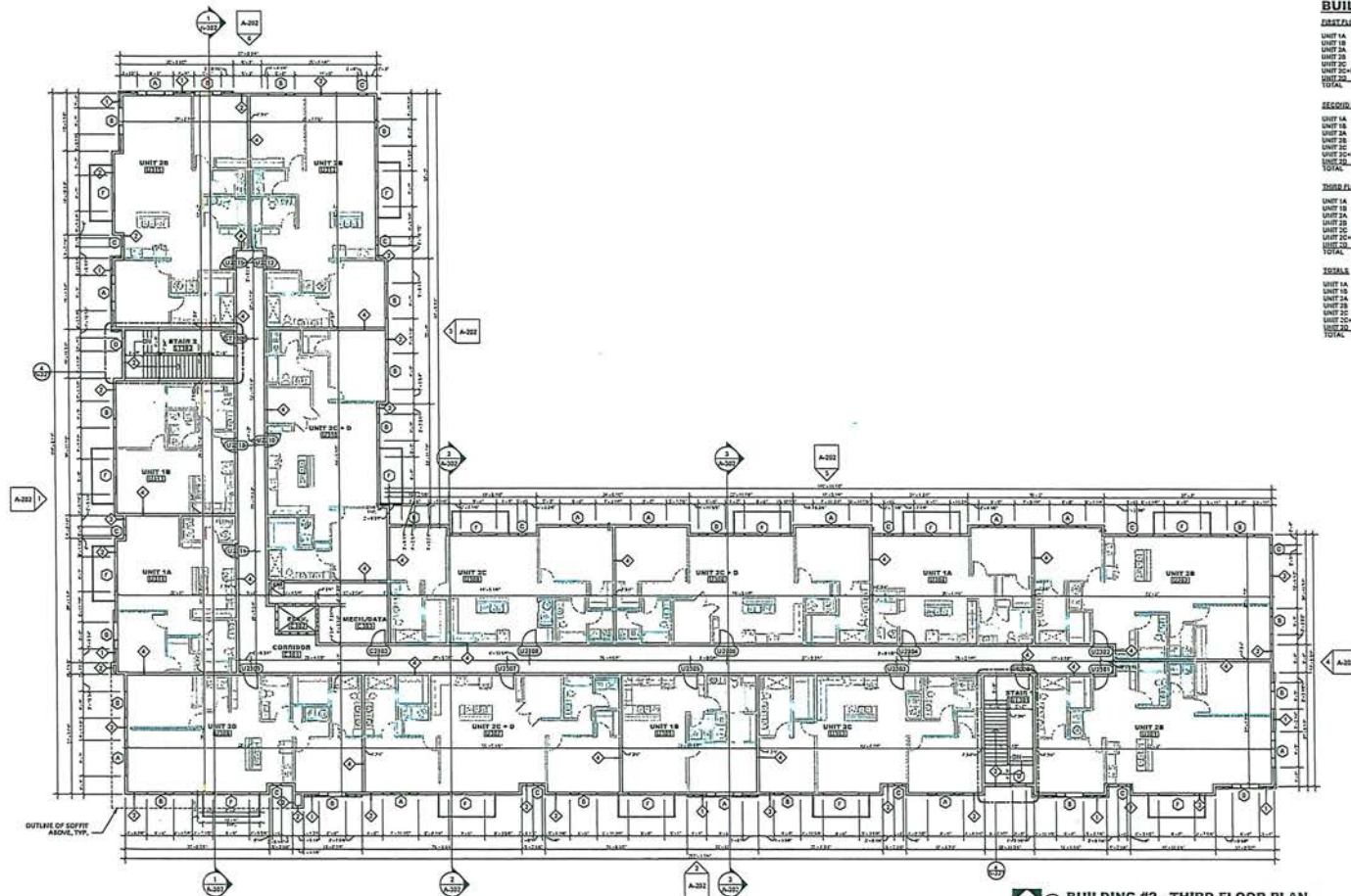


TELUS CONDOS
DES MOINES, IA

DATE	NOV 13 2013
BY	MARK FRANKS
CHECKED BY	MARK FRANKS
SCALE	AS SHOWN ON ALL SHEETS
PROJECT	TELUS CONDOS

Proj. No. 21162
Pkg. No. SP
SHEET
BUILDING #2 - SECOND FLOOR PLAN

Scale: 1/8" = 1'-0"
A-112



BUILDING #2 - THIRD FLOOR PLAN
 NORTH
 SCALE 3/8" = 1'-0"

- GENERAL FLOOR PLAN NOTES**
1. REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DOCUMENTS.
 2. ALL SITE INFO, FURNITURE, FIXTURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND MAY HAVE TO BE PROVIDED FOR COORDINATION PURPOSES ONLY. REFER TO CIVIL, FIRE SUPPRESSION, PLUMBING, MECHANICAL, ELECTRICAL, COMMUNICATIONS, ELEVATION SAFETY & SECURITY DOCUMENTS AND CONSULTANTS FOR SPECIFIC DESIGN INFORMATION & REQUIREMENTS.
 3. DIMENSIONS ARE TO FACE OF FINISH MATERIAL OR CENTER OF STRUCTURE UNLESS OTHERWISE SPECIFIED.
 4. ALL CONDUITS, PIPES, WIRING, HOUGHRAIL, ETC. SHOULD BE HELD AS TIGHT AS FEASIBLE & TO THE JOINT OR END OF EACH LEG OR SHOULD BE KEPT WITHIN INTERIOR WALL CAVITIES. NOTHING SHALL BE BELOW THE BOTTOM EDGES OF MAIN BUILDING FRAMES. NOTHING SHALL BE BELOW FINISH FLOOR FINISHES. NOTHING SHALL PREVENT A SMOOTH CONTINUOUS FINISHED WALL OR CEILING AS NOTED ON THE DRAWINGS AND NO FINISHING SHALL BE EXTERIOR WALLS UNLESS PRED APPROVAL IS GRANTED BY THE ARCHITECT OR OWNER. PLACEMENT COORDINATION BETWEEN TRADES IS REQUIRED.
 5. INSTALL VERTICAL EXPANSION JOINT MINIMAL AT COLUMN/BEAM LINES AND AT THE HEAD OF DOOR JAMB OPENINGS OR AS OTHERWISE NOTED OR WHERE REQUIRED PER STANDARD MATERIAL PRACTICES TO REDUCE STRESS CONCENTRATIONS. CONTROL JOINT/EXPANSION JOINT IN LOAD BEARING AREAS, STAY AWAY 2'-4" MIN.
 6. GRADE & SEAL ALL CONTROL/EXPANSION/SAWTOOTH JOINTS AT ALL INTERIOR & EXTERIOR MASSIVE & CONCRETE.
 7. PROVIDE LOWER LEVEL AND/OR UNDERFLOOR DRAINAGE SYSTEM TO REDUCE HYDROSTATIC UPLIFT AND PROTECT AGAINST MOISTURE SEEPAGE. PROVIDE FOUNDATION WALL DRAINAGE SYSTEM TO REDUCE LATERAL PRESSURE AND PROTECT AGAINST MOISTURE SEEPAGE. BOTH SYSTEMS SHALL OUTLET TO APPROPRIATE RECELY.
 8. PROVIDE POSITIVE DRAINAGE OF SURFACE WATER AWAY FROM BUILDING WITHOUT FLOWING OF WATER ADJACENT TO BUILDING OR ON PAVEMENTS.

BUILDING #2 UNIT MIX:

FLOOR	UNIT TYPE	QUANTITY	TOTAL
FIRST FLOOR	UNIT 1A	1	1
	UNIT 1B	1	1
	UNIT 2A	1	1
	UNIT 2B	1	1
	UNIT 2C	1	1
TOTAL			3 (31 REG. GARAGE, 3 ADA GARAGE)
SECOND FLOOR	UNIT 1A	1	1
	UNIT 1B	1	1
	UNIT 2A	1	1
	UNIT 2B	1	1
	UNIT 2C	1	1
TOTAL			3 (31 REG. GARAGE, 3 ADA GARAGE)
THIRD FLOOR	UNIT 1A	1	1
	UNIT 1B	1	1
	UNIT 2A	1	1
	UNIT 2B	1	1
	UNIT 2C	1	1
TOTAL			3 (31 REG. GARAGE, 3 ADA GARAGE)
TOTALS	UNIT 1A	3	3
	UNIT 1B	3	3
	UNIT 2A	3	3
	UNIT 2B	3	3
	UNIT 2C	3	3
TOTAL			11 1-BEDROOMS + 30K 33 (33 GARAGES)

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TELUS CONDOS
 DES MOINES, IA

DATE	2/15/2024
BY	ARCHITECT
PROJECT	TELUS CONDOS
SCALE	3/8" = 1'-0"

DATE: 2/15/2024
 FIG. NO.: 21162
 SHEET NO.: 2P
 SHEET TITLE: BUILDING #2 - THIRD FLOOR PLAN
 DRAWING NO.: A-113

DRAFTSTOPPING NOTES

DRAFTSTOPPING ONLY REQUIRED BY COMBUSTIBLE BUILDINGS WITHOUT AUTOMATIC SPRINKLER SYSTEMS THROUGHOUT.

- DRAFTSTOPPING IN FLOORS SHALL BE INSTALLED TO SUBSIDE FLOORCEILING ASSEMBLIES.
 - GROUPS ALL R-1 & R-2 WITH 3 OR MORE DWELLING UNITS, R-1 WITH 2 DWELLING UNITS, & ALL R-3 BUILDINGS - SHALL BE LOCATED ABOVE A FIRE LINE WITH THE DWELLING UNIT AND SLEEPING UNIT SEPARATIONS.
 - ALL OTHER GROUPS - SHALL BE INSTALLED SO THAT HORIZONTAL FLOOR AREAS DO NOT EXCEED 1,000 SF.
- DRAFTSTOPPING ATTIC SPACES SHALL BE INSTALLED TO SUBSIDE ATTIC SPACES & CONCEALED ROOF SPACES.
 - GROUPS ALL R-1 & R-2 WITH 3 OR MORE DWELLING UNITS - SHALL BE LOCATED ABOVE & IN LINE WITH THE SLEEPING UNIT & DWELLING UNIT SEPARATIONS, WALLS THAT DO NOT EXTEND TO THE UNDERSIDE OF THE ROOF SHEATHING ABOVE.
 - EXCEPTION 1: WHERE CORRIDOR WALLS PROVIDE A SEPARATION, ONLY REQUIRED ABOVE ONE OF THE CORRIDOR WALLS.
 - EXCEPTION 2: R-3 THAT DO NOT EXCEED 4 STOREYS ABOVE GRADE PLANE, THE ATTIC SPACE SHALL BE SUBDIVIDED INTO AREAS NOT EXCEEDING 3,000 SF OR ABOVE EVERY 2 DWELLING UNITS, WHICHEVER IS SMALLER.
 - ALL OTHER GROUPS - SHALL BE INSTALLED SUCH THAT ANY HORIZONTAL AREA DOES NOT EXCEED 3,000 SF.
- DRAFTSTOPPING MATERIALS SHALL BE NO LESS THAN 1/2" GYP SB, 3/4" WOOD STRUCTURAL PANEL, OR 3/8" PARTICLEBOARD, 1" NOMINAL LUMBER, CEMENT FIBERBOARD, BATTLS OR BLANKETS OF MINERAL WOOL OR GLASS FIBER, ADEQUATELY SUPPORTED.
- OPENINGS IN THE DRAFTSTOPPING SHALL BE PROTECTED BY SELF-CLOSING DOORS OR AUTOMATIC LATCHES CONSTRUCTED AS REQUIRED FOR THE DRAFTSTOPPING PARTITIONS.

GENERAL DRAFTSTOPPING

DRAFTSTOPPING ONLY REQUIRED BY COMBUSTIBLE BUILDINGS WITHOUT AUTOMATIC SPRINKLER SYSTEMS THROUGHOUT.

- DRAFTSTOPPING IN FLOORS SHALL BE INSTALLED TO SUBSIDE FLOORCEILING ASSEMBLIES.
 - GROUPS ALL R-1 & R-2 WITH 3 OR MORE DWELLING UNITS, R-1 WITH 2 DWELLING UNITS, & ALL R-3 BUILDINGS - SHALL BE LOCATED ABOVE & IN LINE WITH THE DWELLING UNIT AND SLEEPING UNIT SEPARATIONS.
 - ALL OTHER GROUPS - SHALL BE INSTALLED SO THAT HORIZONTAL FLOOR AREAS DO NOT EXCEED 1,000 SF.
- DRAFTSTOPPING IN ATTIC SPACES SHALL BE INSTALLED TO SUBSIDE ATTIC SPACES & CONCEALED ROOF SPACES.
 - GROUPS ALL R-1 & R-2 WITH 3 OR MORE DWELLING UNITS - SHALL BE LOCATED ABOVE & IN LINE WITH THE SLEEPING UNIT & DWELLING UNIT SEPARATIONS, WALLS THAT DO NOT EXTEND TO THE UNDERSIDE OF THE ROOF SHEATHING ABOVE.
 - EXCEPTION 1: WHERE CORRIDOR WALLS PROVIDE A SEPARATION, ONLY REQUIRED ABOVE ONE OF THE CORRIDOR WALLS.
 - EXCEPTION 2: R-3 THAT DO NOT EXCEED 4 STOREYS ABOVE GRADE PLANE, THE ATTIC SPACE SHALL BE SUBDIVIDED INTO AREAS NOT EXCEEDING 3,000 SF OR ABOVE EVERY 2 DWELLING UNITS, WHICHEVER IS SMALLER.
 - ALL OTHER GROUPS - SHALL BE INSTALLED SUCH THAT ANY HORIZONTAL AREA DOES NOT EXCEED 3,000 SF.
- OPENINGS IN THE DRAFTSTOPPING SHALL BE PROTECTED BY SELF-CLOSING DOORS OR AUTOMATIC LATCHES CONSTRUCTED AS REQUIRED FOR THE DRAFTSTOPPING PARTITIONS.

GENERAL ROOF PLAN NOTES

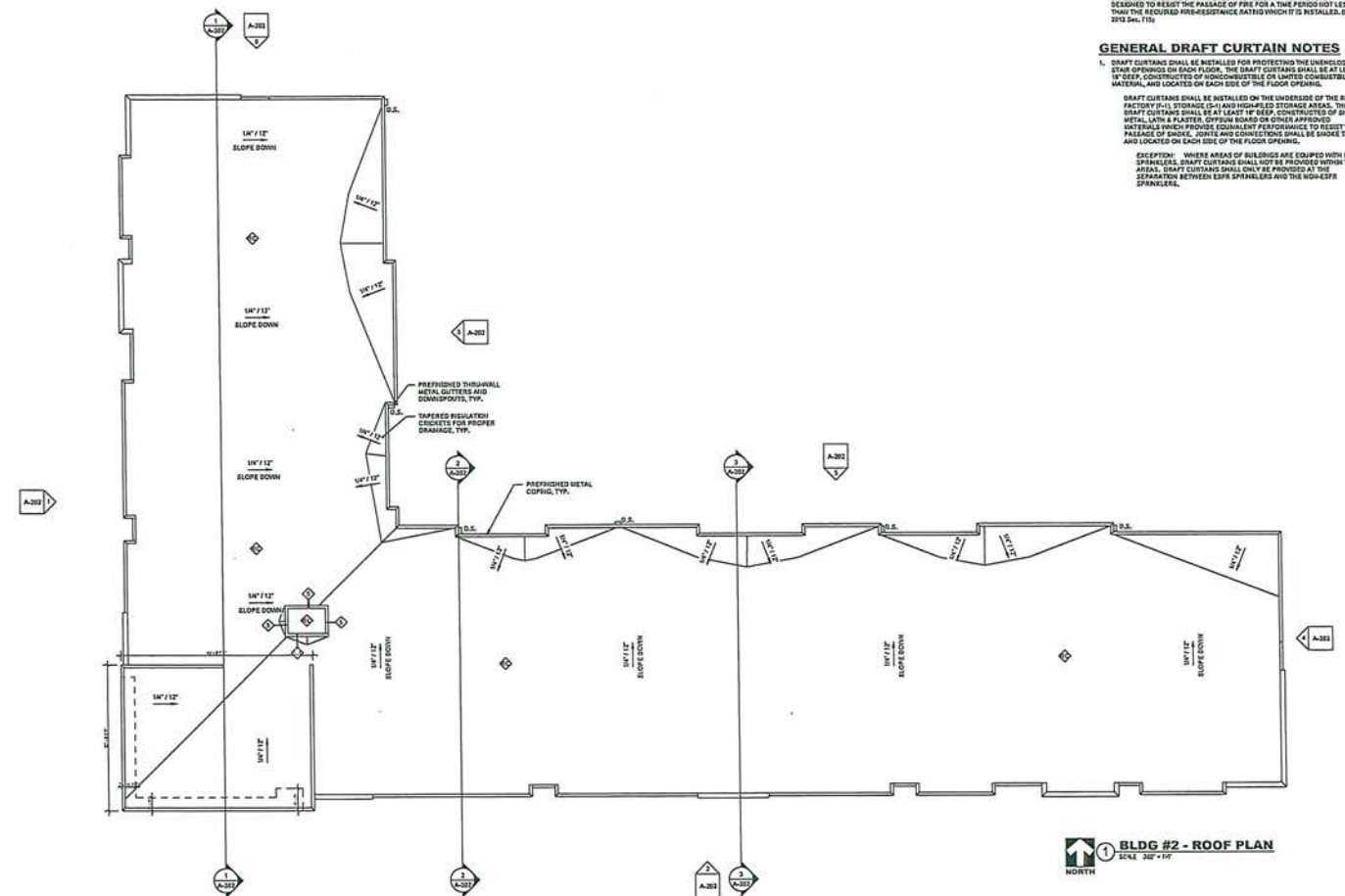
- PENETRATIONS INTO OR THROUGH FIRE-RESISTIVE WALL ASSEMBLIES AND HORIZONTAL ASSEMBLIES SHALL BE PROTECTED BY APPROVED PENETRATION FIRESTOP SYSTEMS DESIGNED TO RESIST THE PASSAGE OF FIRE FOR THE PERIOD NOT LESS THAN THE REQUIRED FIRE-RESISTANCE RATING WHICH IT IS INSTALLED. IBC 703.5.11.4.
- ALL JOINTS INSTALLED IN OR BETWEEN FIRE-RESISTIVE WALLS, FLOORS OR FLOORCEILING ASSEMBLIES AND ROOFS OR ROOFCEILING ASSEMBLIES SHALL BE PROTECTED BY APPROVED FIRE-RESISTANT JOINT SYSTEM DESIGNED TO RESIST THE PASSAGE OF FIRE FOR A TIME PERIOD NOT LESS THAN THE REQUIRED FIRE-RESISTANCE RATING WHICH IT IS INSTALLED. IBC 703.5.11.4.

GENERAL DRAFT CURTAIN NOTES

- DRAFT CURTAINS SHALL BE INSTALLED FOR PROTECTING THE UNENCLOSED STAIR OPENINGS OR STAIR LOBBIES. THE DRAFT CURTAINS SHALL BE AT LEAST 1/2" DEEP, CONSTRUCTED OF NON-COMBUSTIBLE OR LIMITED COMBUSTIBLE MATERIAL, AND LOCATED ON EACH SIDE OF THE FLOOR OPENING.
 - DRAFT CURTAINS SHALL BE INSTALLED ON THE UNDERSIDE OF THE ROOF FACTORY-FIT STORAGE (S-I) AND HANGAR STORAGE AREAS. THE DRAFT CURTAINS SHALL BE AT LEAST 1/2" DEEP, CONSTRUCTED OF SHEET METAL, LATH & PLASTER, OFFSHORE BOARD OR OTHER APPROVED MATERIALS WHICH PROVIDE EQUIVALENT PERFORMANCE TO RESIST THE PASSAGE OF FIRE. JOINTS AND EDGES SHALL BE SHOCKED, SHIM, AND LOCATED ON EACH SIDE OF THE FLOOR OPENING.
 - EXCEPTION: WHERE AREAS OF BUILDINGS ARE EQUIPPED WITH CEILING SPRINKLERS, DRAFT CURTAINS SHALL NOT BE PROVIDED AT THESE AREAS. DRAFT CURTAINS SHALL ONLY BE PROVIDED AT THE SEPARATION BETWEEN CEILING SPRINKLERS AND THE NON-CEILING SPRINKLERS.

GENERAL ROOF PLAN NOTES

- REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINE DOCUMENTS.
- ALL FEATURES AND EQUIPMENT SHOWN SHOULD BE CONSIDERED CONCEPTUAL AND NOT BE PROVIDED FOR THE CONTRACTOR PURPOSES ONLY. REFER TO CONTRACTOR'S SAFETY & SECURITY DOCUMENTS AND CONSULTANTS FOR SPECIFIC REQUIREMENTS. REFER TO STRUCTURAL DRAWINGS FOR ROOF OPENING STRUCTURAL FRAMING.
- ALL ROOF PENETRATIONS FOR VENT OR ROOF VENTS AND ESCAPE PATHS MAY NOT BE INSTALLED ON THE ROOF PLAN. HOWEVER, ALL ROOF FLASHING WORK SHALL BE PROVIDED BY THE ROOFING SUBCONTRACTOR SO AS TO OBTAIN FULL ROOFING WARRANTIES. REFER TO ALL OTHER CONSTRUCTION DOCUMENTS FOR ADDITIONAL ROOF PENETRATIONS REQUIRING ROOF FLASHING WORK.
- IT SHOULD BE THE RESPONSIBILITY OF THE ROOF SUBCONTRACTOR TO VERIFY THE EXISTING DETAIL AND SYSTEMS TO ACCOMMODATE THE EXPANSION/CONSTRUCTION OF THE BUILDING AND BUILDING COMPONENTS FOR A LONG-LASTING ROOF.
- CONTRACTOR SHALL KEEP ROOF CLEAN OF ALL DEBRIS DURING ALL CONSTRUCTION WORKS.
- ROOF SYSTEM AND ANY WORK ASSOCIATED WITH THE ROOF SHALL BE DONE AS TO NOT VOID ANY ROOF WARRANTIES.
- FLASHING:
 - FLASHING SHALL BE INSTALLED IN SUCH A MANNER SO AS TO PREVENT LIQUID WATER FROM PENETRATING THROUGH JOINTS BY COPINGS & INTERSECTIONS WITH PARAPET WALLS & OTHER PENETRATIONS THROUGH THE ROOF PLANE.
 - FLASHING SHALL BE INSTALLED AT WALL AND ROOF INTERSECTIONS AT OUTLET, WHEREVER THERE IS A CHANGE IN ROOF SLOPE OR DIRECTION AND THROUGH ROOF OPENINGS, WHERE FLASHING IS METAL, THE METAL SHALL BE CORROSION RESISTANT WITH A THICKNESS OF NOT LESS THAN 1/8" OF GALVANIZED SHEET.
 - PARAPET WALLS SHALL BE PROPERLY COPED WITH NON-COMBUSTIBLE, WEATHER-RESISTANT MATERIALS OF A WIDTH NO LESS THAN THE THICKNESS OF THE PARAPET WALL.
- ROOF DRAINAGE:
 - DESIGN OF ROOF DRAINAGE SYSTEMS SHALL COMPLY WITH THE CITY OF DES MOINES CODE. THE DETAIL DESIGN OF ROOF DRAINAGE SYSTEMS SHALL BE BY PLUMBING CONSULTANT.
 - DESIGNED ROOF DRAINAGE, GUTTERS AND/OR DOWNSPOUTS SHOULD BE PROVIDED TO ELIMINATE ANY EVERFLOW OR BACKUP.
 - NOT LESS THAN 3 ROOF DRAIN SHALL BE INSTALLED IN ROOF AREAS 1800 SF OR LESS AND NOT LESS THAN FOUR ROOF DRAINS SHALL BE INSTALLED IN ROOFS OVER 1800 SF IN AREA.
 - WHERE ROOF DRAIN ARE REQUIRED, SECONDARY (EMERGENCY) ROOF DRAINAGE EQUIPMENT SHALL BE PROVIDED WHERE THE ROOF PENETRATES THROUGH EXTERIOR WALLS ABOVE GRADE. ROOF DRAINAGE EQUIPMENT WATER SHALL BE DRAINAGE TO THE PRIMARY DRAIN ALONG BUILDUP FOR ANY AREAS.
 - SECONDARY ROOF DRAIN SYSTEMS SHALL HAVE THE END POINT OF DISCHARGE SEPARATE FROM THE PRIMARY SYSTEM. DISCHARGE SHALL BE ABOVE GRADE, IN A LOCATION THAT WILL NOT BE OBSERVED BY THE BUILDING OCCUPANTS OR MAINTENANCE PERSONNEL.
 - SECONDARY SCUPPERS SHALL BE DESIGNED TO PREVENT THE DEPTH OF FLOWING WATER FROM EXCEEDING THAT FOR WHICH THE ROOF IS DESIGNED. THEY SHALL HAVE AN OPENING DIMENSION OF NOT LESS THAN 4".
 - A CRACKER OR SADDLE SHALL BE INSTALLED ON THE RIDGE SIDE OF ANY PENETRATION GREATER THAN 3" WIDE AS MEASURED PERPENDICULAR TO THE SLOPE.
 - ALL ROOF SLOPES, INCLUDING FLATTENED ROOF AREAS TO OBTAIN POSITIVE DRAINAGE, TO BE INTEGRATED INTO UNDERDRYING SUPPORT DESIGN. PROVIDE TYPICAL ROOF DRAINAGE TO MAINTAIN POSITIVE DRAINAGE ALL THE WAY TO ROOF DRAIN FROM ALL PENETRATIONS AND CHANGES IN ROOF SLOPES.
 - PROVIDE CONCRETE FLASH BLOCKS AT ALL DOWNSPOUT LOCATIONS.
 - ROOF VENTILATION INTAKE & EXHAUST VENTS SHALL BE PROVIDED IN EACH & ALL ENCLOSED ATTIC AND RAFTER SPACES TO CREATE CROSS VENTILATION IN ACCORDANCE WITH CODES & MANUFACTURER'S INSTRUCTIONS.
 - INSULATE AND SEAL TO ENSURE ADEQUATE AIR INFILTRATION BARRIER AROUND ALL EXTERIOR PENETRATIONS TO PREVENT WATER AND SPARKLER PIPE PENETRATION.
 - ROOF ACCESS & GUARDS:
 - WHERE EQUIPMENT REQUIRING ACCESS OR APPLIANCES ARE LOCATED ON AN ELEVATED STRUCTURE ON THE ROOF OF A BUILDING SUCH THAT PERSONNEL WILL HAVE TO CLIMB HEIGHTS MORE THAN 4 FEET ABOVE GRADE TO ACCESS EQUIPMENT OR APPLIANCE, A PERMANENT ATTIC OR EXTERIOR MEANS OF ACCESS SHALL BE PROVIDED. SUCH ACCESS SHALL NOT REQUIRE CLIMBING OR OBSTACLES GREATER THAN 24" IN HEIGHT OR WALKWAYS ON ROOFS HAVING A SLOPE GREATER THAN 4:1.
 - WHERE THE ROOF HATCH OPENING OR ROOF EQUIPMENT IS LOCATED WITHIN 4' OF THE ROOF EDGE, HIGH ROOF ACCESS OR ROOF TOES SHALL BE PROVIDED BY GUARDS, GUARDS TO BE 42" ABOVE WALKING SURFACE & PREVENT THE PASSAGE OF A SPHERE 3" IN DIAMETER.
 - ANY ROOF HATCHES SHALL BE THERMALLY BROKEN.
 - SEE DETAIL 204 ACCESS NOTES FOR ADDITIONAL REQUIREMENTS.
 - ACCESS TO UNOCCUPIED SPACES
 - DRAIN SPACES SHALL BE PROVIDED WITH NO FLOOR THAN 1 ACCESS OPENING WHICH SHALL NOT BE LESS THAN 18" X 14".
 - AN OPENING OF NOT LESS THAN 30" X 36" SHALL BE PROVIDED TO ANY ATTIC AREA HAVING A CLEAR HEIGHT OF OVER 30". CLEAR HEADROOM OF NOT LESS THAN 6' SHALL BE PROVIDED IN THE ATTIC SPACE AT OR ABOVE THE ACCESS OPENING.
 - ACCESS TO MECHANICAL APPLIANCES INSTALLED IN UNDER FLOOR AREAS, IN ATTIC SPACES AND ON ROOFS OR ELEVATED STRUCTURES SHALL BE IN ACCORDANCE WITH THE INT. MECHANICAL CODE.
 - ATTIC CONTAINING APPLIANCES SHALL BE PROVIDED WITH AN OPENING AND UNRESTRICTED PASSAGEWAY LARGE ENOUGH TO ALLOW REMOVAL OF THE LARGEST APPLIANCE. THE PASSAGEWAY SHALL NOT BE LESS THAN 30" X 24" AND NOT MORE THAN 24" IN LENGTH MEASURED ALONG THE CENTERLINE OF THE PASSAGEWAY FROM THE OPENING TO THE APPLIANCE. THE PASSAGEWAY SHALL HAVE CONTINUOUS SOLID FLOORING NOT LESS THAN 24" WIDE. A LEVEL FLOOR SPACE NOT LESS THAN 30" X 36" SHALL BE PROVIDED AT THE FRONT OF SERVICE SIDE OF THE APPLIANCE.
 - PROVIDE ROOF SNOW GUARDS/SPICES ABOVE ALL ENTRANCE PORTS TO BUILDING.
 - ROOF TRUSSES DESIGN BUILT BY TRUSS MANUFACTURER.
 - PROVIDE MANUFACTURED ROOF PROTECTED GAB PIPING AND CONDUIT SUPPORTS AS REQUIRED. PAINT OBSCURELY TOLUOLY AT WALKWAY OR CROSSINGS ONLY.
 - FLAT ROOF SLOPE - 2:12 MEMBRANE ROOF:
 - ROOF MEMBRANE TO EXTEND UP AND OVER TOP OF ANY PARAPET WALLS AND DRAPE OVER FRONT OF WALL. MEMBRANE TO EXTEND UP VERTICALLY UP OVER FRONT MATERIAL 12".
 - PROVIDE WALKWAY PADS ON ALL MEMBRANE ROOFING FROM ROOF ACCESS PORT(S) TO EACH END OF BUILDING.
 - SINGLE-PLY MEMBRANE ROOFING:
 - BRICK-LY MEMBRANE ROOF SHALL HAVE A DESIGN SLOPE OF A MINIMUM OF 1/8" (1%) FOR DRAINAGE.



BLDG #2 - ROOF PLAN
2046 30' x 10'

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TELUS CONDOS
DES MOINES, IA

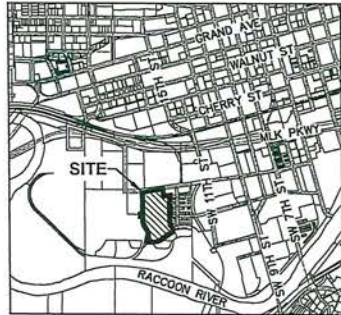
DATE: 10/20/2017 10:28 AM
PROJECT: TELUS CONDOS
DRAWN BY: J. HUBBELL
CHECKED BY: J. HUBBELL
SCALE: AS SHOWN

NO. 21162
SP
BLDG #2 - ROOF PLAN
A-114

PRELIMINARY PLAT / DEVELOPMENT PLAN FOR: GRAY'S STATION PLAT 5

DES MOINES, IOWA

VICINITY MAP NOT TO SCALE



DES MOINES, IOWA

OWNER
HRC NPS I LLC
6900 WESTOWN PKWY
WEST DES MOINES, IA 50268

APPLICANT
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6300 WESTOWN PKWY
WEST DES MOINES, IA 50268
CONTACT: CALEB SMITH
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ENGINEER
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DATE OF SURVEY
08/05/2022

BENCHMARKS

- CITY BENCHMARK #725: BRASS CAP IN THE NORTHEAST TRAFFIC SIGNAL BASE AT THE SOUTHWEST CORNER OF MARTIN LUTHER KING DRIVE AND SOUTHWEST 11TH STREET. ELEVATION=28.81
- CITY BENCHMARK #5561: BRASS TABLET IN THE CONCRETE WALL 43.8 FEET WEST OF THE EAST FACE NEAR THE SANITARY SEWER PUMP STATION. ELEVATION=25.40

SUBMITTAL DATES

FIRST SUBMITTAL: 10/19/2022
SECOND SUBMITTAL: 01/04/2023

LEGAL DESCRIPTION

A PART OF LOTS 6, 7, AND 9 CENTRAL DES MOINES INDUSTRIAL PARK AND A PART OF PARCELS 2018-145 OF SAID LOTS 6, 7, AND 9, AS SHOWN ON THE PLAT OF SURVEY RECORDED IN BOOK 17873, PAGE 735, BOTH OFFICIAL PLATS IN THE CITY OF DES MOINES, POLK COUNTY, IOWA AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF LOT 12, GRAY'S STATION PLAT 2, AN OFFICIAL PLAT, THENCE SOUTH 89°20'42" EAST ALONG THE WEST LINE OF SAID LOT 12, A DISTANCE OF 145.00 FEET TO THE SOUTHWEST CORNER OF SAID LOT 12; THENCE NORTH 81°39'18" EAST ALONG THE SOUTHERLY LINE OF SAID LOT 12, A DISTANCE OF 128.11 FEET TO THE NORTHWEST CORNER OF OUTLOT 1 AND GRAYS STATION PLAT 2; THENCE SOUTH 00°01'40" EAST ALONG THE WESTERLY LINE OF SAID OUTLOT "1" AND THE WESTERLY LINE OF GRAYS STATION PLAT 3, AN OFFICIAL PLAT, 68.73 FEET; THENCE SOUTH 68°58'20" WEST CONTINUING ALONG SAID WESTERLY LINE OF SAID GRAYS STATION PLAT 3, A DISTANCE OF 12.77 FEET; THENCE SOUTH 00°01'40" EAST CONTINUING ALONG SAID WESTERLY LINE 107.91 FEET; THENCE SOUTH 89°08'20" WEST CONTINUING ALONG SAID WESTERLY LINE, 6.23 FEET; THENCE SOUTH 00°01'40" EAST CONTINUING ALONG SAID WESTERLY LINE AND THE WESTERLY LINE OF GRAY'S STATION PLAT 4, AN OFFICIAL PLAT, 122.09 FEET; THENCE NORTH 89°58'20" EAST CONTINUING ALONG SAID WESTERLY LINE OF SAID GRAY'S STATION PLAT 4, A DISTANCE OF 17.61 FEET; THENCE SOUTH 00°01'40" EAST CONTINUING ALONG SAID WESTERLY LINE, 63.00 FEET; THENCE SOUTH 89°58'20" WEST CONTINUING ALONG SAID WESTERLY LINE, 8.11 FEET; THENCE SOUTH 00°01'40" EAST CONTINUING ALONG SAID WESTERLY LINE, 148.23 FEET; THENCE SOUTHERLY CONTINUING ALONG SAID WESTERLY LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 80.00 FEET, WHOSE ARC LENGTH IS 58.38 FEET AND WHOSE CHORD BEARS SOUTH 20°09'18" WEST, 55.20 FEET; THENCE SOUTH 49°29'41" EAST CONTINUING ALONG SAID WESTERLY LINE, 62.00 FEET; THENCE SOUTH 22°22'59" EAST CONTINUING ALONG SAID WESTERLY LINE, 105.01 FEET TO THE SOUTHWESTERLY CORNER OF STREET LOT "A", SAID GRAY'S STATION PLAT 4; THENCE SOUTHWESTERLY ALONG A CURVE CONCAVE SOUTHEASTERLY WHOSE RADIUS IS 288.00 FEET, WHOSE ARC LENGTH IS 32.35 FEET AND WHOSE CHORD BEARS SOUTH 57°30'24" WEST, 32.34 FEET; THENCE SOUTH 54°23'46" WEST, 246.83 FEET; THENCE WESTERLY ALONG A CURVE CONCAVE NORTHERLY WHOSE RADIUS IS 388.00 FEET, WHOSE ARC LENGTH IS 302.72 FEET AND WHOSE CHORD BEARS SOUTH 77°57'44" WEST, 294.28 FEET; THENCE NORTH 78°28'18" WEST, 26.20 FEET; THENCE NORTH 11°31'42" EAST, 70.00 FEET; THENCE SOUTH 78°28'18" EAST, 22.83 FEET; THENCE NORTH 00°08'11" WEST, 61.22 FEET; THENCE NORTHERLY ALONG A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 224.00 FEET, WHOSE ARC LENGTH IS 128.46 FEET; THENCE NORTH 33°43'49" WEST, 43.36 FEET; THENCE NORTHERLY ALONG A CURVE CONCAVE EASTERLY WHOSE RADIUS IS 278.00 FEET, WHOSE ARC LENGTH IS 181.83 FEET AND WHOSE CHORD BEARS NORTH 16°56'00" WEST, 159.62 FEET; THENCE NORTH 00°08'11" WEST, 58.58 FEET; THENCE NORTH 89°19'18" WEST, 43.92 FEET; THENCE NORTH 00°40'42" EAST, 52.00 FEET TO THE SOUTH LINE OF LOT 8, SAID CENTRAL DES MOINES INDUSTRIAL PARK; THENCE SOUTH 89°19'18" EAST ALONG SAID SOUTH LINE, 43.18 FEET TO THE SOUTHEAST CORNER OF SAID LOT 8; THENCE NORTH 00°08'11" WEST ALONG THE EAST LINE OF SAID LOT 8, A DISTANCE OF 333.17 FEET; THENCE NORTHERLY CONTINUING ALONG SAID EAST LINE AND A CURVE CONCAVE WESTERLY WHOSE RADIUS IS 804.42 FEET, WHOSE ARC LENGTH IS 84.04 FEET AND WHOSE CHORD BEARS NORTH 03°28'39" WEST, 83.89 FEET; THENCE NORTH 06°58'09" WEST CONTINUING ALONG SAID EAST LINE, 15.78 FEET TO THE NORTHEAST CORNER OF SAID LOT 8, ALSO BEING A POINT ON THE SOUTHERLY LINE OF TUTTLE STREET RIGHT OF WAY; THENCE NORTH 81°10'49" EAST ALONG SAID SOUTHERLY RIGHT OF WAY LINE, 411.78 FEET TO THE POINT OF BEGINNING AND CONTAINING 11.47 ACRES (489,715 SQUARE FEET).

THE PROPERTY IS SUBJECT TO ANY AND ALL EASEMENTS OF RECORD.

ZONING

GRAY'S STATION PUD -- ZON2017-00087

EXISTING/ PROPOSED USE

EXISTING USE: UNDEVELOPED
PROPOSED USE: RESIDENTIAL

TAX ABATEMENT - SUSTAINABILITY

- BIKE RACKS FOR PUBLIC USE THAT PROVIDE A MINIMUM OF 10% OF THE AUTOMOBILE PARKING PROVIDED.
- 50% INCREASE IN REQUIRED LANDSCAPING.
- PRIMARY ENTRY WITHIN A MILE OF A DART TRANSIT STOP.
- REDEVELOPMENT OF A PREVIOUSLY DEVELOPED SITE.

INDEX OF SHEETS

NO.	DESCRIPTION
CO.1	COVER SHEET
C1.1	TOPOGRAPHIC SURVEY / DEMOLITION PLAN
C2.0-2.4	DIMENSION PLAN
C3.0-3.5	GRADING PLAN
C4.0-4.4	UTILITY PLAN
CS.1-5.2	DETAILS
LO2.01-L02.05	RDG LANDSCAPE PLAN (SITE LAYOUT PLAN)
LO4.01-LO4.05	RDG LANDSCAPE PLAN (SITE PLANTING PLAN)
L05.01	RDG LANDSCAPE PLAN (DETAILS)

DEVELOPMENT SUMMARY

TOTAL SITE AREA:	11.47 ACRES (489,715 SF)
EXISTING IMPROVISED AREA:	0.00 ACRES (0 SF)
OPEN SPACE CALCULATION:	
TOTAL SITE:	= 489,715 SF (11.47 AC.)
BUILDINGS:	= 95,894 SF
STREETS:	= 12,745 SF
DRIVEWAYS:	= 24,389 SF
SIDEWALKS:	= 282,622 SF (58.6%)
OPEN SPACE PROVIDED:	
UNITS:	
85 TOWNHOME UNITS	
84 CONDOMINIUM UNITS (LOT 67)	
149 TOTAL UNITS (12.99 UNITS PER ACRE)	

NOTES

- ALL PUBLIC IMPROVEMENTS, INCLUDING SPOT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR PUBLIC IMPROVEMENT PLANS. IF THERE ARE ANY DISCREPANCIES BETWEEN THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLANS THE DETAILED PUBLIC IMPROVEMENT PLANS SHALL GOVERN.
- XXXXTHIS PROJECT IS ELIGIBLE FOR TAX ABATEMENT IN ACCORDANCE WITH THE URBAN RENAISSANCE DEVELOPMENT AGREEMENT - SECTION 5.02 (8K16629 P2291-289).xxxx

UTILITY WARNING

ANY UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY AND RECORDS OBTAINED BY THIS SURVEYOR. THE SURVEYOR MAKES NO GUARANTEE THAT THE UTILITIES SHOWN COMPRISE ALL THE UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UTILITIES SHOWN ARE IN THE EXACT LOCATION SHOWN.

GENERAL LEGEND

PROPOSED	EXISTING
PROPERTY BOUNDARY	SANITARY MANHOLE
LOT LINE	WATER VALVE BOX
CENTER LINE	FIRE HYDRANT
RIGHT OF WAY	WATER CURB STOP
BUILDING SETBACK	WELL
PERMANENT EASEMENT	STORM SEWER MANHOLE
TEMPORARY EASEMENT	STORM SEWER SINGLE INTAKE
TYPE SW-501 STORM INTAKE	STORM SEWER DOUBLE INTAKE
TYPE SW-502 STORM INTAKE	FLARED END SECTION
TYPE SW-503 STORM INTAKE	DECIDUOUS TREE
TYPE SW-505 STORM INTAKE	CONIFEROUS TREE
TYPE SW-506 STORM INTAKE	DECIDUOUS SHRUB
TYPE SW-512 STORM INTAKE	CONIFEROUS SHRUB
TYPE SW-513 STORM INTAKE	ELECTRIC POWER POLE
TYPE SW-401 STORM MANHOLE	GUY ANCHOR
TYPE SW-402 STORM MANHOLE	STREET LIGHT
FLARED END SECTION	POWER POLE W/ TRANSFORMER
TYPE SW-301 SANITARY MANHOLE	UTILITY POLE W/ LIGHT
STORM/SANITARY CLEANOUT	ELECTRIC BOX
WATER VALVE	ELECTRIC TRANSFORMER
FIRE HYDRANT ASSEMBLY	ELECTRIC MANHOLE OR VAULT
SIGN	TRAFFIC SIGN
DETECTABLE WARNING PANEL	TELEPHONE JUNCTION BOX
WATER CURB STOP	TELEPHONE MANHOLE/VAULT
SANITARY SEWER	TELEPHONE POLE
SANITARY SERVICE	GAS VALVE BOX
STORM SEWER	CABLE TV JUNCTION BDX
STORM SERVICE	CABLE TV MANHOLE/VAULT
WATERMAIN WITH SIZE	MAIL BOX
WATER SERVICE	BENCHMARK
SAWCUT (FULL DEPTH)	SOIL BORING
SILT FENCE	UNDERGROUND TV CABLE
USE AS CONSTRUCTED	GAS MAIN
MINIMUM PROTECTION ELEVATION	FIBER OPTIC
(UAC)	UNDERGROUND TELEPHONE
MPE	OVERHEAD ELECTRIC
	UNDERGROUND ELECTRIC
	FIELD TILE
	SANITARY SEWER W/ SIZE
	STORM SEWER W/ SIZE
	WATER MAIN W/ SIZE



THE PROJECT REQUIRES AN IOWA NPDES PERMIT #2 AND CITY OF DES MOINES GRADING PERMIT. CIVIL DESIGN ADVANTAGE WILL PROVIDE THE PERMITS AND THE INITIAL STORM WATER POLLUTION PREVENTION PLAN (SWPPP) FOR THE CONTRACTORS USE DURING CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR UPDATING THE SWPPP THROUGHOUT CONSTRUCTION AND MEETING LOCAL, STATE AND FEDERAL REQUIREMENTS.

ALL CONSTRUCTION MATERIALS, DUMPSTERS, DETACHED TRAILERS OR SIMILAR ITEMS ARE PROHIBITED ON PUBLIC STREETS OR WITHIN THE PUBLIC R.O.W.

THE 2022 EDITION OF THE SDAS STANDARD SPECIFICATIONS, THE PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) AND ALL CITY SUPPLEMENTALS, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.

PRELIMINARY PLAT APPROVAL:

APPROVED APPROVED WITH CONDITIONS - SEE EXHIBIT "A"

IN ACCORDANCE WITH CHAPTER 105, DES MOINES MUNICIPAL CODE, AS AMENDED.

DEVELOPMENT SERVICES DIRECTOR: _____ DATE: _____



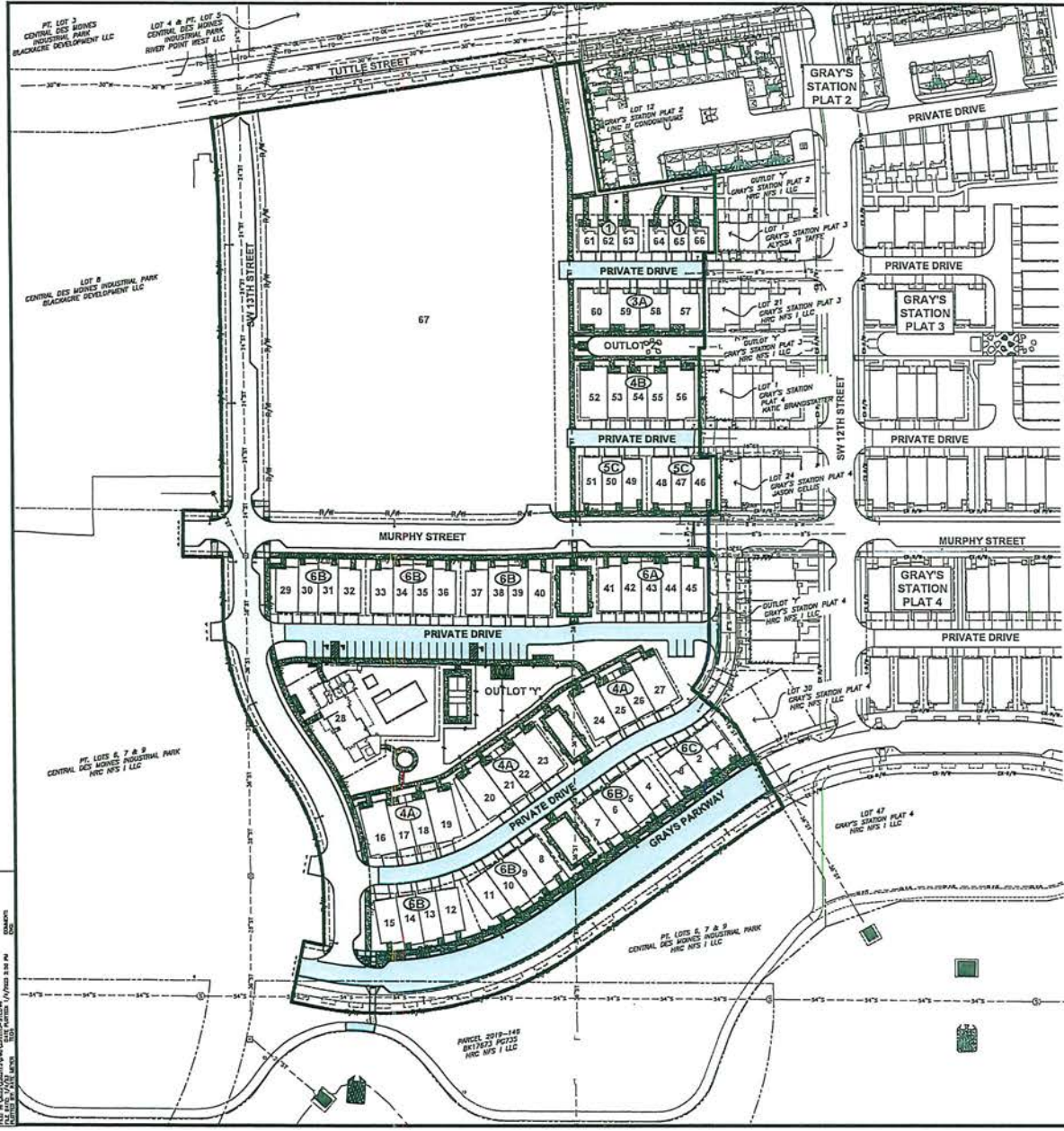
SITE PLAN APPROVAL:

APPROVED APPROVED WITH CONDITIONS - SEE EXHIBIT "A"

IN ACCORDANCE WITH SECTION 135-9.1, 2009 DES MOINES MUNICIPAL CODE, AS AMENDED.

NO CHANGES TO THIS PLAN UNLESS APPROVED IN WRITING FROM THE DEVELOPMENT SERVICES DIRECTOR.

DEVELOPMENT SERVICES DIRECTOR: _____ DATE: _____



GRAY'S STATION TOWNHOME PRODUCT LIST

PLAT	PRODUCT NUMBER	PRODUCT DESCRIPTION
5	1 (LOTS 61-66)	PRODUCT 1 - 3 UNITS
5	3A (LOTS 57-60)	PRODUCT 3A - 4 UNITS
5	4A (LOTS 16-27)	PRODUCT 4A - 4 UNITS
5	4B (LOTS 52-56)	PRODUCT 4B - 5 UNITS
5	5C (LOTS 46-51)	PRODUCT 5C - 3 UNITS
5	6A (LOTS 41-45)	PRODUCT 6A - 5 UNITS
5	6B (LOTS 4-15, 28-40)	PRODUCT 6B - 4 UNITS
5	6C (LOTS 1-3)	PRODUCT 6C - 3 UNITS

⊗ = PRODUCT NUMBER

GRAY'S STATION TOWNHOME PRODUCT COLOR PACKAGE

PLAT	PRODUCT NUMBER	LOT NUMBERS	COLOR PACKAGE NUMBER
5	1	61-63	1
5	1	64-66	1
5	3A	57-60	1
5	4A	16-19	2
5	4A	20-23	1
5	4A	24-27	2
5	4B	52-56	2
5	5C	46-48	1
5	5C	49-51	1
5	6A	41-45	1
5	6B	4-7	3
5	6B	8-11	1
5	6B	12-15	3
5	6B	29-32	3
5	6B	33-36	2
5	6B	37-40	3
5	6C	1-3	2

⊗ = PRODUCT NUMBER



NOTE: ALL PUBLIC IMPROVEMENTS, INCLUDING SPOT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR PUBLIC IMPROVEMENT PLANS. IF THERE ARE ANY DISCREPANCIES BETWEEN THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLANS THE DETAILED PUBLIC IMPROVEMENT PLANS SHALL GOVERN.

DATE	2022.11.13
REVISIONS	
SECOND SUBMITTAL	
FIRST SUBMITTAL	

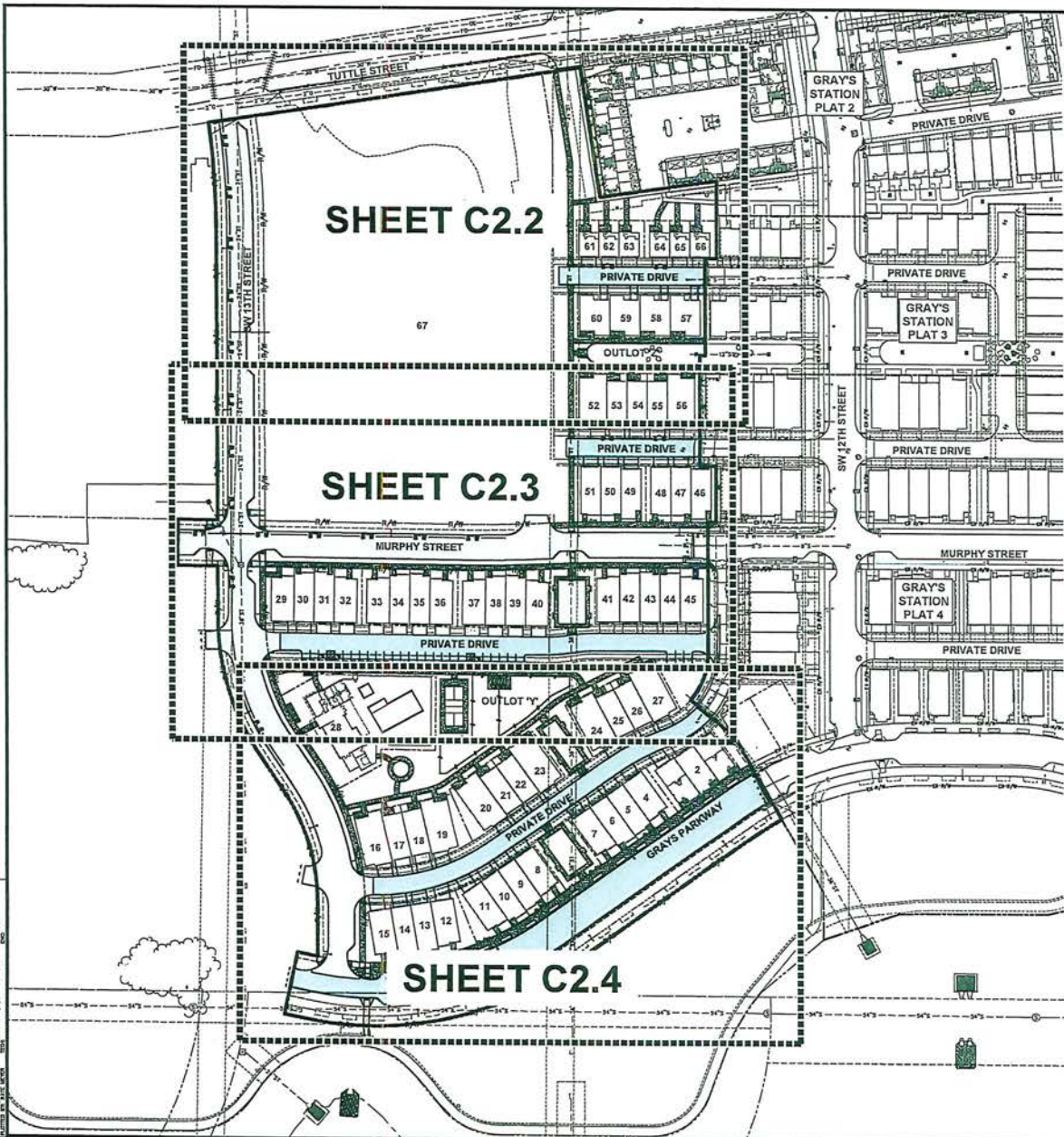
4121 NW URBANDALE DRIVE
URBANDALE, IA 50322
PHONE: (515) 359-4400

EA
CIVIL DESIGN ADVANTAGE
DES MOINES, IOWA

GRAY'S STATION PLAT 5
OVERALL DIMENSION PLAN

C2.0

2202.113



GENERAL NOTES

1. THE 2002 EDITION OF THE SEAS STANDARD SPECIFICATIONS AND ALL CITY SUPPLEMENTALS, IF APPLICABLE, SHALL APPLY TO ALL WORK ON THIS PROJECT UNLESS OTHERWISE NOTED.
2. ALL WORK SHALL COMPLY WITH ADA ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES.
3. ALL WORK SHALL BE IN ACCORDANCE WITH OSHA CODES AND STANDARDS. NOTING INDICATED ON THE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH ANY APPLICABLE SAFETY REGULATIONS.
4. PRIOR TO ANY WORK AT THE SITE, THE CONTRACTOR SHALL EXAMINE ANY APPLICABLE DRAWINGS AVAILABLE FROM THE OWNER, ENGINEER, ARCHITECT, ARCHITECT AND CONSULT WITH OWNER'S PERSONNEL AND UTILITY COMPANY REPRESENTATIVES. NO CONSTRUCTION SHALL BE ALLOWED FOR SHAME FROM FAILING TO COMPLY WITH THIS REQUIREMENT.
5. ONE WEEK PRIOR TO CONSTRUCTION WITHIN CITY BLOCK OR ANY CONNECTION TO PUBLIC SEWER CONTRACTOR SHALL NOTIFY THE CITY'S CONSTRUCTION DIVISION. CONTRACTOR SHALL NOTIFY CITY OF DES MOINES ENGINEERING DEPT. AT LEAST 48 HOURS PRIOR TO COMMENCEMENT OF CONSTRUCTION TO COORDINATE INSPECTIONS. CALL 515-281-4177 OR 615-288-4177.
6. ALL CONSTRUCTION WITHIN PUBLIC BLOCKS/ EASEMENTS, AND/OR ANY CONNECTION TO PUBLIC SEWERS AND STREETS SHALL COMPLY WITH THE CITY'S STANDARDS. NO STEPS ARE ALLOWED IN SANITARY MANHOLES.
7. ALL ENGINEERING SHALL COMPLY WITH ANY APPLICABLE STANDARDS UNLESS OTHERWISE NOTED.
8. CONTRACTOR TO VERIFY BUILDING DIMENSIONS WITH ARCHITECTURAL PLANS.
9. PLACE 3/4" INCH EXPANSION JOINT BETWEEN ALL P.C.C. PAVEMENT/SEWERMAKS AND BUILDINGS. PLACE 1/2" INCH EXPANSION JOINT BETWEEN SEWERMAKS AND P.C.C. PAVEMENT.
10. REMOVE ALL CURBS SPALLED INTO BLOCK AT THE END OF EACH WORK DAY.
11. ALL PROPERTY PINS SHALL BE PROTECTED FROM GRADING OR OTHER OPERATIONS. ANY PINS DISTURBED SHALL BE RESET AT THE CONTRACTOR'S EXPENSE.
12. DO NOT STORE CONSTRUCTION MATERIALS AND EQUIPMENT IN THE RIGHT OF WAY.
13. THE CONTRACTOR SHALL NOT DISTURB DESIGNATED GRASS AREAS AND EXISTING TREES OUTSIDE THE CONSTRUCTION LIMITS. THE CONTRACTOR WILL NOT BE PERMITTED TO PILE OR STORE MATERIALS AND EQUIPMENT ON USE, THESE AREAS FOR STORAGE OF MATERIALS, STORAGE, PAVING AND SERVICE AREAS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.
14. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY AREAS OF PAVEMENT OR EQUIPMENT ON THE PAVEMENT OR SIDEWALK.
15. THE CONTRACTOR SHALL BE RESPONSIBLE TO PLACE TEMPORARY WARNING DEVICES AND SAFETY FENCE AT CERTAIN LOCATIONS INSIDE REPLACEMENT FEATURED AREAS AS DIRECTED BY THE ENGINEER OR THE CITY.
16. THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR THE COORDINATION OF ALL CONSTRUCTION ACTIVITIES AND SEASONAL APPROXIMATIONS IN THE PROJECT, INCLUDING STAGING OF CONSTRUCTION DETAILS.
17. CONCRETE REMOVAL FOR SEWER APPROXIMATIONS SHALL BE REMOVED TO THE NEAREST TRANSVERSE JOINT. CONTRACTOR SHALL VERIFY REMOVAL LIMITS WITH CITY'S CONSTRUCTION SUPERVISOR PRIOR TO ANY CONCRETE REMOVAL.
18. ALL PERMITS AND ADDITIONAL FEES REQUIRED TO COMPLETE THE WORK SHALL BE INCLUDED IN THE CONTRACT BILL.
19. CONTRACTOR SHALL APPLY FOR SPECIAL PAVING PERMIT IN ADVANCE OF WORK.
20. ALL WORK WITHIN THE CITY RIGHT-OF-WAY REQUIRES A RIGHT-OF-WAY PERMIT.
21. CONTRACTOR SHALL APPLY FOR A SIDEWALK/DRIVEWAY APPROACH PERMIT IN ADVANCE OF WORK.

CITY OF DES MOINES STANDARD NOTES

1. ANY AMENDMENTS OR CHANGES TO THE PROJECT SITE THAT DO NOT MEET WHAT IS SHOWN ON THE SITE PLAN NEED TO BE APPROVED WITH THE PERMIT AND DEVELOPMENT CENTER PRIOR TO INSTALLATION/CONSTRUCTION.
2. LIGHTING SHALL CONSIST OF LOW-VOLTAGE CUT OFF TYPE FIXTURES TO REDUCE THE RISK OF LIGHT POLLUTION ON SURROUNDING PROPERTIES.
3. THE CONTRACTOR SHALL MAINTAIN EXISTING CURBS AND SIDEWALKS OR CITY MAINTAINED FOR THE LIFE OF THE CONTRACTUAL OF EQUIPMENT OR CERTIFICATE OF WORKING EQUIPMENT.
4. ALL DISTURBED AREAS SHALL BE RESTORED BY SEEDING OR SOCCOR.
5. THIS SITE SHALL BE MAINTAINED IN COMPLIANCE WITH ALL CITY CODES.
6. ANY GROUND-MOUNTED OR WALL-MOUNTED UTILITIES AND METERS SHALL BE PROTECTED AND NOT VISIBLE FROM A PUBLIC STREET.
7. ANY UTILITIES SHOWN ON THIS PLAN SHALL BE SCREENED ON ALL SIDES AND PROTECTED TO MATCH BUILDING.
8. ALL ROOFTOP MECHANICAL EQUIPMENT MUST BE SCREENED ON ALL SIDES WITH UTILITIES SCREENING TO THE REAR OF THE EQUIPMENT.
9. MECHANICAL EQUIPMENT SHALL NOT BE LOCATED IN THE FRONT YARD OR FRONT SIDE YARD AND SHALL BE SCREENED FROM VIEW FROM ANY PUBLIC WAY WITH LANDSCAPING, FENCING, OR WALLS CONSISTENT WITH THE BUILDING FRONT SEWING AND MATERIALS.
10. NO ROCK SHALL BE UTILIZED AS A GROUNDCOVER MATERIAL, PER CITY CODE SECTION 150-7.3.
11. FOR TOWNHOMES, INDIVIDUAL RESIDENTIAL TOTES FOR TRASH AND RECYCLING PROGRAM WILL BE SUPPLIED, PROVIDED, AND PLACED AT THE CURB UPON DAY OF POSSESSION.
12. CONTRACTOR SHALL CONFORM WITH THE PROVISIONS DESCRIBED IN THE GRAY'S STATION PLAN.

PAVEMENT THICKNESS

- | | |
|----------------------|---------------|
| 1. SIDEWALKS | 4" P.C.C. |
| 2. SLABS W/ SIDEWALK | 6" P.C.C. |
| 3. CYCLE TRACK | 6" P.C.C. |
| 4. PRIVATE DRIVES | 6" P.C.C. |
| 5. SIDEWALK RAMP | 6" P.C.C. |
| 6. PUBLIC STREETS | 7 1/2" P.C.C. |

GRAY'S STATION ADDRESSING			
PLAT	LOT NUMBER	ADDRESS NUMBER	STREET NAME
5	1	XXX	XXX
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5	3		
5	4		
5	5		
5	6		
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5	67		

NOTE: ALL PUBLIC IMPROVEMENTS, INCLUDING SPOT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR PUBLIC IMPROVEMENT PLANS. IF THERE ARE ANY DISCREPANCIES BETWEEN THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLANS THE DETAILED PUBLIC IMPROVEMENT PLANS SHALL GOVERN.

DATE: _____
 REVISIONS: _____
 STANDARD: _____
 10/10/2023
 SECOND SUBMITTAL
 FIRST SUBMITTAL

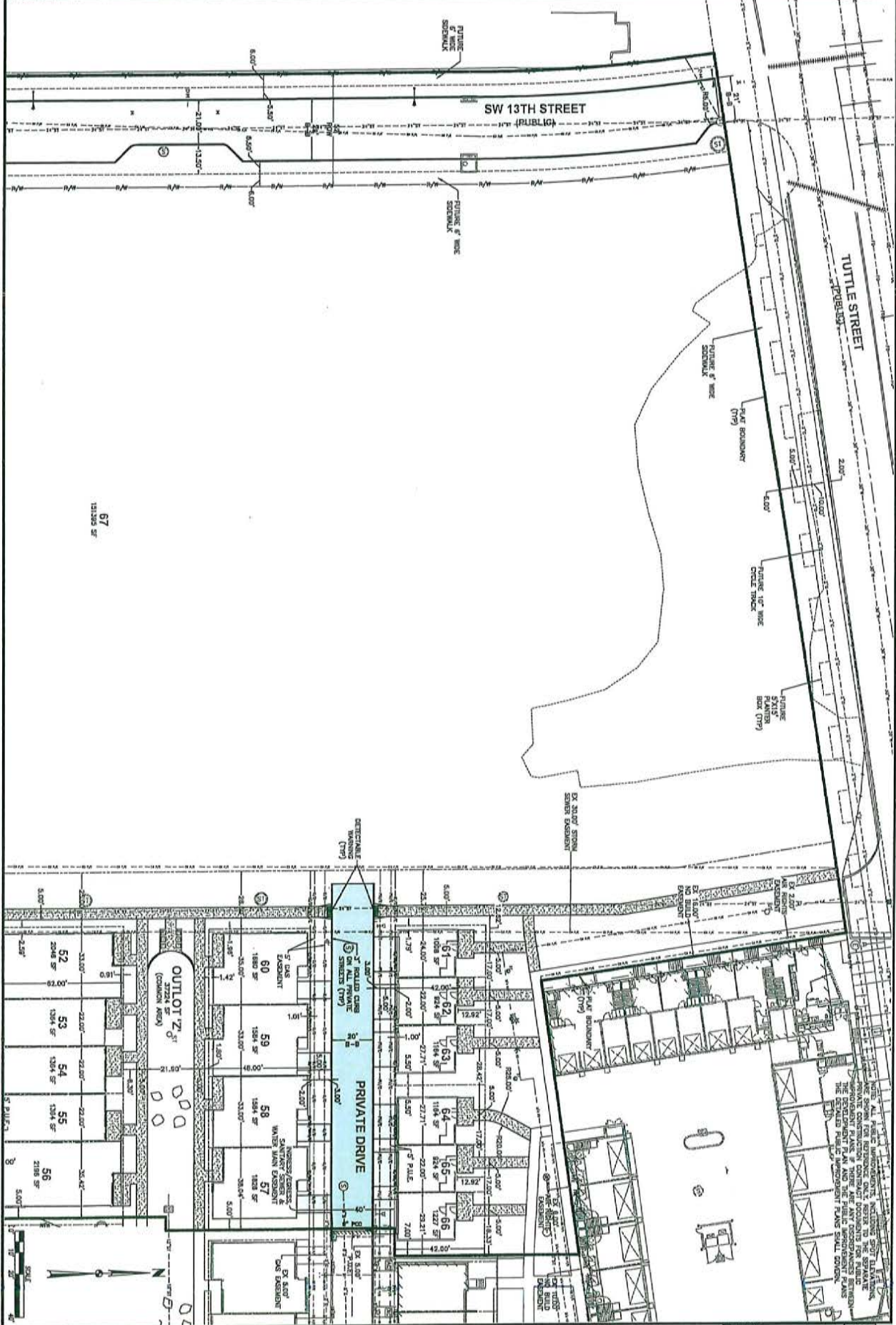
4121 NW URBANDALE DRIVE
 URBANDALE, IA 50322
 PHONE: (515) 365-4400

TECH: _____
 ENGINEER: _____

DES MOINES, IOWA

GRAY'S STATION PLAT 5
 DIMENSION PLAN

C2.1
 2202.113



C2.2
 2202.113

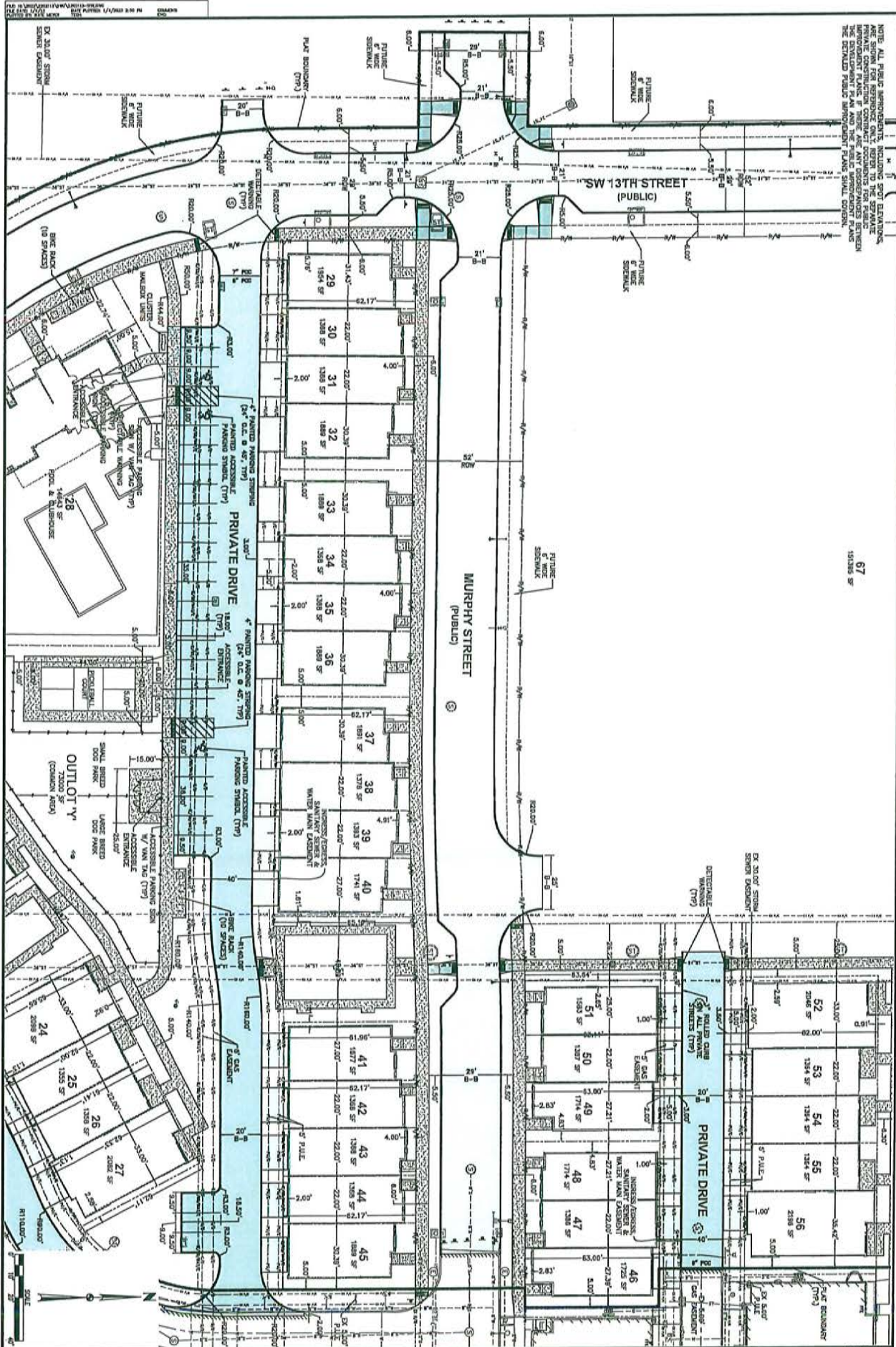
GRAY'S STATION PLAT 5
 DIMENSION PLAN



4121 NW URBANDALE DRIVE
 URBANDALE, IA 50322
 PHONE: (515) 369-4400

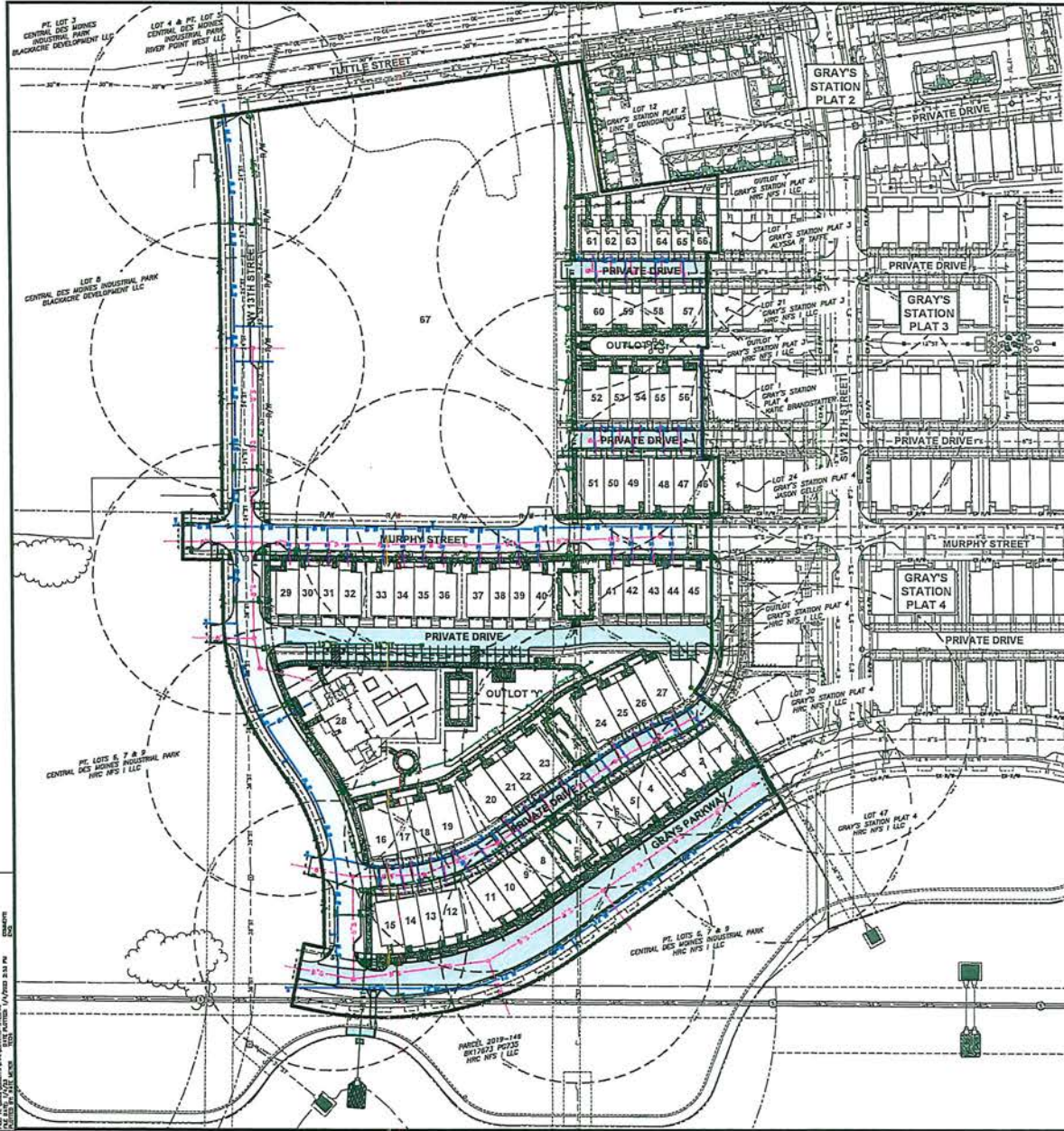
REVISIONS	DATE
SECOND SUBMITTAL	01/06/2003
FIRST SUBMITTAL	10/16/2002

NOTE: ALL FIELD MEASUREMENTS, DIMENSIONS, AND SETBACKS ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR PUBLIC MEASUREMENTS AND SETBACKS. THE DEVELOPER SHALL OBTAIN THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLAN FROM THE DESIGNED PUBLIC IMPROVEMENT PLANS SHALL DESIGN.



67
151265 SF

	GRAY'S STATION PLAT 5 DIMENSION PLAN		4121 NW URBANDALE DRIVE URBANDALE, IA 50322 PHONE: (515) 369-4400		REVISION: _____ DATE: _____
			DES MOINES, IOWA	ENGINEER: _____	TECH: _____



NOTE: ALL PUBLIC IMPROVEMENTS, INCLUDING SPOT ELEVATIONS, ARE SHOWN FOR REFERENCE ONLY. REFER TO THE SEPARATE PRIVATE CONSTRUCTION CONTRACT DOCUMENTS FOR PUBLIC IMPROVEMENT PLANS. IF THERE ARE ANY DISCREPANCIES BETWEEN THE DEVELOPMENT PLAN AND THE PUBLIC IMPROVEMENT PLANS THE DETAILED PUBLIC IMPROVEMENT PLANS SHALL GOVERN.

DATE: 11/15/2019
 TIME: 10:00 AM
 PROJECT: GRAY'S STATION PLAT 5
 SHEET: 2202.113

DATE	REVISIONS

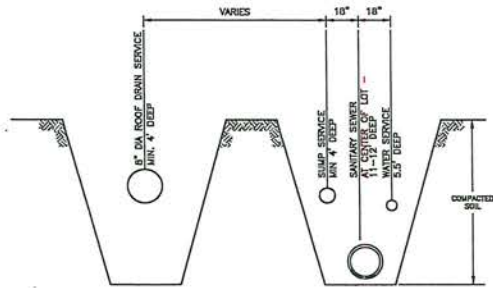
4124 NW URBANDALE DRIVE
 URBANDALE, IA 50322
 PHONE: (515) 369-4400
 TECH:



DES MOINES, IOWA
 CIVIL DESIGN ADVANTAGE
 ENGINEER:

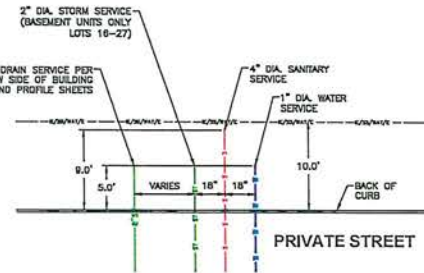
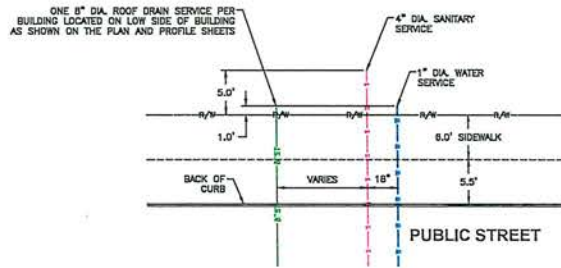
GRAY'S STATION PLAT 5
 OVERALL UTILITY PLAN

C4.0
 2202.113

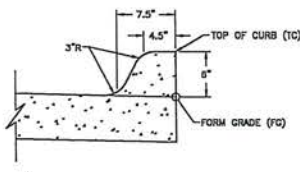


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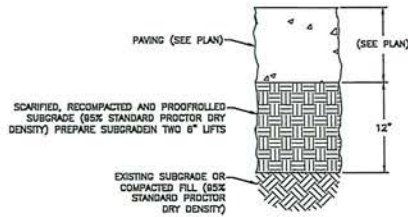
1. MARK THE LOCATION OF ALL SANITARY SEWER SERVICE STUBS AT THE TIME OF INSTALLATION BY A TWO-INCH WIDE DETECTABLE MARKING TAPE INSTALLED AT A DEPTH OF 18 INCHES TO 24 INCHES BELOW FINISHED GRADE, DIRECTLY OVER THE SERVICE STUB, FOR ITS ENTIRE LENGTH AND BROUGHT UP TO THE SURFACE AT THE END OF THE SERVICE STUB ADJACENT TO THE POST MARKING THE STUB LOCATION. THE TAPE SHALL BE GREEN IN COLOR AND MARKED "SANITARY SEWER SERVICE STUB BURIED BELOW".
2. PLACE ONE 6" BLUE STEEL POST BURIED 3' AT WATER CURB BOX.
3. PLACE ONE 6" RED STEEL POST BURIED 3' TO LOCATE STORM & ROOF DRAIN SERVICES.
4. 2" DIA STORM SERVICES SHALL BE INSTALLED ONLY FOR UNITS WITH BASEMENTS (LOTS 18-27).



1 SERVICE LOCATION DETAIL
NOT TO SCALE

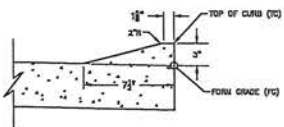


2 6" INTEGRAL CURB
NOT TO SCALE

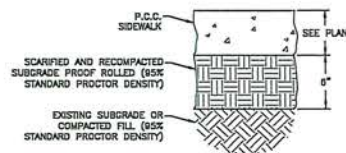


- NOTES:**
1. SUBGRADE PREPARATION SHALL EXTEND 24" BEYOND THE EDGE OF PAVING OR BACK OF CURB, EXCEPT WHEN ABUTTING EXISTING PAVEMENT.

3 12" SUBGRADE PREPARATION
NOT TO SCALE

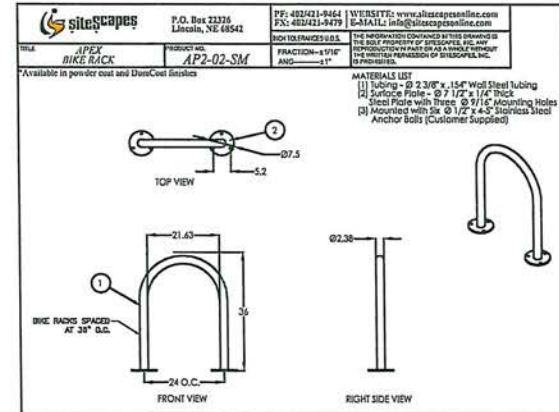


4 3" INTEGRAL ROLLED CURB
NOT TO SCALE



- NOTES:**
1. SUBGRADE PREPARATION SHALL EXTEND 12" BEYOND THE EDGE OF PAVING, EXCEPT WHEN ABUTTING EXISTING PAVEMENT.

5 SIDEWALK
NOT TO SCALE



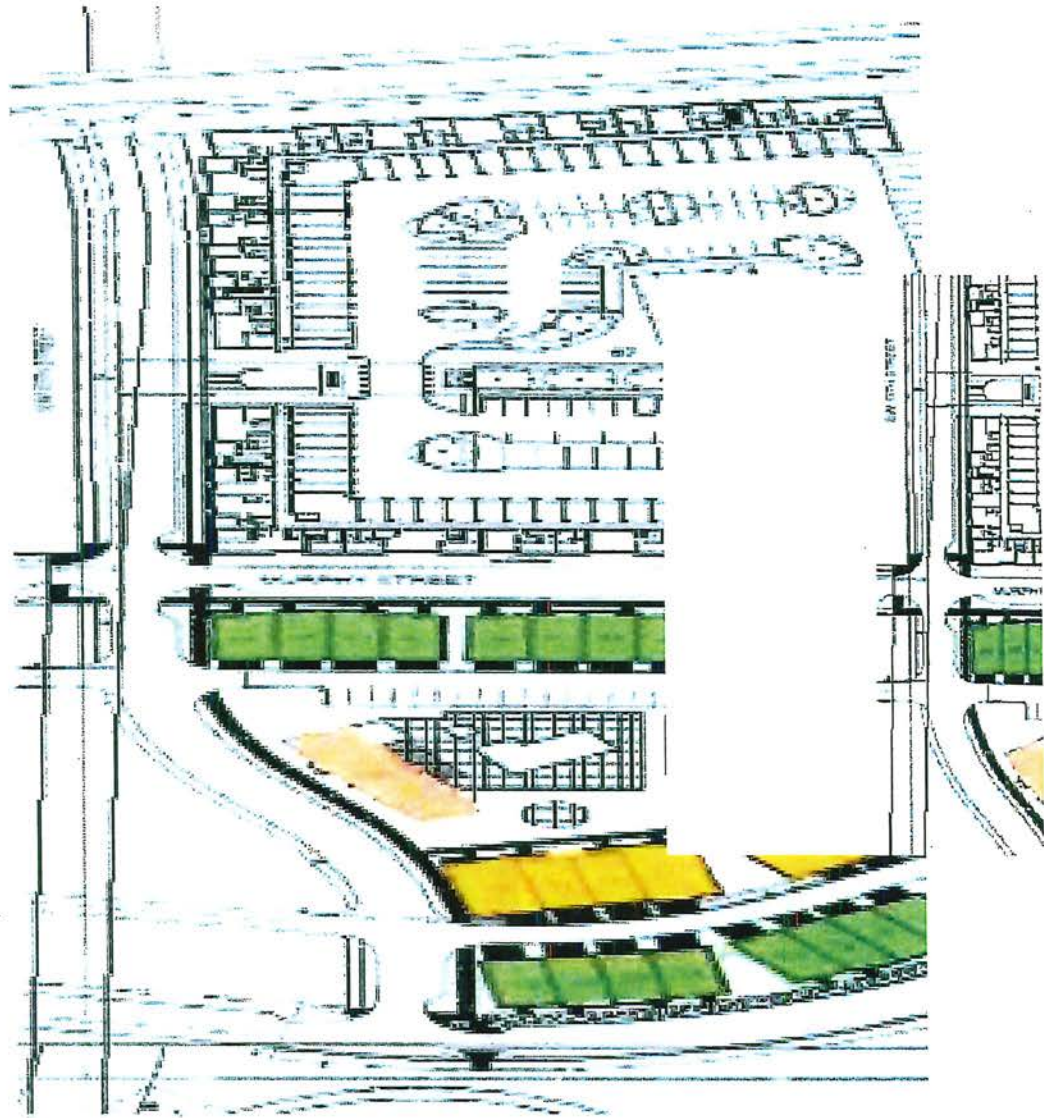
6 BIKE RACK
NOT TO SCALE

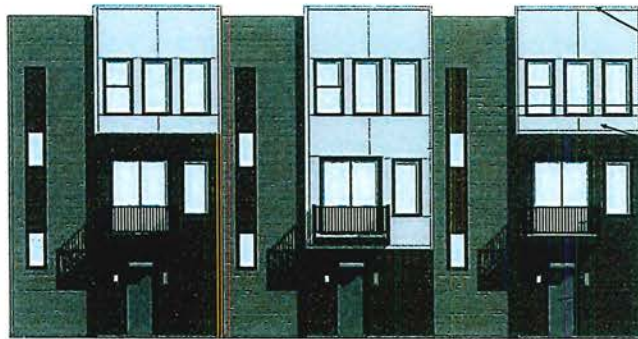
AND ALL DIMENSIONS UNLESS OTHERWISE SPECIFIED TO BE IN INCHES. DIMENSIONS SHOWN IN FEET AND INCHES SHALL BE TO FACE UNLESS OTHERWISE SPECIFIED.

DESIGN DEVELOPMENT

GRAY'S STATION PLAT 5

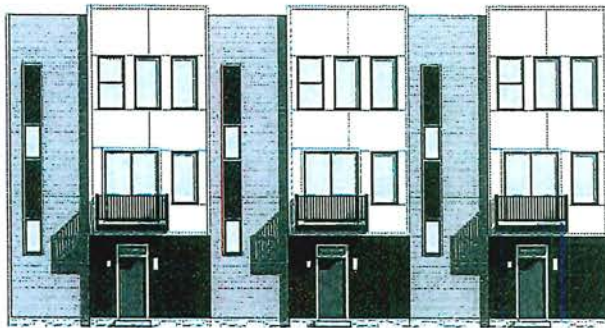
ABBREV.	COMMON NAME	BOTANICAL NAME	SIZE	NOTES
CO	COYOTE	COTONEASTER	2"	SEE PLAN
CR	CRABAPPLE	MALUS DOUGLASII	2"	SEE PLAN
CU	CURRYTREE	ANTHURUS	2"	SEE PLAN
DA	DAISY	GERBERA	2"	SEE PLAN
DE	DEERWEED	GERANIUM	2"	SEE PLAN
DI	DIANUS	DIANUS	2"	SEE PLAN
DR	DRAGONFLY	IRIDIUM	2"	SEE PLAN
EA	EASTWIND	ERIGONUM	2"	SEE PLAN
EL	ELM	ULMUS	2"	SEE PLAN
EM	EMERALD	EMERALD	2"	SEE PLAN
ER	ERGO	ERGO	2"	SEE PLAN
ES	ESSENCE	ESSENCE	2"	SEE PLAN
ET	ETERNAL	ETERNAL	2"	SEE PLAN
EU	EUROPEAN	EUROPEAN	2"	SEE PLAN
EV	EVANESCENS	EVANESCENS	2"	SEE PLAN
EW	EWING	EWING	2"	SEE PLAN
EX	EXETER	EXETER	2"	SEE PLAN
FA	FALLOUT	FALLOUT	2"	SEE PLAN
FB	FBI	FBI	2"	SEE PLAN
FC	FCC	FCC	2"	SEE PLAN
FD	FDD	FDD	2"	SEE PLAN
FE	FEE	FEE	2"	SEE PLAN
FF	FFF	FFF	2"	SEE PLAN
FG	FGG	FGG	2"	SEE PLAN
FH	FHH	FHH	2"	SEE PLAN
FI	FII	FII	2"	SEE PLAN
FJ	FJJ	FJJ	2"	SEE PLAN
FK	FKK	FKK	2"	SEE PLAN
FL	FLF	FLF	2"	SEE PLAN
FM	FMM	FMM	2"	SEE PLAN
FN	FNN	FNN	2"	SEE PLAN
FO	FOO	FOO	2"	SEE PLAN
FP	FPF	FPF	2"	SEE PLAN
FQ	FQQ	FQQ	2"	SEE PLAN
FR	FRF	FRF	2"	SEE PLAN
FS	FSF	FSF	2"	SEE PLAN
FT	FTF	FTF	2"	SEE PLAN
FU	FUF	FUF	2"	SEE PLAN
FV	FVF	FVF	2"	SEE PLAN
FW	FWF	FWF	2"	SEE PLAN
FX	FXF	FXF	2"	SEE PLAN
FY	FYF	FYF	2"	SEE PLAN
FZ	FZF	FZF	2"	SEE PLAN
GA	GAA	GAA	2"	SEE PLAN
GB	GBB	GBB	2"	SEE PLAN
GC	GCC	GCC	2"	SEE PLAN
GD	GDD	GDD	2"	SEE PLAN
GE	GED	GED	2"	SEE PLAN
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GM	GGI	GGI	2"	SEE PLAN
GN	GGJ	GGJ	2"	SEE PLAN
GO	GGK	GGK	2"	SEE PLAN
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GU	GGI	GGI	2"	SEE PLAN
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- ROOF MEMBRANE -
CARLISLE SURE WELD TPO | GRAY
- FIBER CEMENT LAP SIDING & TRIM -
HARDIE | AGED PEWTER
- FIBER CEMENT PANEL & TRIM -
HARDIE | PEARL GRAY
- RAILING | BLACK
- METAL PANEL -
FIRESTONE | DARK BRONZE
- PAINT -
SW 7674—PEPPERCORN

NEW FRONT ELEVATION



OLD FRONT ELEVATION

Elevation Updates in Phase II

In Phase 2 we are proposing the same Product types/units as were approved for Phase 1. The floorplans for each unit type will remain the same. We made some revisions to the Elevations and Exterior Color Packages as we felt that they needed to be updated for Phase II. Some of the elevations didn't have enough intentional variations and the color packages were too light in color for an urban neighborhood.

The following pages show the new and the old front elevations of each product type and a description of the changes.

Product 1

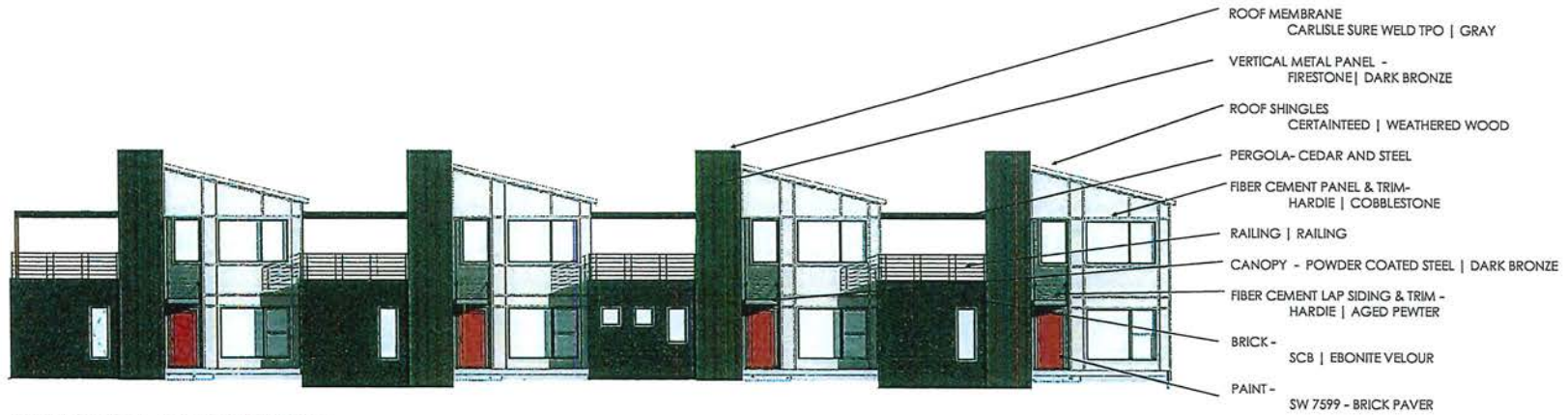
The desire was to create a darker color package for this Product and add some variation between the units.

- Increased visual interest through varying heights of the entry element clad in metal panels.
- Revised the color scheme and made it darker to achieve a more urban look.

PRODUCT 1 | COMPARISON

COLOR PACKAGE: Product 1—#1

GRAY'S STATION | 07.07.2022 | 3

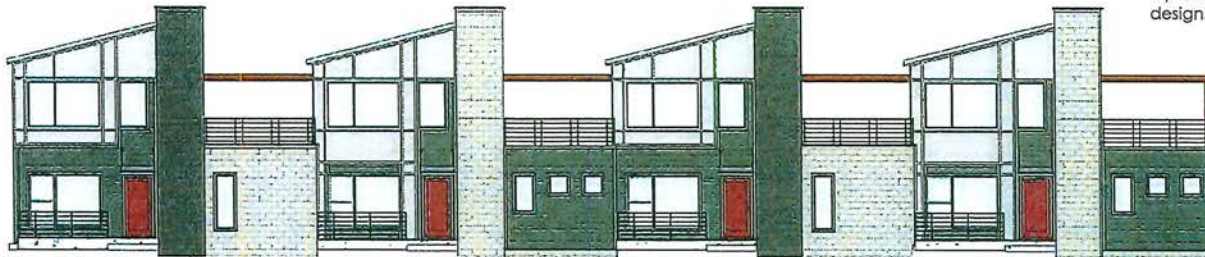


NEW FRONT ELEVATION

Product 3

This elevation looked too busy and had too many design themes and variations.

- Revised vertical elements to all be the same color and revised material to dark metal panels with a vertical pattern.
- The cube-shaped building elements were revised from Hardie lap siding/stone to all stone, to unify the overall building design.



OLD FRONT ELEVATION

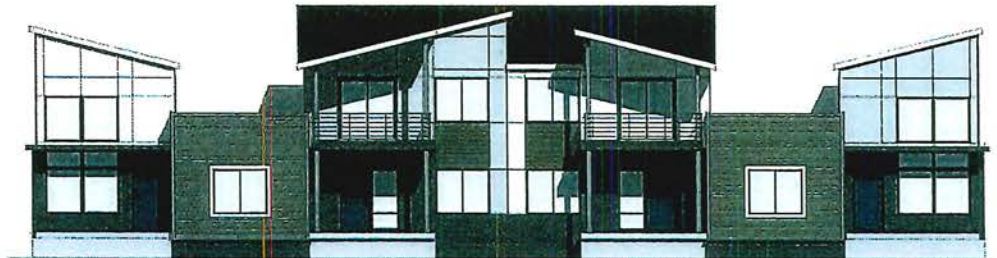
PRODUCT 3 | COMPARISON

COLOR PACKAGE: Product 3—#1

GRAY'S STATION | 07.07.2022 | 4



NEW FRONT ELEVATION



OLD FRONT ELEVATION

Product 4

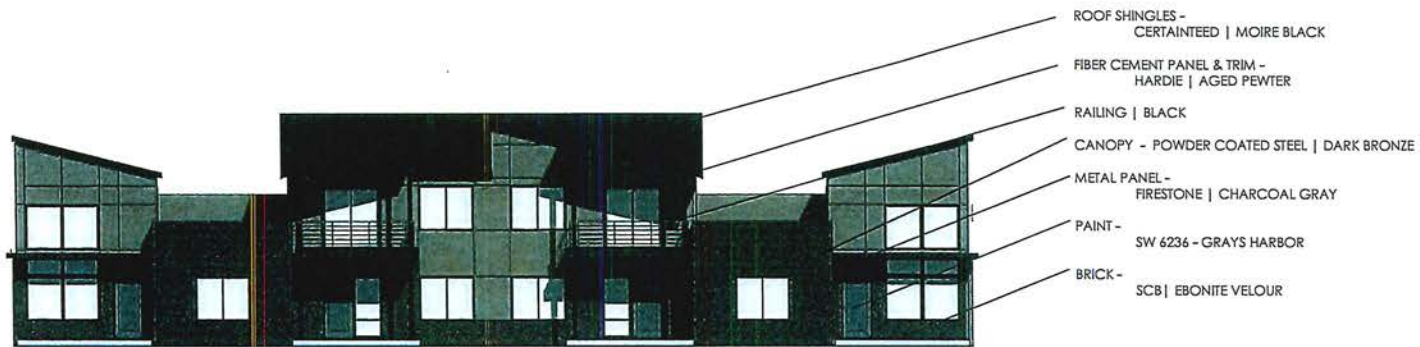
This elevation was too busy and the color scheme too light for this urban environment.

- Changed to a darker color scheme to achieve a more urban look.
- Changed Hardie lap siding on front to metal panels.
- Eliminated the white-colored Hardie panels and matched the panel color on both sides to achieve a unified appearance.
- Steel metal posts will be painted dark gray.

PRODUCT 4 | COMPARISON

COLOR PACKAGE: Product 4—#1

GRAY'S STATION | 07.07.2022 | 5

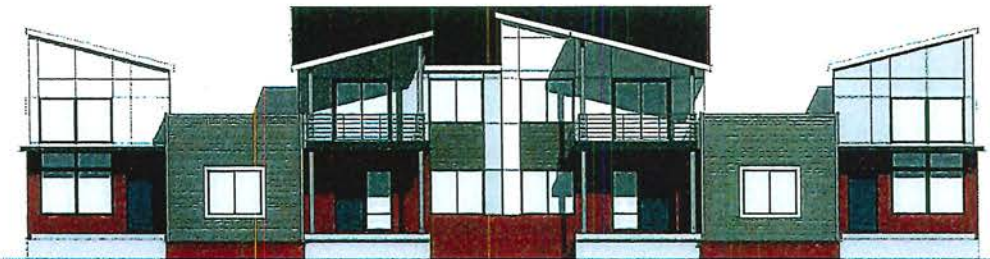


NEW FRONT ELEVATION

Product 4

This elevation was too busy and the color scheme too light for this urban environment.

- Changed to a darker color scheme to achieve a more urban look.
- Changed Hardie lap siding on front to metal panels.
- Eliminated the white-colored Hardie panels and matched the panel color on both sides to achieve a unified appearance.
- Steel metal posts will be painted dark gray.

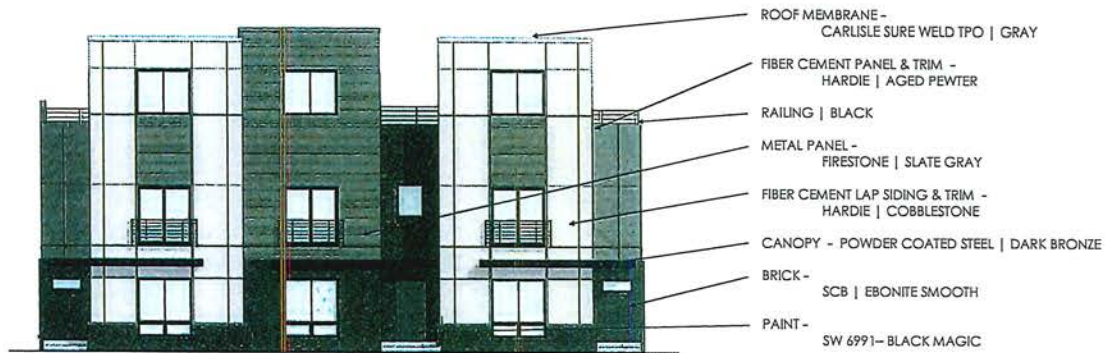


OLD FRONT ELEVATION

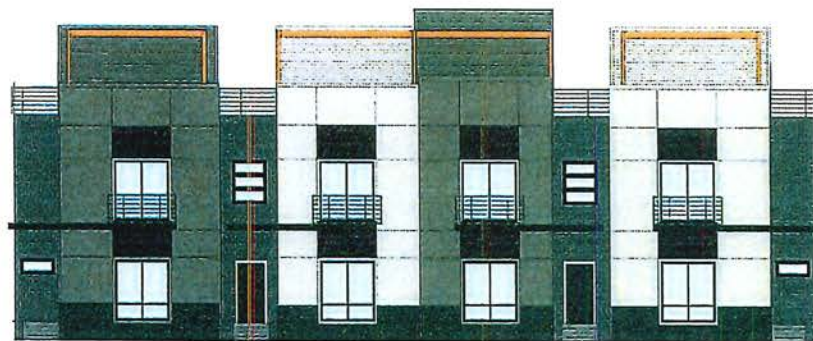
PRODUCT 4 | COMPARISON

COLOR PACKAGE: Product 4—#2

GRAY'S STATION | 07.07.2022 | 6



NEW FRONT ELEVATION



OLD FRONT ELEVATION

Product 5

Elevations appear repetitive and uninspired. We only built one Product 5 building in Phase 1 so far. It was built without the 3rd level optional bonus room. We intend to build the bonus room on Product 5 in Phase 2.

- Varied the height of the brick to create diverse building elements and to reduce the effect of the stark vertical building elements.
- Replaced the Hardie panels of one of the main building elements with metal panels.
- Black windows

PRODUCT 5 | COMPARISON

COLOR PACKAGE: Product 5—#1

GRAY'S STATION | 07.07.2022 | 7



NEW FRONT ELEVATION



OLD FRONT ELEVATION

Product 6

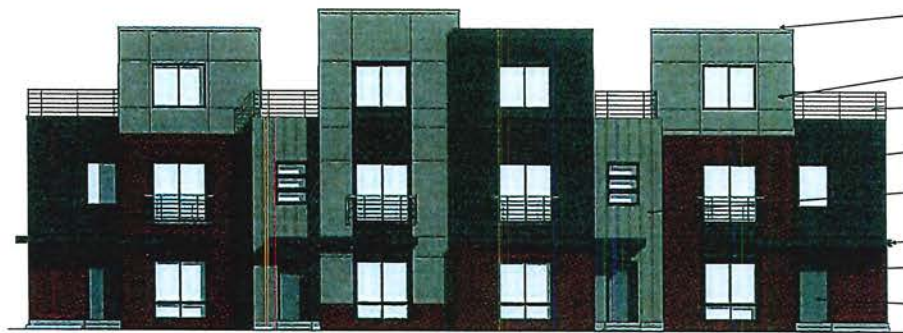
Elevation appeared boring and repetitive. These needed additional visual interest and a more urban look.

- Varied the height of the brick/stone to create diverse building elements and to reduce the repetitive effect of the vertical building elements.
- Introduced building elements sided in metal panels with a vertical pattern.
- Replaced the Hardie panels of one or more of the main building elements with metal panels (horizontal pattern).
- Black windows
- Darkened the color packages

PRODUCT 6 | COMPARISON

COLOR PACKAGE: Product 6—#1

GRAY'S STATION | 07.07.2022 | 8



NEW FRONT ELEVATION

- ROOF MEMBRANE - CARLISLE SURE WELD TPO | GRAY
- FIBER CEMENT PANEL & TRIM - HARDIE | AGED PEWTER
- RAILING | BLACK
- METAL PANEL - FIRESTONE | DARK BRONZE
- VERTICAL METAL PANEL - FIRESTONE | SLATE GRAY
- CANOPY - POWDER COATED STEEL | DARK BRONZE
- BRICK - SCB | MOUNTAIN SHADOW SMOOTH
- PAINT - SW 6991- BLACK MAGIC



OLD FRONT ELEVATION

Product 6

Elevation appeared boring and repetitive. These needed additional visual interest and a more urban look.

- Varied the height of the brick/stone to create diverse building elements and to reduce the repetitive effect of the vertical building elements.
- Introduced building elements sided in metal panels with a vertical pattern.
- Replaced the Hardie panels of one or more of the main building elements with metal panels (horizontal pattern).
- Black windows
- Darkened the color packages

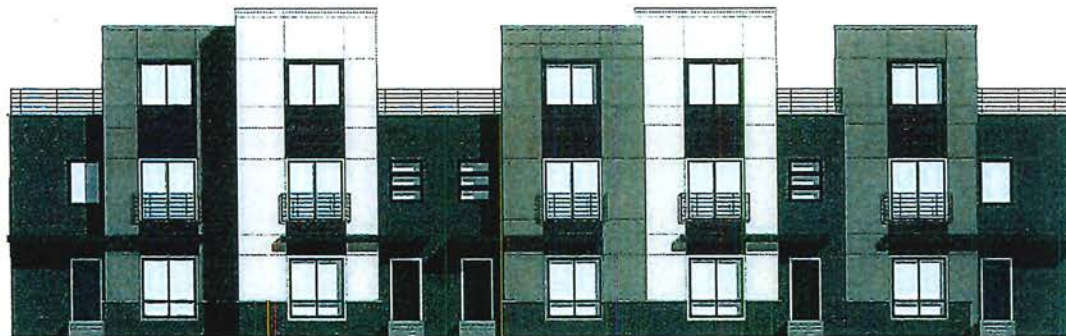
PRODUCT 6 | COMPARISON

COLOR PACKAGE: Product 6—#2

GRAY'S STATION | 07.07.2022 | 9



NEW FRONT ELEVATION



OLD FRONT ELEVATION

Product 6

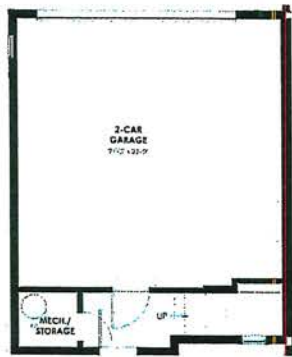
Elevation appeared boring and repetitive. These needed additional visual interest and a more urban look.

- Varied the height of the brick/stone to create diverse building elements and to reduce the repetitive effect of the vertical building elements.
- Introduced building elements sided in metal panels with a vertical pattern.
- Replaced the Hardie panels of one or more of the main building elements with metal panels (horizontal pattern).
- Black windows
- Darkened the color packages

PRODUCT 6 | COMPARISON

COLOR PACKAGE: Product 6—#3

GRAY'S STATION | 07.07.2022 | 10



FIRST FLOOR



SECOND FLOOR

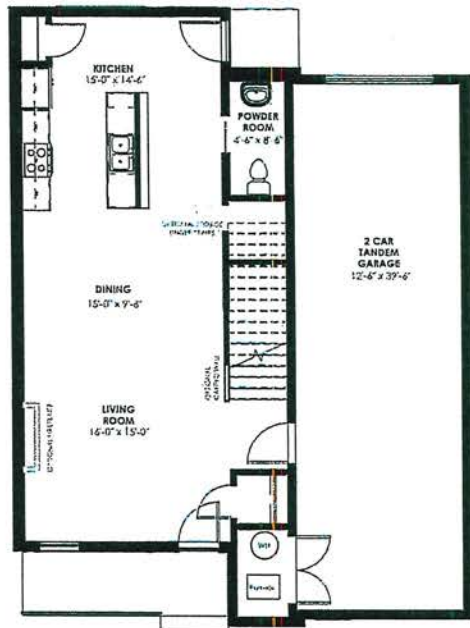


THIRD FLOOR

PRODUCT 1 | PRESTON

Lots: 61-63 and 64-66

GRAY'S STATION | 07.07.2022 | 45

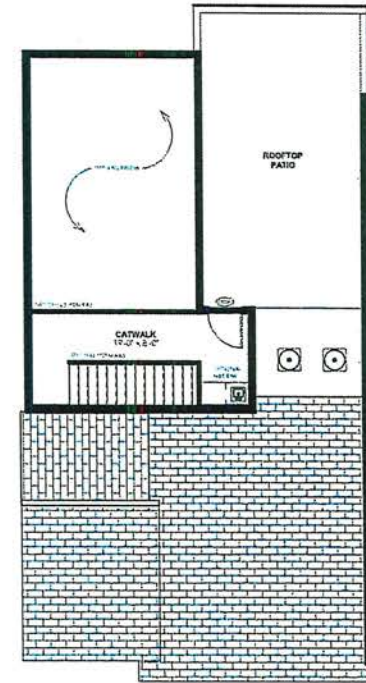
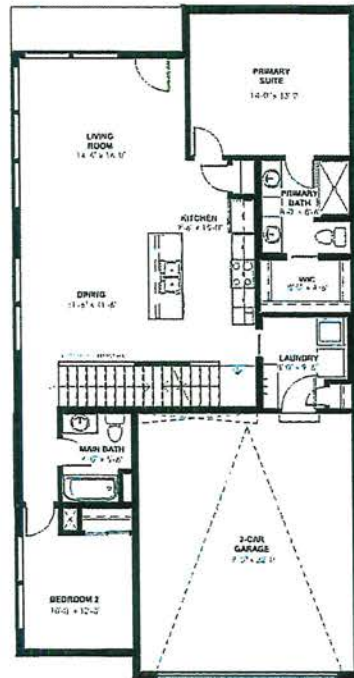
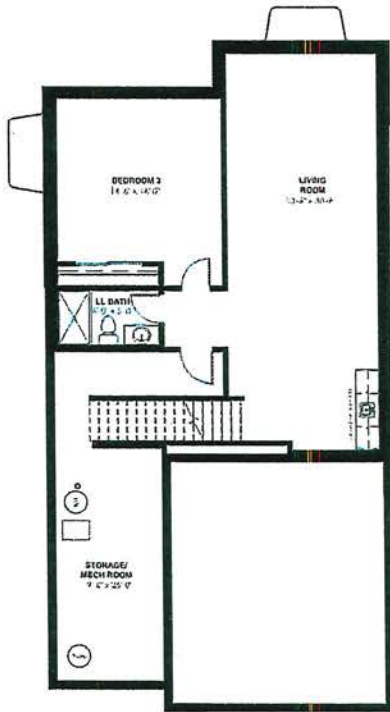


FIRST FLOOR

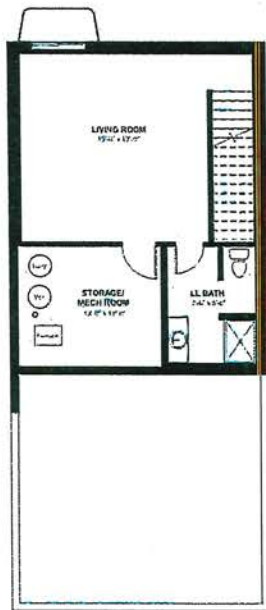


SECOND FLOOR

PRODUCT 3 | BEXLEY
 Lots: 57-60
 GRAY'S STATION | 07.07.2022 | 46



PRODUCT 4 | MAISON
 Lots: 16&19, 20&23, 24&27 and 52&56
 GRAY'S STATION | 07.07.2022 | 47



LOWER LEVEL



FIRST FLOOR

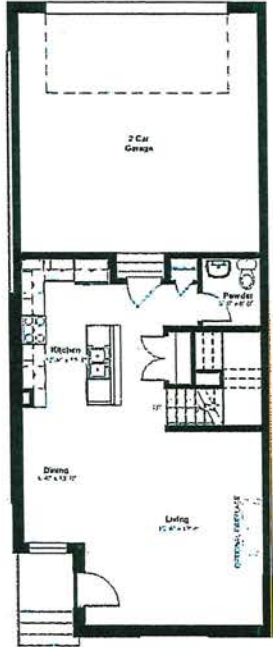


SECOND FLOOR

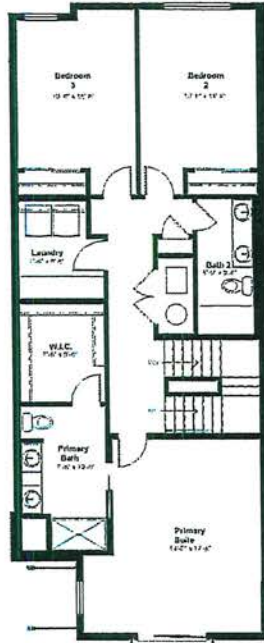
PRODUCT 4 | MARLOW

Lots: 17&18, 21&22, 25&26, and 53-55

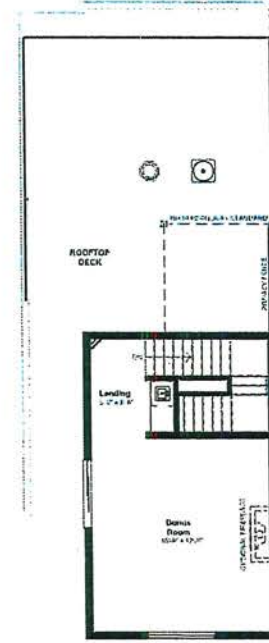
GRAY'S STATION | 07.07.2022 | 48



FIRST FLOOR



SECOND FLOOR

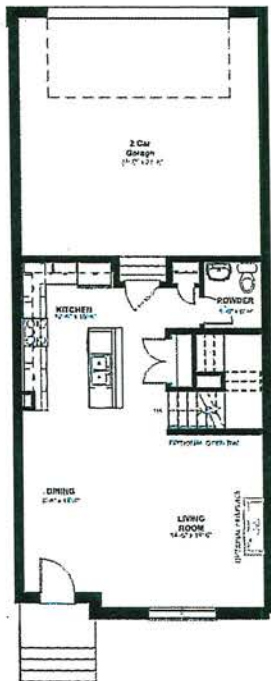


THIRD FLOOR

PRODUCT 5 | CHAVA

Lots: 46&48 and 49&51

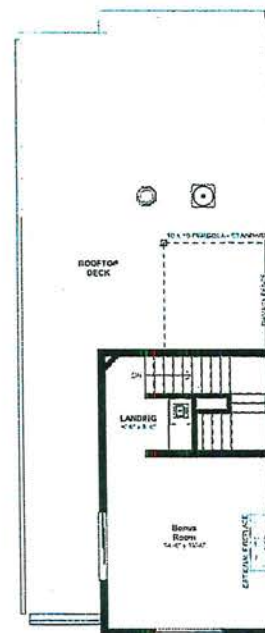
GRAY'S STATION | 07.07.2022 | 49



FIRST FLOOR



SECOND FLOOR



THIRD FLOOR

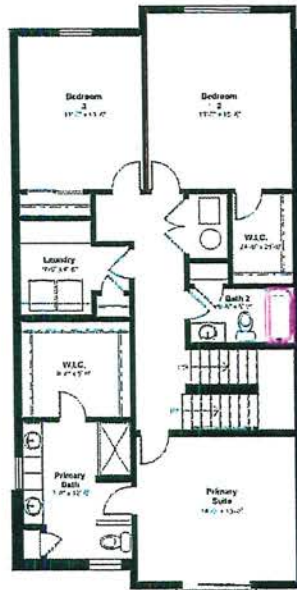
PRODUCT 5 | ASANA

Lots: 47 and 50

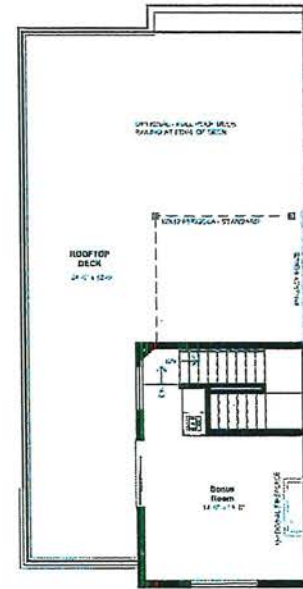
GRAY'S STATION | 07.07.2022 | 50



FIRST FLOOR



SECOND FLOOR

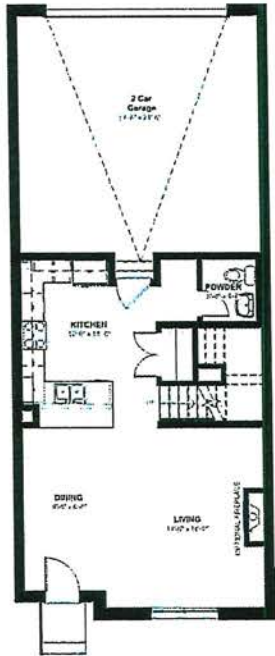


THIRD FLOOR

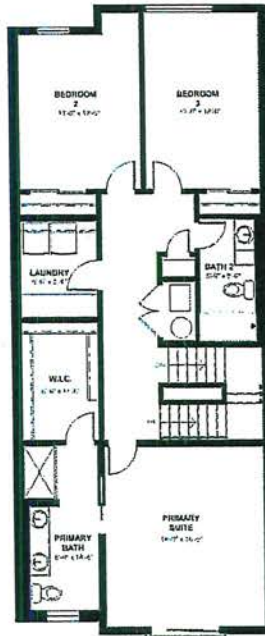
PRODUCT 6 | GRAYSON

Lots: 1&3, 4&7, 8&11, 12&15, 29&32, 33&36, 37&40, and 41&45

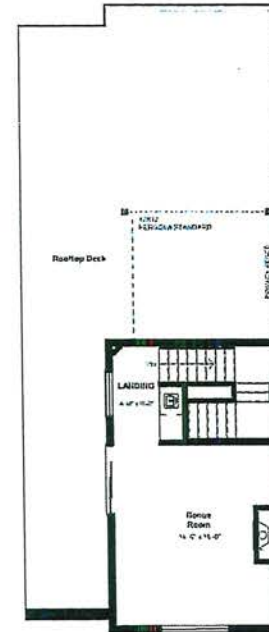
GRAY'S STATION | 07.07.2022 | 51



FIRST FLOOR

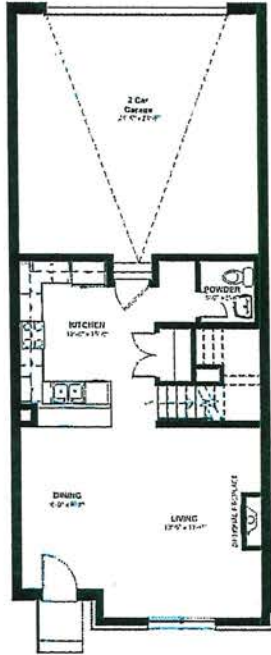


SECOND FLOOR

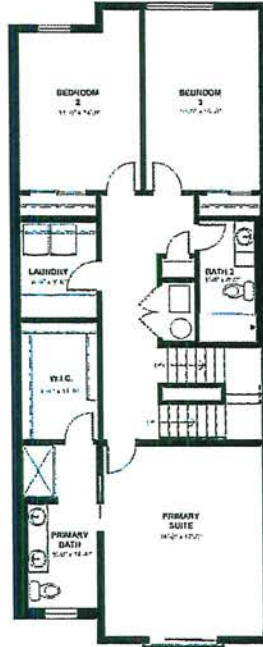


THIRD FLOOR

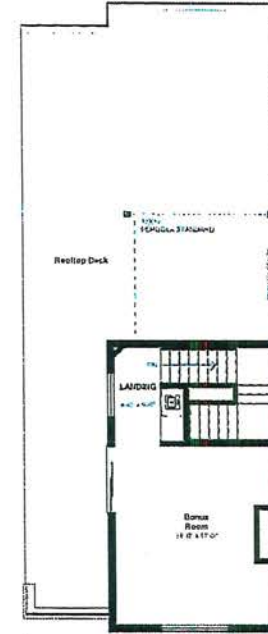
PRODUCT 6 | CORDOVA
 Lots: 2, 5, 9, 13, 30, 34, 38 and 42&44
 GRAY'S STATION | 07.07.2022 | 52



FIRST FLOOR



SECOND FLOOR



THIRD FLOOR

PRODUCT 6 | CORDOVA PLUS

Lots: 6, 10, 14, 31, 35, 39 and 43

GRAY'S STATION | 07.07.2022 | 53



Hubbell Homes
Des Moines, Iowa

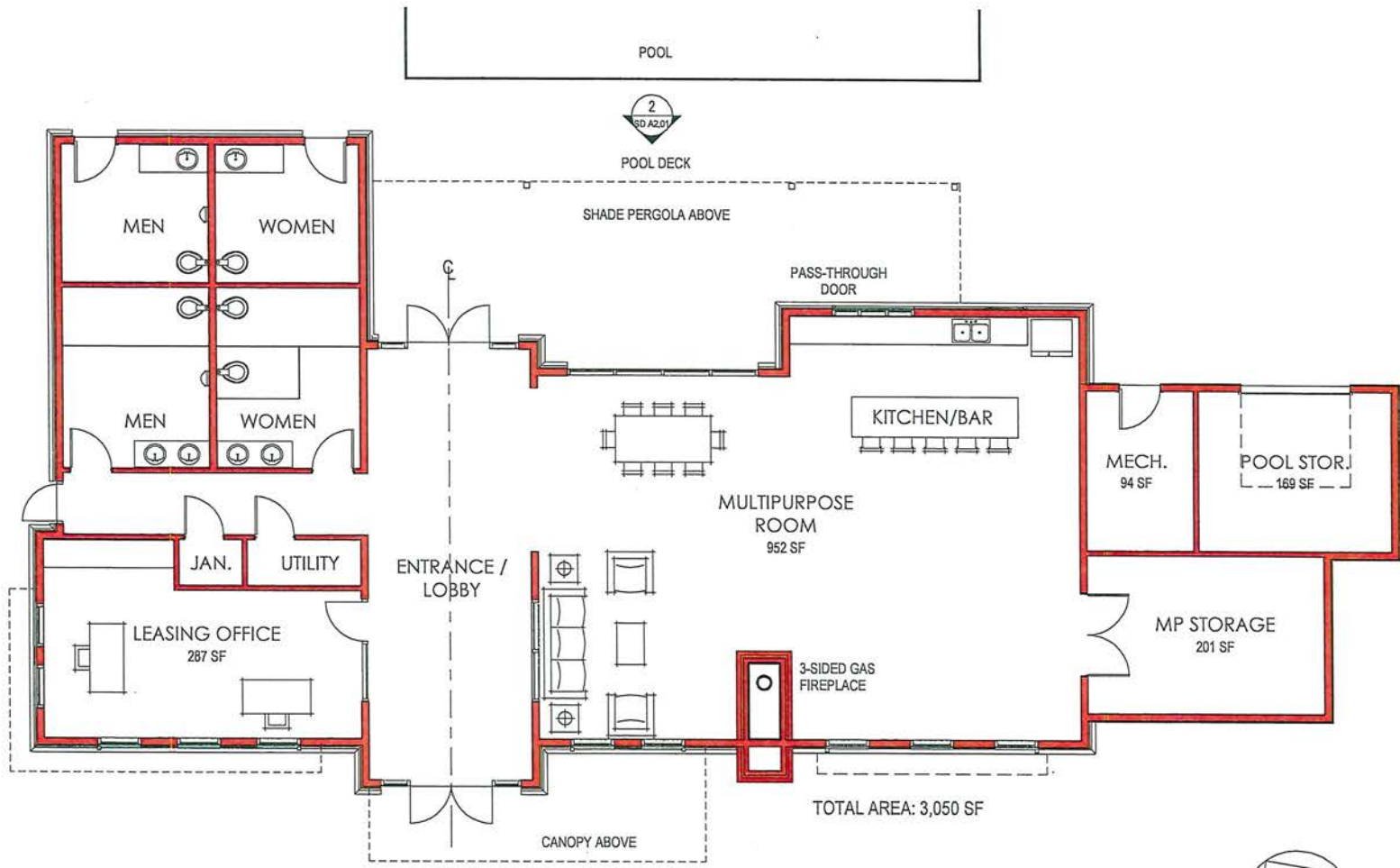
PERSPECTIVE

GRAY'S STATION CLUBHOUSE

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[AM220361]





TOTAL AREA: 3,050 SF

CONCEPTUAL FLOOR PLAN 7/14/2022

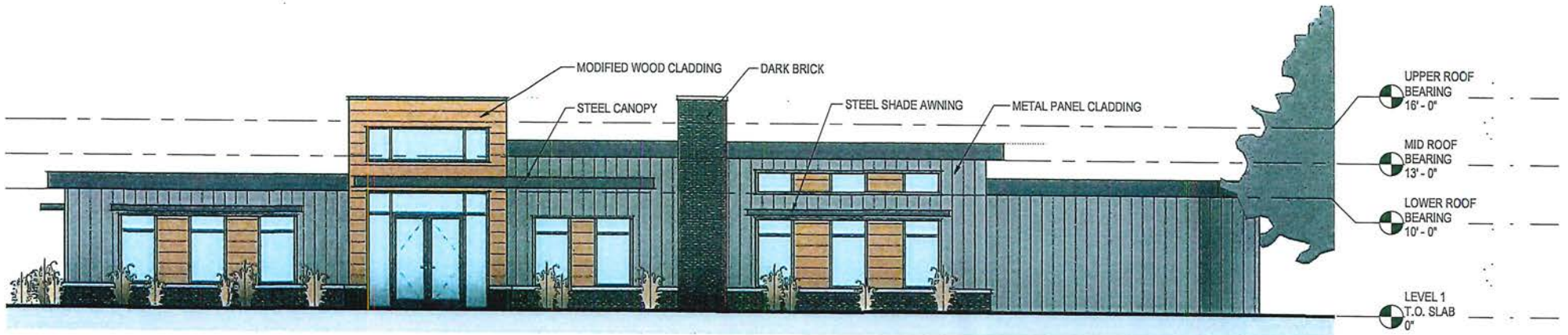
GRAY'S STATION CLUBHOUSE

Hubbell Homes
Des Moines, Iowa

The drawings presented are illustrative character and design ideas only, and are subject to change based upon final design considerations (i.e. applicable codes, structural, and MEP design requirements, unit placement plan changes, etc.) © 2018 BSB Design, Inc.

[AM220361]





1 SOUTH ELEVATION
1/8" = 1'-0"



2 NORTH ELEVATION
1/8" = 1'-0"

ELEVATIONS

Hubbell Homes
Des Moines, Iowa

GRAY'S STATION CLUBHOUSE

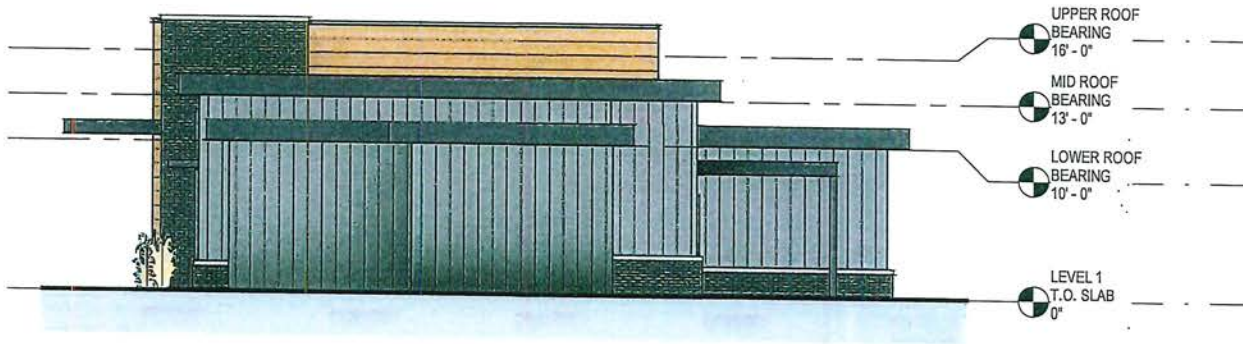


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[AM220361]



1 WEST ELEVATION
1/8" = 1'-0"



2 EAST ELEVATION
1/8" = 1'-0"

ELEVATIONS